

NOTES:

1. ALL TRANSVERSE CONTRACTION JOINTS SHALL MATCH AND ALIGN WITH JOINTS IN CURB AND GUTTER UNLESS PAVING AND CURBS ARE SEPARATED BY AN ISOLATION JOINT. JOINTS IN CUL-DE-SAC CURBS SHOULD BE PLANNED TO MATCH JOINT PATTERN IN PAVING.
2. MAXIMUM JOINT SPACING, IN FEET, SHALL BE 2 ½ TIMES THE PAVEMENT THICKNESS IN INCHES. (EXAMPLE: 8" THICKNESS x 2.5 = 20' SPACING).
3. SPECIAL TREATMENT WILL BE REQUIRED FOR JOINTING ADJACENT TO MANHOLES, VAULTS OR OTHER STRUCTURES INCORPORATED INTO THE PAVING SURFACE.
4. USE APPROVED FLEXIBLE CRACK SEALANT TO FILL ALL CONTRACTION JOINTS FLUSH WITH PAVEMENT SURFACE.
5. SEE SUPP. STD. DWG. 00700-02 FOR JOINT DETAILS.

CITY of LEBANON 2021 SUPPLEMENTAL STANDARD DRAWING



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CITY ENGINEER

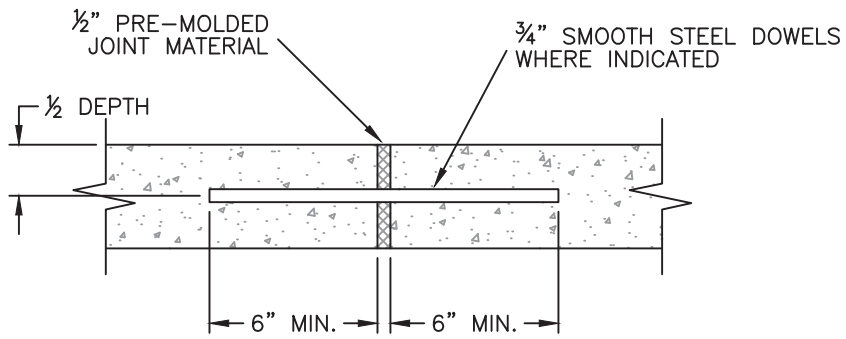
December, 2021

DATE

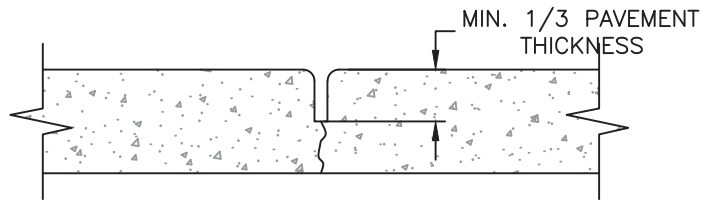
**CONTRACTION JOINT
PATTERN FOR
P.C.C. PAVING**

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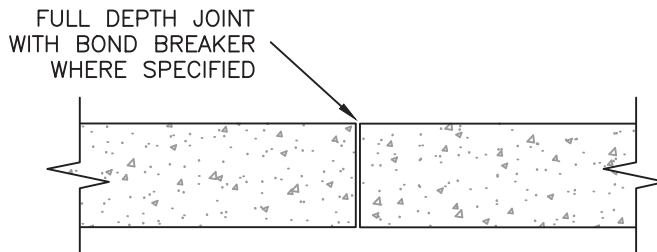
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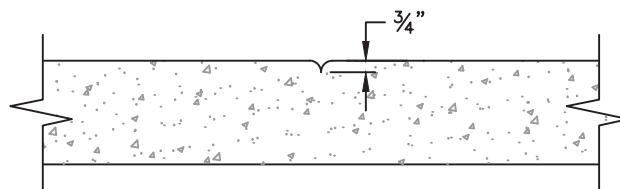
TYPICAL EXPANSION JOINT
(WITH DOWELS WHERE SPECIFIED)



TYPICAL CONTRACTION JOINT



TYPICAL ISOLATION (COLD) JOINT



TYPICAL TOOLED (DUMMY) JOINT

NOTE:

1. ALL JOINTS SHALL BE TOOLED WITH 3/4" RADIUS UNLESS SAWCUT.

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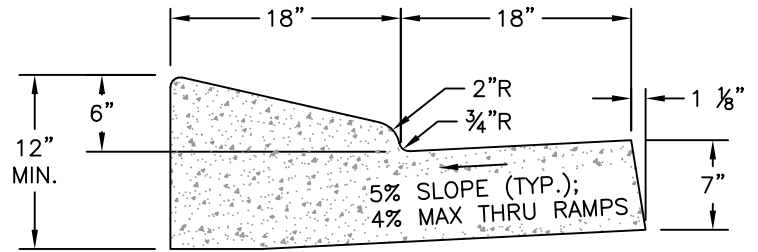
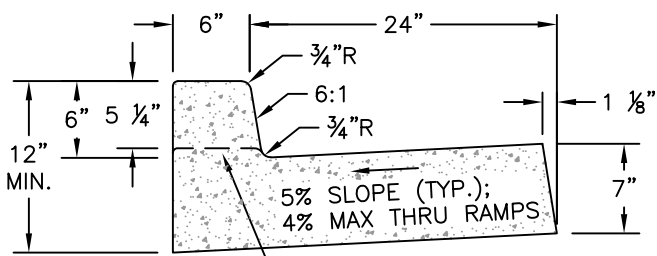
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P.C.C. PAVING JOINTS

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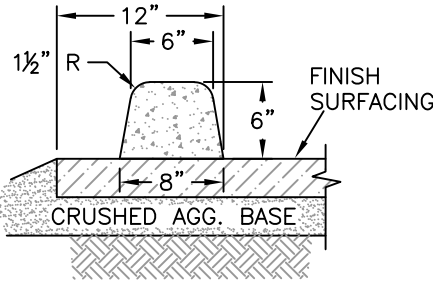
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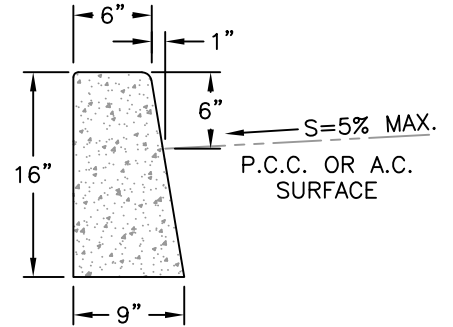
CURB FOR DRIVEWAY ACCESS $\frac{3}{4}$ " TYP.
 CURB FOR SIDEWALK ACCESS RAMP 0" TYP.
 SLOPE=5% NORMAL OR 4% MAX. AT CURB RAMPS
 2 FOOT WIDTH MINIMUM A.C.P. PATCH

