



REVISED 11/7/2023

LEBANON CITY COUNCIL AGENDA

November 8, 2023

6:00 PM Regular Session

**Santiam Travel Station
750 3rd Street, Lebanon, Oregon**

Mayor Kenneth E. Jackola

*Council President Michelle Steinhebel Councilor Wayne Dykstra Councilor Carl Mann
Councilor Jeremy Salvage Councilor Kim Ullfers Councilor Dave Workman*

MISSION STATEMENT

The City of Lebanon is dedicated to providing exceptional services and opportunities that enhance the quality of life for present and future members of the community.

CALL TO ORDER / FLAG SALUTE

ROLL CALL

CONSENT CALENDAR *The following item(s) are considered routine and will be enacted by one motion. There will not be a separate discussion of these items unless a Councilor so requests. In this case, the item(s) will be removed from the Consent Calendar and considered separately.*

AGENDA	Lebanon City Council Agenda – November 8, 2023
BOARD MINUTES	Library Advisory Committee – August 8, 2023 Parks, Trees & Trails Advisory Committee – July 19, 2023 Planning Commission – September 20, 2023 Senior & Disabled Services Advisory Committee – August 16, 2023
COUNCIL MINUTES	October 11, 2023 Work & Regular Sessions

PUBLIC COMMENTS *Citizens may address the Council by filling out a testimony/comment card prior to speaking and handing it to the City Recorder. Each citizen is provided up to 5 minutes to provide comments to the Council. The Council may take an additional two minutes to ask clarifying questions. The City Recorder will accept and distribute written comments at a speaker's request. Public comments can also be submitted by email to city.recorder@lebanonoregon.gov prior to 5:00 PM on November 8. The City Recorder will distribute comments to the Mayor and Council prior to the meeting.*

PRESENTATIONS

- Annual Risk Report – Darrin Godfrey, HUB International Northwest LLC
- Republic Services – Julie Jackson
- Lebanon Chamber of Commerce – Rebecca Grizzle

PUBLIC HEARING

1) Comprehensive Plan Map Amendment & Zone Map Amendment (CPMA-23-03 & ZMA-23-03)

Presented by: Kelly Hart, Community Development Director

Approval/Denial by ORDINANCE BILL NO. 2023-20, ORDINANCE NO. 3015

REGULAR SESSION

2) Repealing Resolution No. 2022-28 and Approving Republic Services Rate Increases

Presented by: Ron Whitlatch, Interim City Manager

Approval/Denial by RESOLUTION NO. 2023-18

3) Lebanon Chamber of Commerce Tourism Agreement

Presented by: Ron Whitlatch, Interim City Manager

Approval/Denial by MOTION

4) Accessory Dwelling Unit Systems Development Fees

Presented by: Kelly Hart, Community Development Director

Approval/Denial by ORDINANCE BILL NO. 2023-19, ORDINANCE NO. 3014

5) Fluoride Discussion

Presented by: Jason Williams, Public Works Director

DISCUSSION

6) City Manager's Report

Presented by: Ron Whitlatch, Interim City Manager

DISCUSSION

ITEMS FROM COUNCIL

PUBLIC/PRESS COMMENTS *An opportunity for citizens and the press to comment on items of city business.*

NEXT SCHEDULED COUNCIL MEETING

- December 13, 2023 Regular Session (6:00 PM) & Executive Session

EXECUTIVE SESSIONS *Executive Sessions are closed to the public due to the highly confidential nature of the subject. It is unlawful to discuss anything outside of the Executive Session. Final action/decisions are to be made in open session.*

- *Per ORS 192.660(2)(e) To conduct deliberations with persons designated by the governing body to negotiate real property transactions.*
- *Per ORS 192.660(2)(a) To consider the employment of a public officer, employee, staff member or individual agent. (City Manager)*

REGULAR SESSION *Council may reconvene into open session should action be required.*

ADJOURNMENT

If you cannot attend the meeting, but wish to address the Council under Public Comments or for a Public Hearing electronically, click [HERE](#) to register in advance for the meeting. You will receive a confirmation email containing information about joining the meeting. Attendees will need to register to receive the link to the meeting. Please register **ONLY** if you wish to address the Council. You will be called in the order received. If you want to watch or listen to the meeting, please click this link to do so on YouTube: <https://youtube.com/live/MrRxKLxMvjQ?feature=share>

City Council meetings are recorded and available on the City's YouTube page at <https://www.youtube.com/user/CityofLebanonOR> The meeting location is accessible to persons with disabilities. A request for an interpreter for the hearing impaired or for other accommodations for persons with disabilities should be made at least 48 hours before the meeting to the City Recorder at 541.258.4905.

Consent Calendar

Board/Committee
Commission Minutes



Minutes
Lebanon Public Library
Advisory Group Meeting
August 8, 2023

The meeting was called to order at 4:25 pm.

Present: Denice Lee (Chairperson), Jane Turner, Russ Fish, Vandy Roadifer (Friends representative), Kendra Antila (Director), Wayne Dykstra (Council Representative)

Minutes of the April meeting were approved as presented.