MOUNTABLE CURB & GUTTER

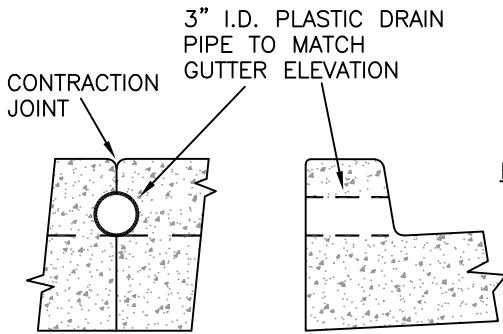
STANDARD CURB & GUTTER



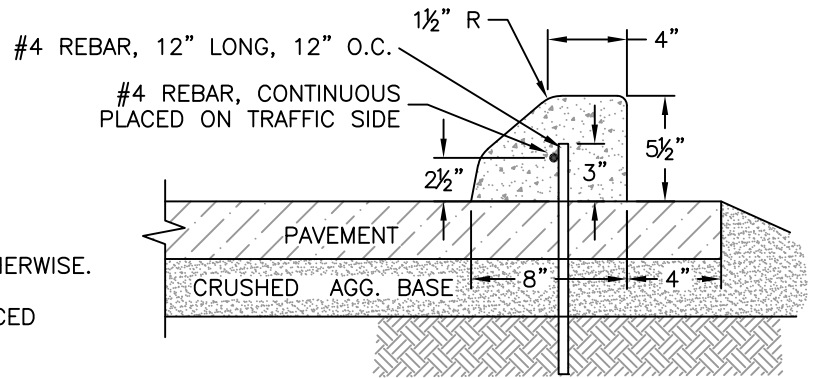
EXTRUDED P.C.C. BONDED CURB



TYPICAL STRAIGHT CURB



STANDARD WEEPHOLE



DOWEL REINFORCED CURB

NOTES:

1. ALL RADII SHALL BE $\frac{3}{4}$ " UNLESS SPECIFIED OTHERWISE.
2. ISOLATION AND EXPANSION JOINTS SHALL BE PLACED ONLY AS SPECIFIED.
3. CONTRACTION JOINTS SHALL BE PLACED AT 15' INTERVALS AND OVER WEEPHOLE LOCATIONS UNLESS SPECIFIED OTHERWISE. JOINTS SHALL EXTEND AT LEAST 50% THROUGH THE CURB OR CURB & GUTTER SECTION.
4. WEEPHOLES SHALL BE PLACED WITHIN 10' OF PROPERTY LINES AND SHALL NOT EXCEED 100 FOOT SPACING. WEEPHOLES SHALL NOT BE PLACED WITHIN 1.0' OF ANY DRIVEWAY OR SIDEWALK RAMP SECTION AND SHALL BE ACCOMPANIED BY A TOOLED CONTRACTION JOINT.
5. CONSTRUCT EXPANSION JOINTS AT 200' MAXIMUM SPACING, AT POINTS OF TANGENCY, AND AT EACH END OF DRIVEWAYS.
6. CURB INSTALLATIONS WITHIN EXISTING ROADWAYS REQUIRE A 2.0' MINIMUM REMOVAL AND REPLACEMENT OF ADJACENT PAVEMENT (SEE SUPP. STD. DWG. 00400-02).
7. STANDARD P.C.C. CURBING SHALL HAVE A 28 DAY COMPRESSIVE STRENGTH OF 3000 P.S.I. (MIN).
8. REINFORCED P.C.C. CURBING SHALL HAVE A 28 DAY COMPRESSIVE STRENGTH OF 4000 P.S.I. (MIN.) AND SHALL BE USED ONLY WHEN SPECIFIED.
9. REFER TO SUPP. STD. DWG. 00300-01 FOR ADDITIONAL INFORMATION.

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Dec., 2021

DATE

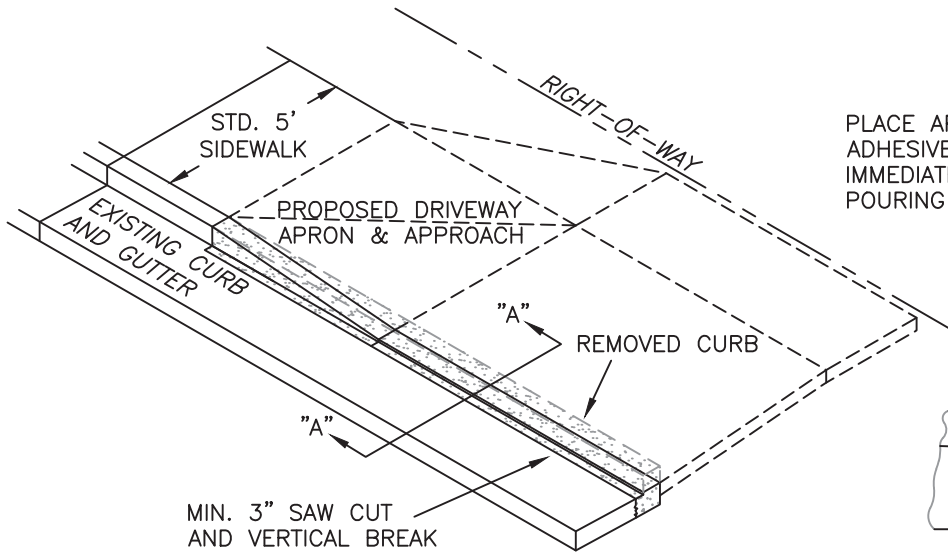
Aug., 2023

REVISIONS

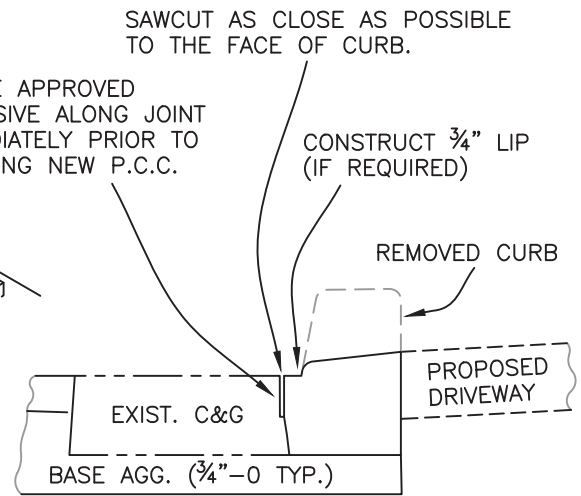
**TYPICAL
CURB & GUTTER**

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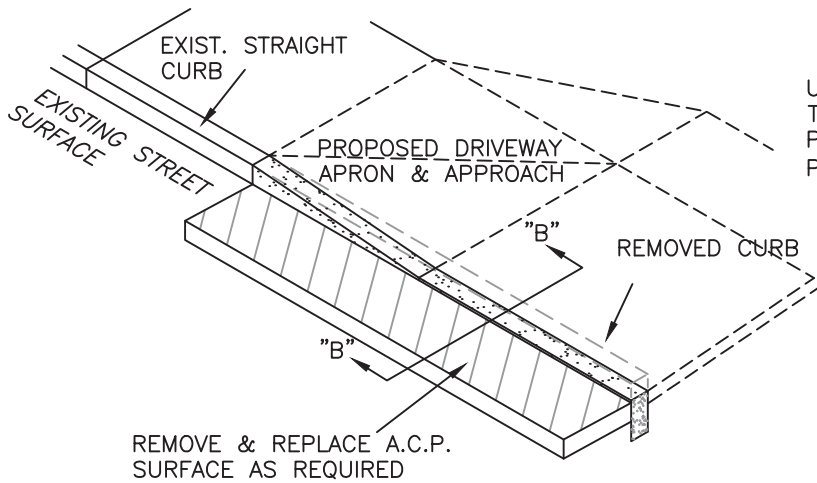
00700-03



STANDARD CURB & GUTTER KNOCK-OUT

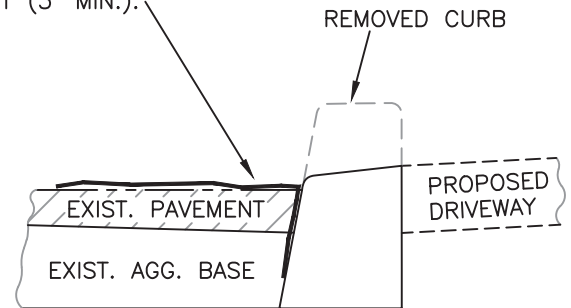


SECTION "A-A"



ALTERNATE STRAIGHT CURB KNOCK-OUT
(WITH APPROVAL OF THE ENGINEER)

USE 6 MIL PLASTIC TO TEMPORARILY PROTECT EXISTING PAVEMENT DURING CONCRETE PLACEMENT (3' MIN.).



SECTION "B-B"

NOTES:

1. SAWCUT THROUGH GUTTER BAR SHALL BE MADE AS CLOSE AS POSSIBLE TO THE EXISTING FACE OF CURB.
2. WHEN STRAIGHT CURB IS TO BE REPLACED, REMOVE AND REPLACE PAVEMENT WITH APPROVAL OF THE ENGINEER.
3. WHEN EXISTING GUTTER SLOPE EXCEEDS 5.0% ON SIDEWALK ACCESS RAMPS, REMOVE AND REPLACE ENTIRE CURB AND GUTTER SECTION. COMPLETE CURB AND GUTTER REMOVAL FOR GUTTER SLOPES LESS THAN 5.0% REQUIRES APPROVAL OF THE ENGINEER.
4. MINIMUM LENGTH OF CURB SECTION TO BE REPLACED IS 5.0 FEET OR AS DIRECTED BY THE ENGINEER.
4. SEE SUPP. STD. DWGS. 00700-07, 00700-08 AND 00700-10 FOR SIDEWALK AND DRIVEWAY ACCESS DETAILS.

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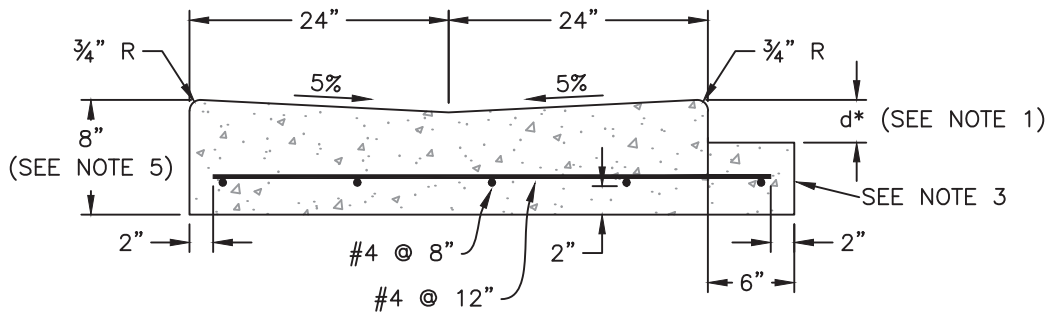
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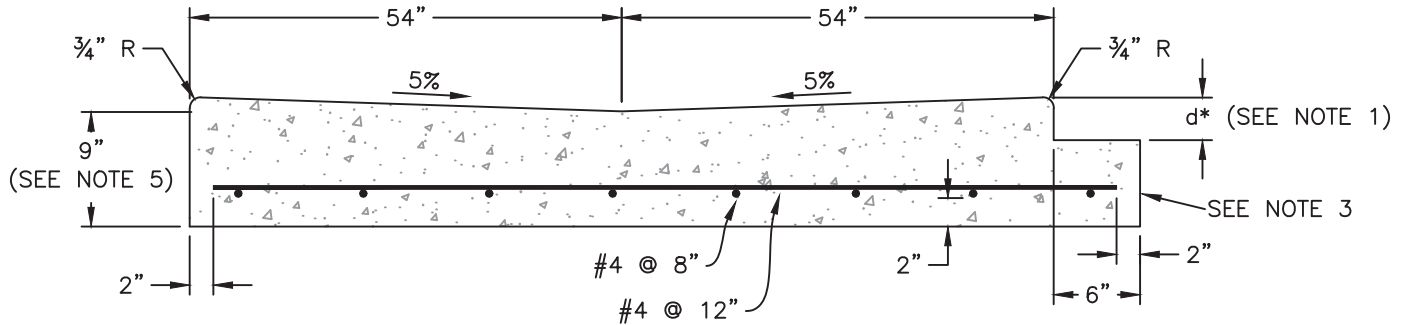
**CURB
KNOCK-OUTS**

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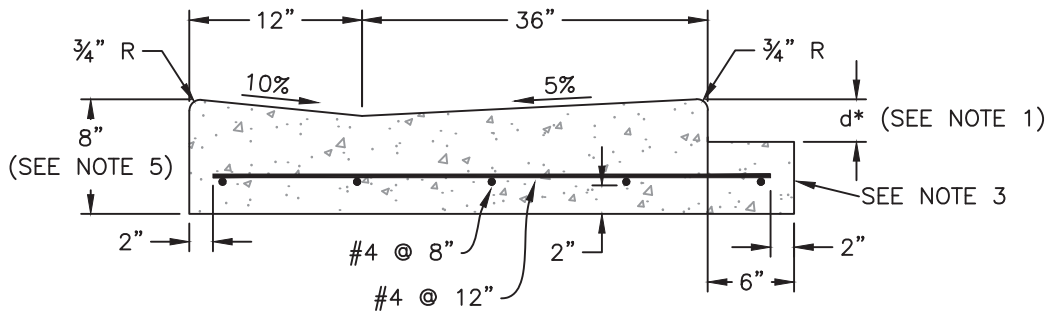
00700-04



SYMMETRICAL "V" TYPE GUTTER



WIDE VALLEY GUTTER



NON-SYMMETRICAL "V" GUTTER

NOTES:

1. d* = THICKNESS OF A.C.P. WEARING COURSE.
2. STANDARD P.C.C. SHALL HAVE A 28 DAY COMPRESSIVE STRENGTH OF 4000 P.S.I. (MIN.).
3. CONSTRUCT 6" BENCHES MONOLITHICALLY WITH THE VALLEY GUTTER TO MATCH DEPTH OF A.C.P. WEARING COURSE ("d").
4. WHERE BENCHING IS NOT REQUIRED, CONSTRUCT A 1" BATTER ON VERTICAL FACES ABUTTING A.C. PAVEMENT.
5. PLACE APPROVED PRE-MOLDED FILLER AGAINST VERTICAL FACES ABUTTING P.C.C. PAVEMENT.
6. ALL VALLEY GUTTER INSTALLATIONS REQUIRE A MINIMUM OF 2.0" COMPACTED AGGREGATE BASE.

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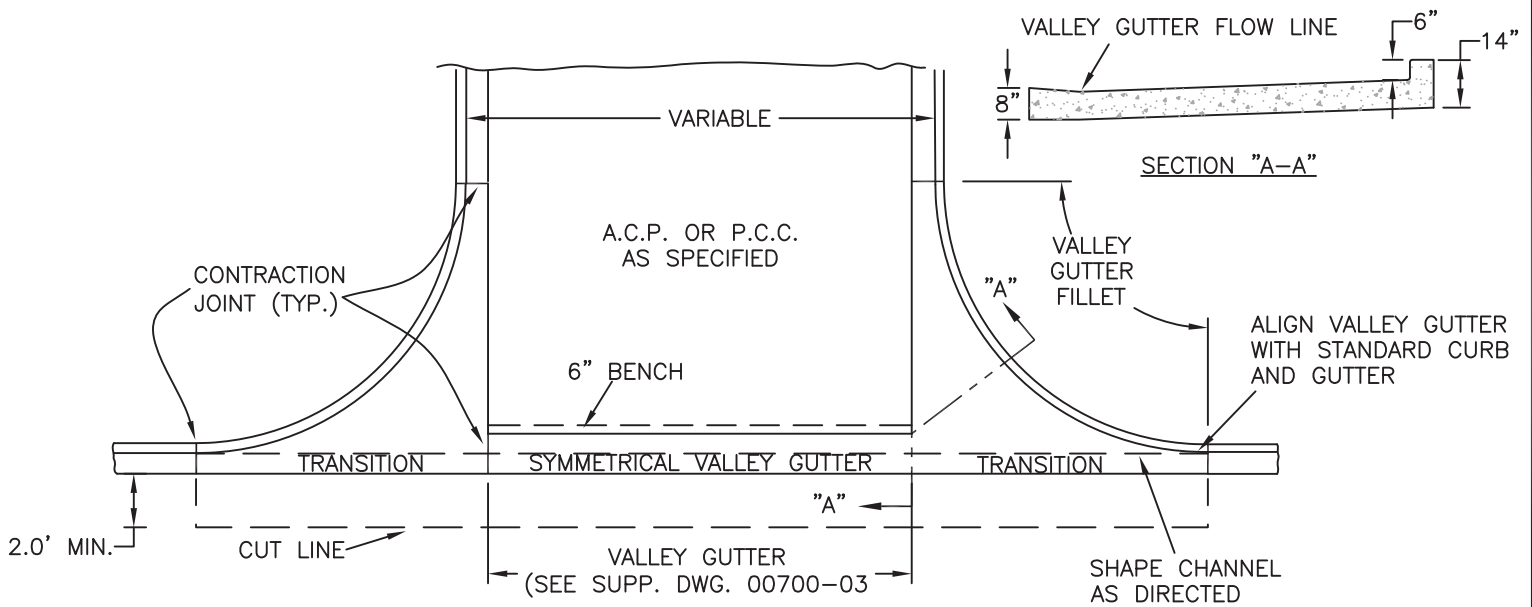
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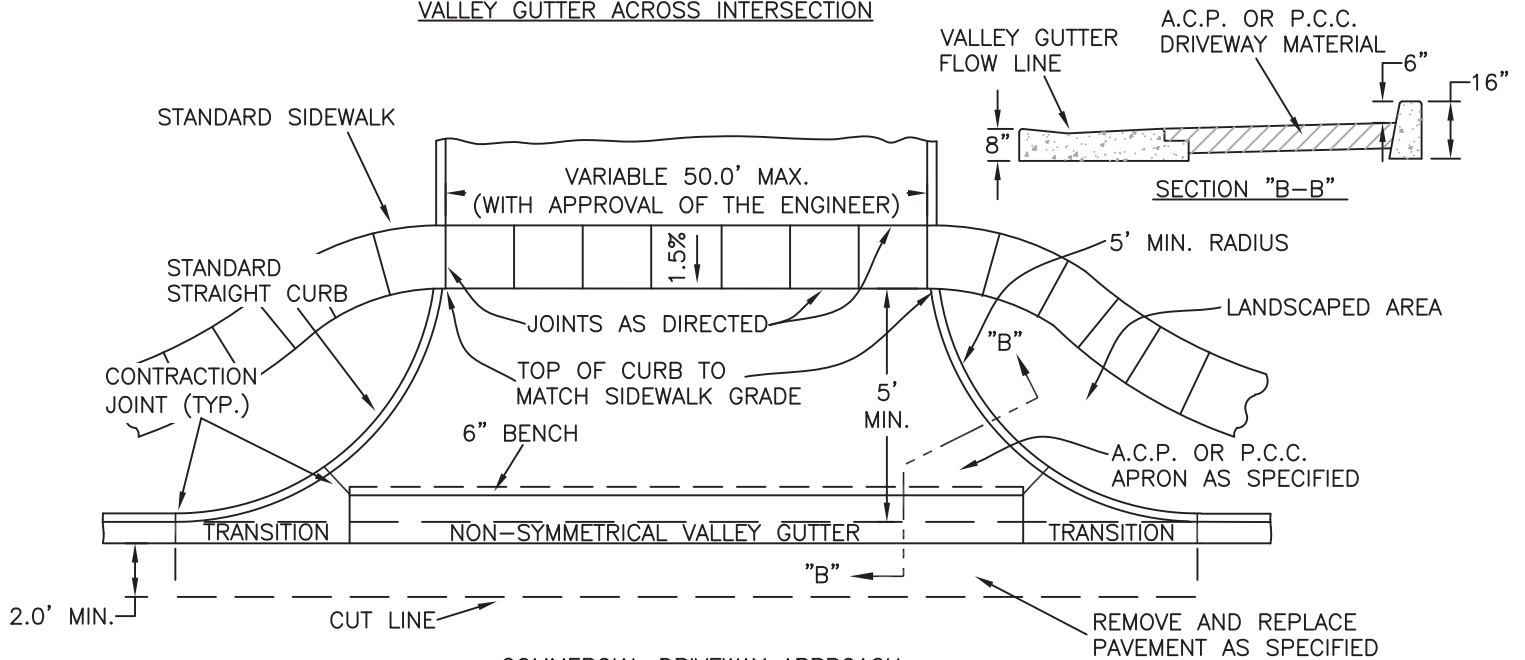
P.C.C. VALLEY
GUTTER

DRAWING NO:

00700-05



VALLEY GUTTER ACROSS INTERSECTION



COMMERCIAL DRIVEWAY APPROACH

NOTES:

1. COMMERCIAL DRIVEWAY ORDER OF CONSTRUCTION:
 - A. CONSTRUCT P.C.C. VALLEY GUTTER AND TRANSITION SECTIONS. TRANSITION SECTIONS SHALL MATCH MAXIMUM THICKNESS OF VALLEY GUTTER SECTION (SEE SUPP. STD. DWG. 00705).
 - B. CONSTRUCT 8" THICK P.C.C. SIDEWALK ACROSS DRIVEWAY AREA.
 - C. CONSTRUCT DRIVEWAY APRON. P.C.C. APRONS SHALL BE JOINTED AS DIRECTED (SUPP. STD. DWG. 00700-02).
2. STANDARD P.C.C. SHALL HAVE 28 DAY COMPRESSIVE STRENGTH OF 4000 P.S.I. (MIN.).
3. CONSTRUCT CURBING MONOLITHICALLY WITH FILLET AND TRANSITION SECTIONS.
4. SEE STANDARD SIDEWALK ACCESS RAMP DETAILS (SUPP. STD. DWG. 00700-08) FOR ACCESS RAMP REQUIREMENTS AT INTERSECTIONS.

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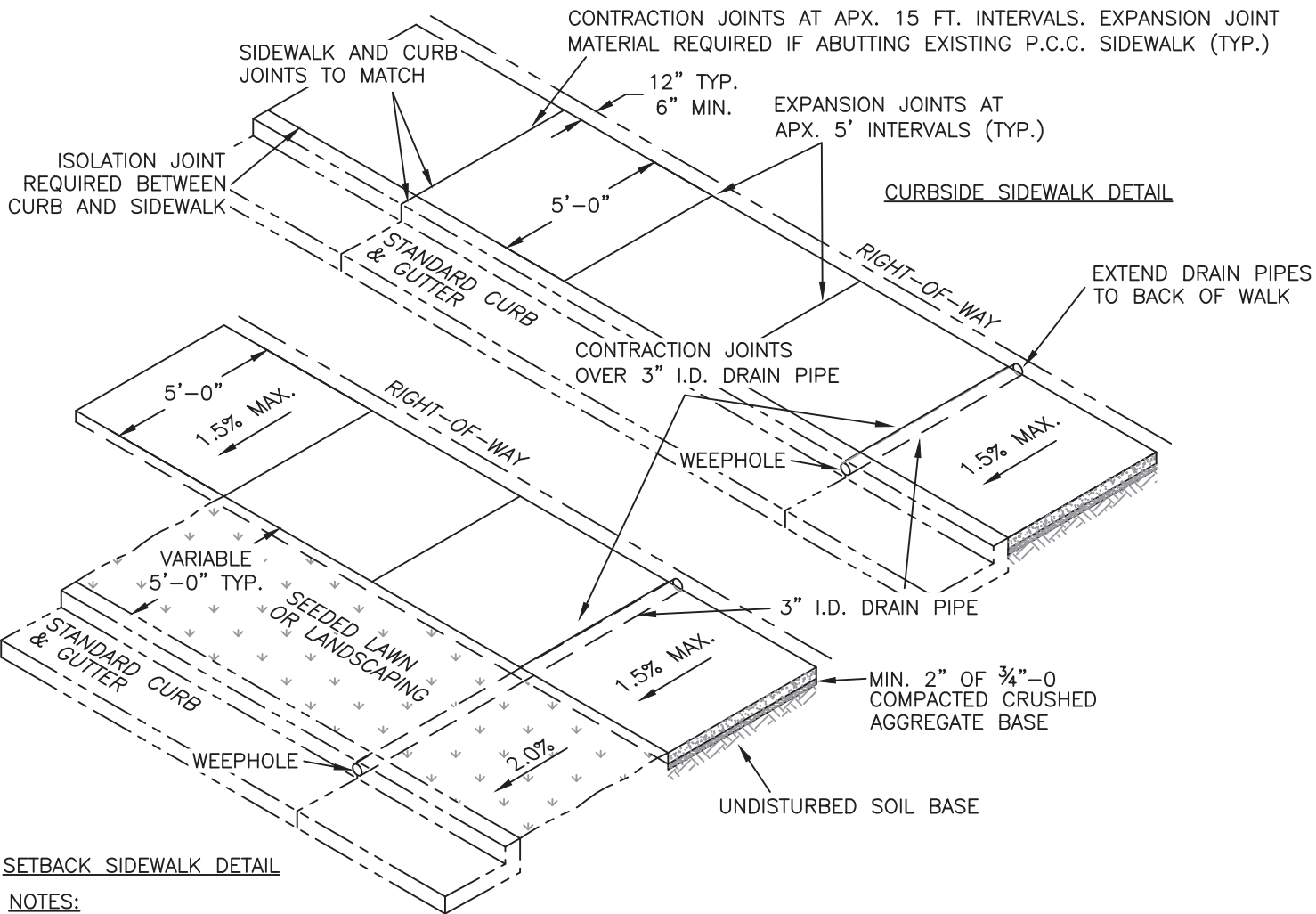
December, 2021

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VALLEY GUTTERS
ACROSS DRIVEWAYS
& INTERSECTIONS

DRAWING NO:

00700-06



SETBACK SIDEWALK DETAIL

NOTES:

1. SIDEWALK CROSS SLOPE SHALL NOT EXCEED 1.5%. WHEN ADJOINING LOT IS BELOW THE TOP OF CURB AND SLOPES AWAY FROM THE CURB, A NEGATIVE (AWAY FROM STREET) 1.5% SLOPE MAY BE REQUIRED.
2. SIDEWALK NOMINAL DEPTH IS 4.0"; P.C.C. SHALL HAVE A 28 DAY COMPRESSIVE STRENGTH OF 3000 P.S.I. (MIN.).
3. UNDER NO CIRCUMSTANCES SHALL SIDEWALK WIDTH BE LESS THAN 5.0' FOR MORE THAN 200 LINEAL FEET.
4. FULL DEPTH EXPANSION JOINTS WITH PRE-MOLDED FILLER SHALL BE INSTALLED BETWEEN DRIVEWAYS AND SIDEWALKS, AROUND ALL SIDEWALK OBSTRUCTIONS, AT POINTS OF TANGENCY, AT ENDS OF EACH DRIVEWAY, AND AT 200' MAX. SPACING (SEE SUPP. STD. DWGS. 00700-09 & 00700-10).
5. ISOLATION JOINTS (FULL DEPTH) SHALL BE INSTALLED BETWEEN BACK OF CURB AND FRONT OF SIDEWALK.
6. CONTRACTION JOINTS (1/3 DEPTH OF CONCRETE) SHALL BE INSTALLED AT 15' INTERVALS.
7. TOOLED JOINTS (3/4" DEPTH) SHALL BE INSTALLED AT APX. 5' INTERVALS IN UNCURED CONCRETE WITH A JOINTER TOOL. TOOLED JOINTS SHALL MATCH CURB CONTRACTION JOINTS WHERE POSSIBLE. SIDEWALKS 8' AND WIDER SHALL HAVE A LONGITUDINAL TOOLED JOINT AT MIDPOINT. (SEE SUPP. STD. DWG. 00700-02 FOR JOINT DETAILS).
8. WEEP HOLES AND PIPING SHALL EXTEND TO BACK OF SIDEWALK AND REQUIRE A COUPLER AT R.O.W. TERMINATION.
9. SIDEWALK SECTIONS MUST BE REPLACED AS FULL PANELS. PARTIAL REMOVAL/REPLACEMENT OF PANELS IS PROHIBITED.

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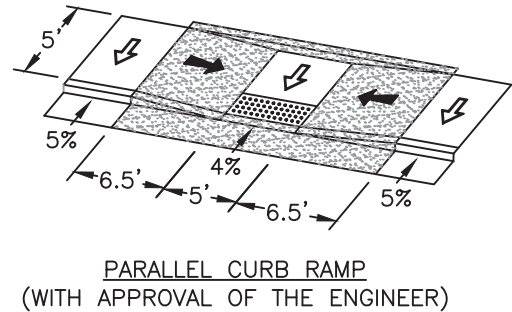
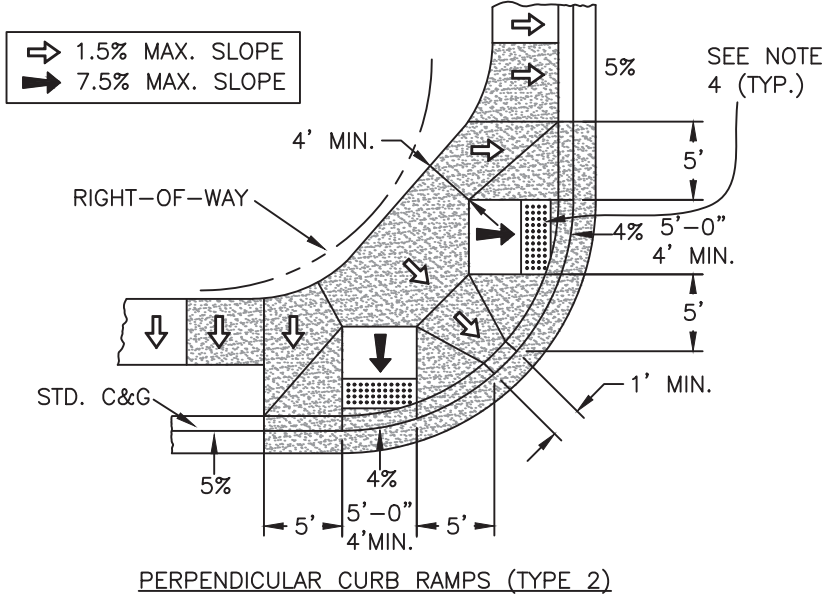
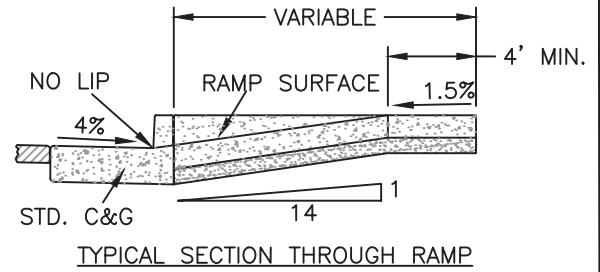
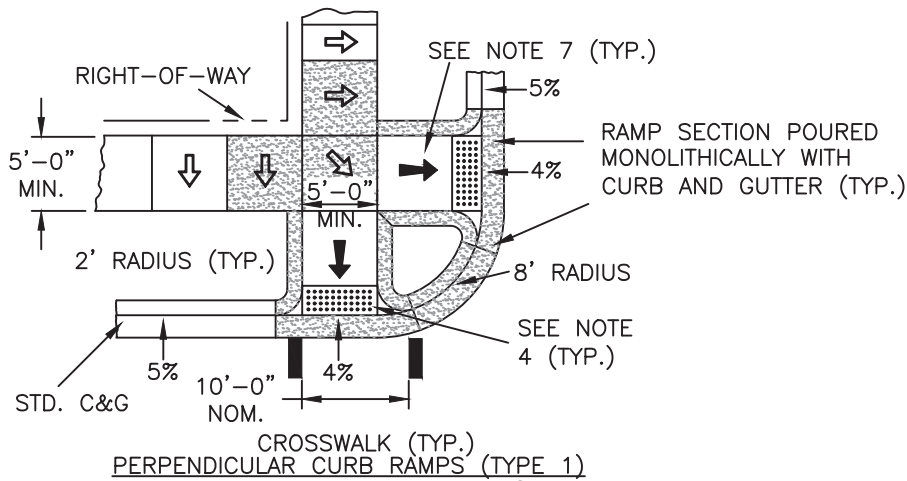
CITY ENGINEER

December, 2021

DATE

**STANDARD P.C.C.
SIDEWALK AND
JOINTING**

DRAWING NO: **00700-07**



NOTES:

1. THE AMERICANS WITH DISABILITIES ACT (A.D.A.) REQUIRES THAT ALL ACCESS RAMPS TO SIDEWALKS CONFORM TO APPLICABLE FEDERAL GUIDELINES.
2. UTILITY POLES ARE NOT PERMITTED WITHIN RAMPS OR LANDING AREAS.
3. 1.5% (MAX.) 20 SF MIN. TURN SPACE SHALL BE PLACED AT THE JUNCTION OF EACH RAMP. GUTTER PANS SHALL NOT EXCEED 4.0% SLOPE THROUGH RAMP THROATS.
4. APPROVED SAFETY YELLOW TRUNCATED DOME DETECTABLE WARNING SURFACES SHALL BE USED WHEREVER RAMP ACCESSES MEET THE STREET (SEE ODOT STD. DWG. RD902).
5. RAMPS FOR BIKEWAYS SHALL EXTEND FULL WIDTH OF THE BIKEWAY (8' WIDTH TYP.).
6. SIDEWALK ACCESS RAMP SLOPES MAY ONLY BE REDUCED FROM 1:14 (MAX.) TO 1:8 (MAX.) WHEN MODIFYING EXISTING FACILITIES, WITH APPROVAL OF THE ENGINEER.
7. RAMP THROATS MUST BE PERPENDICULAR TO THE STREET AND IN LINE WITH THE OPPOSITE ACCESS RAMP.
8. WHEN INSTALLING RAMPS ADJACENT TO AN EXISTING ROADWAY, REMOVE AND REPLACE EXISTING PAVEMENT AS REQUIRED (2.0' MIN.; SEE SUPP. STD. DWG. 00400-02).

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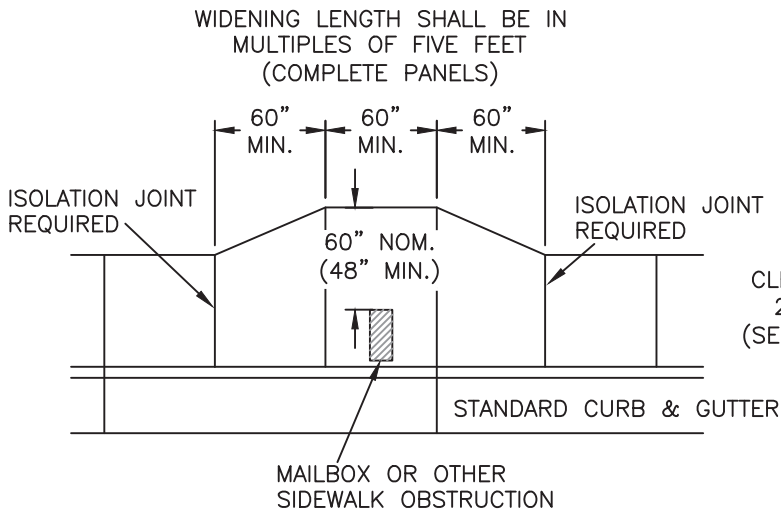
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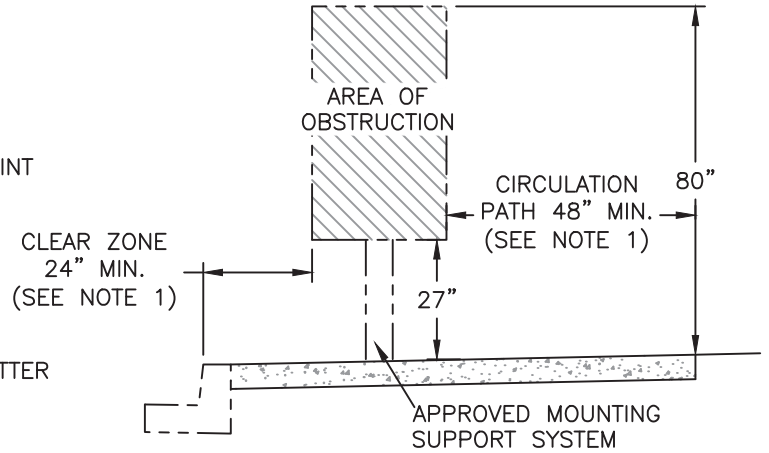
**A.D.A. SIDEWALK
ACCESS RAMPS**

DRAWING NO:

00700-08

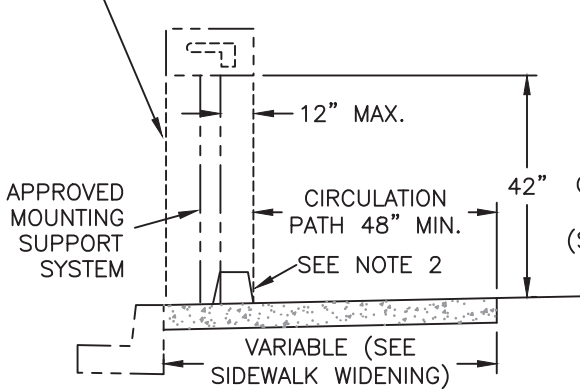


SIDEWALK WIDENING AROUND OBSTRUCTIONS

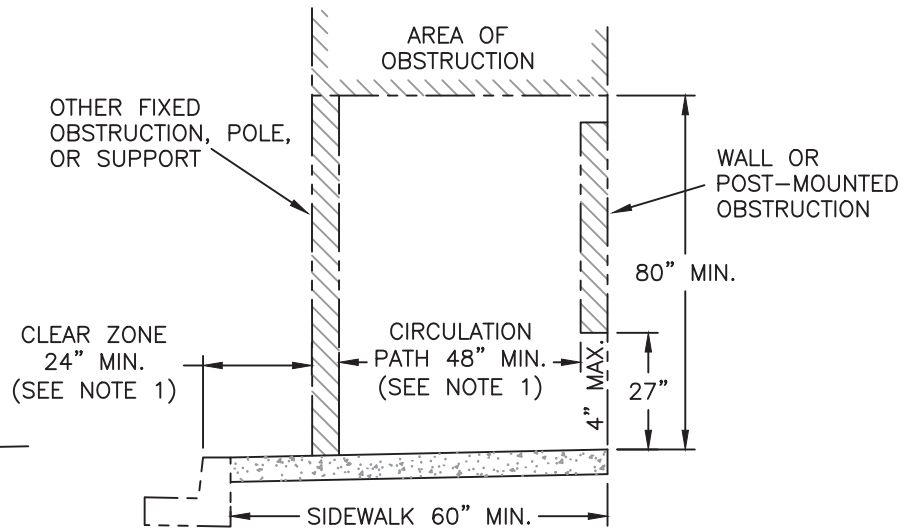


POST MOUNTED OBSTRUCTION WITHIN SIDEWALK

PLACE FRONT EDGE OF BOX ALIGNED VERTICALLY WITH THE BACK OF CURB, WITH THE EXCEPTION OF CUL-DE-SACS (SEE NOTE 5)



TYPICAL MAILBOX INSTALLATION



PROTRUDING OBJECTS AND OTHER OBSTRUCTIONS

NOTES:

1. THE CLEAR ZONE AND CIRCULATION PATH MAY BE COMBINED, PROVIDED A 48" MINIMUM SIDEWALK WIDTH IS MAINTAINED.
2. DEFLECT SIDEWALKS AROUND AREAS OF OVERHANGING OBSTRUCTIONS WHEN OVERHANGS EXCEED ALLOWABLE LIMITS. CURBING MAY BE PLACED ON THE SIDEWALK AS AN ALTERNATIVE TO DEFLECTION WITH APPROVAL OF THE ENGINEER (SEE SUPP. STD. DWG. 00700-03).
3. DETAILS SHOWN HERE SHOW TYPICAL CONFIGURATIONS; ALL OBSTRUCTIONS AND PROTRUDING OBJECTS WITHIN THE SIDEWALK AREA MUST BE APPROVED BY THE ENGINEER.
4. INSTALL FULL-DEPTH EXPANSION JOINT MATERIAL AROUND ALL SIDEWALK OBSTRUCTIONS.
5. IN CUL-DE-SACS, PLACE THE FRONT EDGE OF MAILBOXES 6" BEHIND THE BACK OF CURB.

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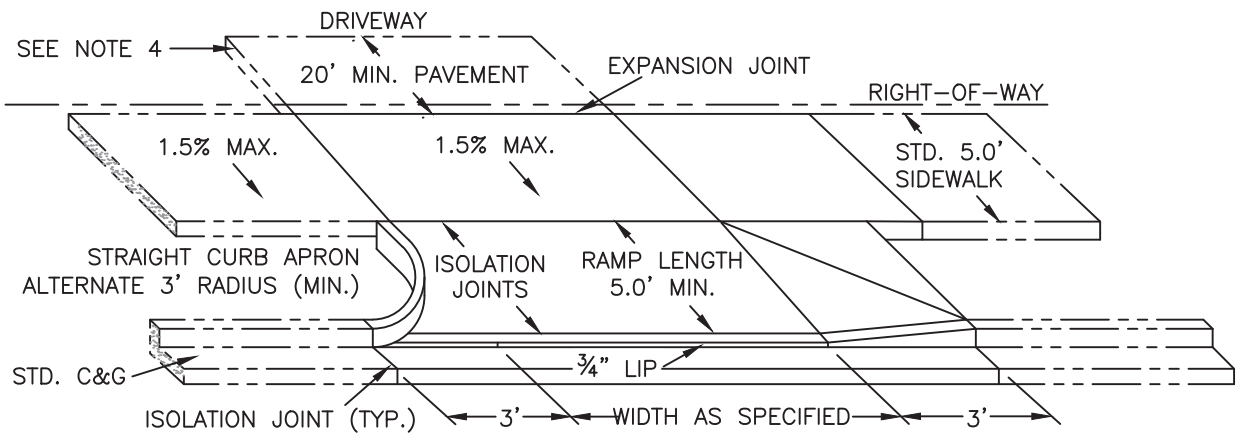
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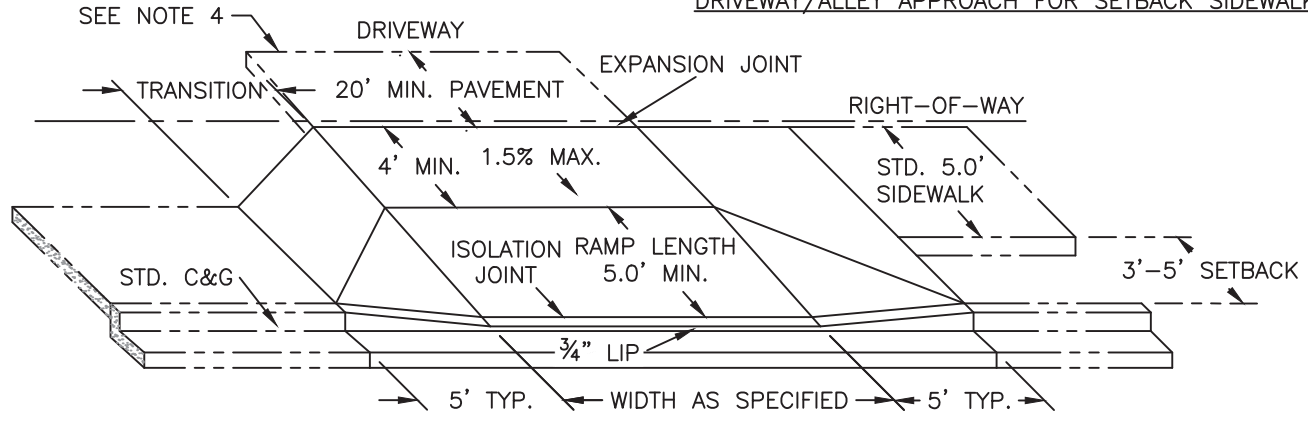
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SIDEWALK OBSTRUCTIONS

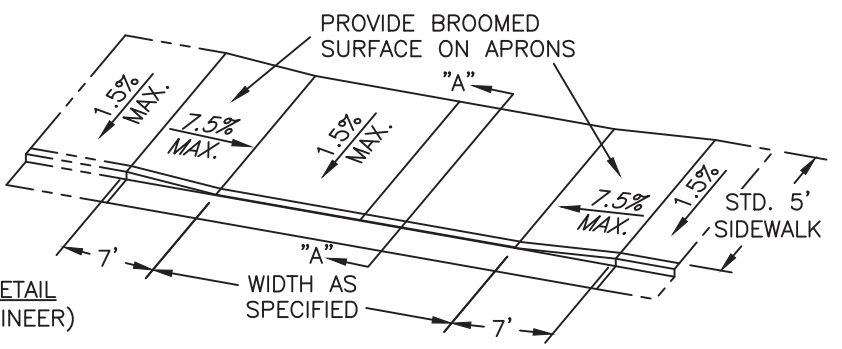
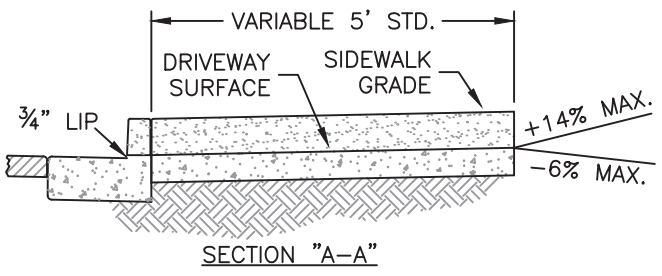
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DRIVEWAY/ALLEY APPROACH FOR SETBACK SIDEWALK



DRIVEWAY/ALLEY APPROACH FOR CURBLINE & PARTIALLY SETBACK SIDEWALKS



NOTES:

ALTERNATE DRIVEWAY DETAIL (WITH APPROVAL OF ENGINEER)

1. ALL RESIDENTIAL DRIVEWAYS AND INCLUDED SIDEWALKS, TRANSITIONS AND WINGS SHALL HAVE A NOMINAL P.C.C. THICKNESS OF 6.0" AND A 28 DAY COMPRESSIVE STRENGTH OF 3000 P.S.I. (MIN.). ALLEY APPROACHES AND COMMERCIAL DRIVEWAYS, INCLUDING TRANSITIONS AND WINGS, SHALL HAVE A NOMINAL P.C.C. THICKNESS OF 8.0" AND A 28 DAY COMPRESSIVE STRENGTH OF 4000 P.S.I. (MIN.).
2. CONSTRUCT ONE OR MORE TOOLED JOINTS (SEE SUPP. STD. DWG. 00700-07).
3. IF THE APPROACH OR DRIVEWAY IS TO BE USED AS A SIDEWALK ACCESS, THE 3/4" LIP SHALL BE OMITTED. (SEE SUPP. STD. DWG. 00700-08).
4. APPROACHES SHALL BE PAVED A MINIMUM OF 20' FROM BACK OF WALK PER ENGINEERING DESIGN STANDARDS.
5. WHEN INSTALLING APPROACHES ADJACENT TO AN EXISTING ROADWAY, REMOVE AND REPLACE EXISTING PAVEMENT AS REQUIRED (2.0' MIN.; SEE SUPP. STD. DWG. 00400-02).

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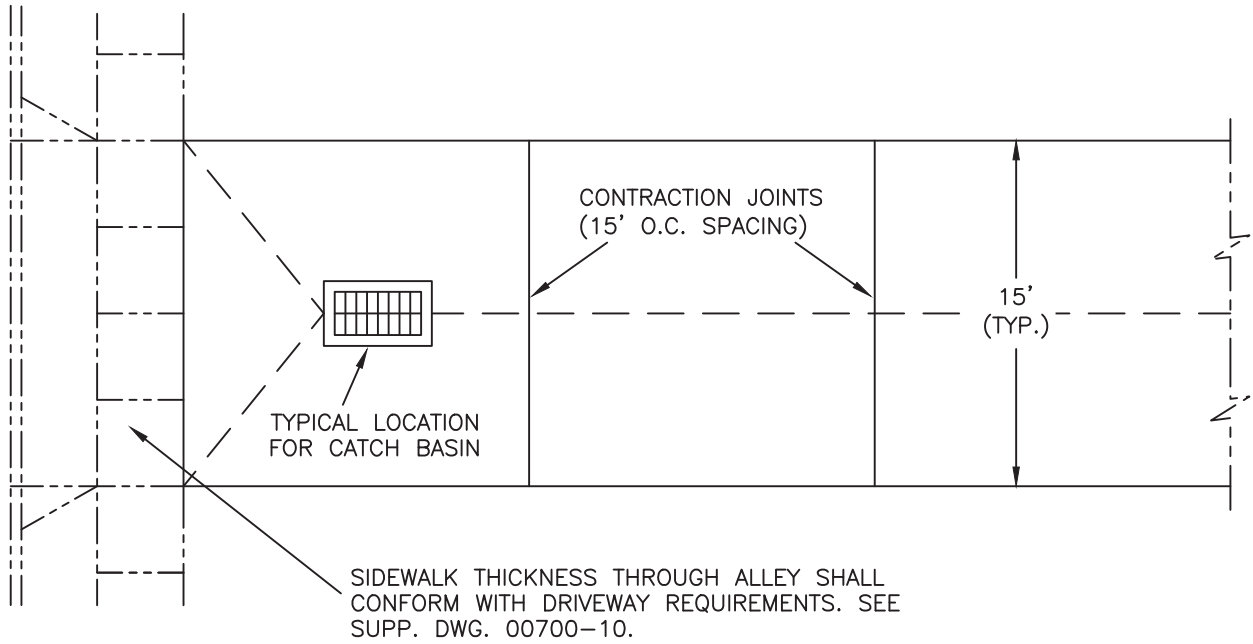
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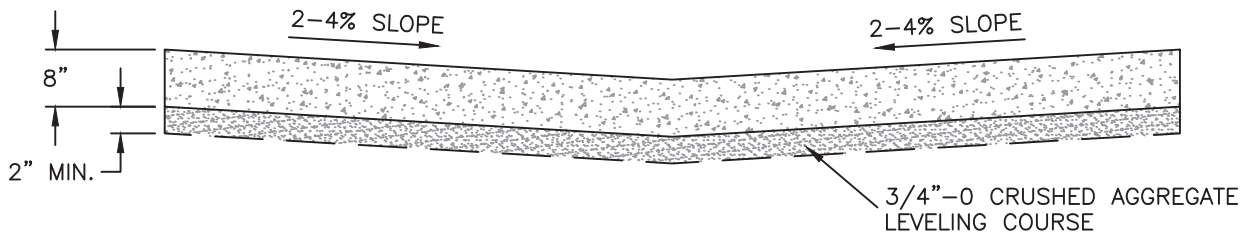
TYPICAL DRIVEWAY AND ALLEY APPROACHES

DRAWING NO:

00700-10



TYPICAL ALLEY (PLAN VIEW)



TYPICAL ALLEY SECTION (INVERTED CROWN)

NOTES:

1. ALL EDGES SHALL BE TOOLED WITH 3/4" RADIUS.
2. ALL P.C.C. SHALL HAVE A 28 DAY COMPRESSIVE STRENGTH OF 4000 P.S.I. (MIN.).
3. SEE SUPP. STD. DWG. 00700-10 FOR APPROACH DETAILS..

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TYPICAL ALLEY
SECTION

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00700-11