Library Director's Report: Kendra shared annual statistics for FY22-23 as well as monthly statistics for July. Last year's numbers show that we are back to pre-pandemic numbers for borrowing, site visits, etc. Staff plan to use grant funds to purchase books for all ages for a Spanish collection. There are also plans to seek grant funding for an EV charging station. The capital campaign for the children's meeting room has stalled until the online donation portal becomes available. Recent complaints made to Council regarding LGBTQ+ titles in the children's and young adult collection were also discussed.

Friends of the Library Report: The Friends have changed their fiscal year to match the Library (July – June). They raised \$26,379.80 last year. They're exploring new revenue streams, including an online RedBubble store. Due to space constraints, there is a moratorium on donations until after the August 19th and September 2nd book sales.

New Business: Members were given multiple applications for 3 open positions on the committee. All agreed to send their top 3 choices to Kendra by Monday so the new members can be officially appointed at the September Council meeting.

Unfinished Business: Review/amend child safety and behavior policy, specifically item #4 regarding unattended children at closing time. This will be resumed at the October meeting.

Public Comments: None

Announcements: None

The meeting was adjourned at 4:51 pm. The next meeting will be October 10, 2023 @ 4:30.

The meeting location is accessible to persons with disabilities. A request for an interpreter for the hearing impaired or for other accommodations for persons with disabilities should be made at least 48 hours before the meeting to Kendra Antila at (541) 258-4232.



Parks, Trees, and Trails Advisory Committee Meeting Minutes July 19, 2023

Committee Present Jan Diamantine, City Councilor, Dave Workman, Linda Ziedrich, Rick Barnett, Cindy Kerby, Anna Creel, and Rodney Sell.

Committee Absent None.

Staff Present Interim City Manager Ron Whitlatch, and Public Works Operations Manager, Jason Rush.

Call to Order Chair Barnett called the meeting to order at 2:00 p.m. in the Santiam Travel Station Board Room.

Approval of Minutes The committee approved the meeting minutes from June 21, 2023.

Discussion

- City of Lebanon Update

Jason Rush provided the City of Lebanon update:

- Completed a tree removal at 213 E Grant Street, no sidewalk has been damaged so only the tree will be removed. The replacement of these trees are the responsibility of the property owner.
- Two birch trees on Mountain River, no sidewalk has been damaged so only the trees were removed. Unknown if these birch trees were affected by the previously discussed Birch Borer. The replacement of these trees will not be in the park strip and are the responsibility of property owner.
- The Lebanon Skate Park cameras have been installed and are functional. The additional fencing is currently being fabricated, once completed it will be powder coated and installation will be scheduled.
- Our new contract with Valley Merchant Police to lock the park restrooms at night has been going smoothly.
- Seasonal Trail Worker, we have had a tough time filling this position due to qualifications and position requirements, so this position has not been filled at this time.
- The city is reviewing final plans for the East side trail at Cheadle Lake and developing a cost estimate for the project. This project is expected to go out for bid this fall.

- Planning Case - S-22-04

BLT receives planning notices to ensure development lines up with the master plan. The current case S-22-04 is an area in which BLT believes trail number 10 is impacted, Sell

requested additional information on what planning or engineering has decided for this site. Whitlatch stated this planning case has no trail conditions based on the development utilizing the majority of the property for housing and the remainder is wetland and would not be suitable for a trail, there is also no direct connection to trail number 10.

- Cheadle Lake Plan Review

An overall conceptual plan was previously provided to the committee. The 2007 Cheadle Lake Master Plan has also been provided.

Kerby, after reviewing the conceptual plan, has concerns about the use of the space. Concerns such as what is the best use of this park, not only for the community but for the city looking forward to funding and maintenance. Originally it was for Strawberry Festival and an event venue, but the conceptual plan has now turned into a sports complex. Kerby asked if this conceptual plan be reconfigured to fit better and include what has been proposed but also include more of what the citizens might be requesting.

Whitlatch stated there is no timeline on spending of funds for this grant so there is time for this committee to make different recommendations regarding the plan. The first phase of construction will not be impacted by design changes. Any recommended changes to design the committee would take to the City Council for approval.

The committee can request Udell Engineering attend a meeting and provide them with ideas for additional conceptual plans.

Barnett suggested the committee brainstorm ideas before the next meeting and bring those to table before bringing in Udell Engineering or taking to the public for suggestions.

Ziedrich would like a full presentation of what is being presented and why to better understand the development, she believes this would be beneficial for the public as well.

Whitlatch suggested looking at the options and what revenue potential they could have to offer self-sustainability.

September / October meeting goals: members bring ideas that are inclusive of the park being self-sustainable, 20 – year plan, community need / want, and meet the grant intent of access.

- Trails Master Plan and Parks Master Plan Update

Sell would like to see an update of the 2006 Parks Master Plan and the 2009 Trails Master Plan to assist with grant opportunities.

Whitlatch stated there is no funding for Master Plan updates and this would be a lot for staff to try and overtake. Previously our RARE intern was going to start this project, but other projects did not allow time. A suggestion would be for the Parks, Trees, and Trails Committee to submit a recommendation to City Council requesting an update be completed and this would allow for council to review funding options for this project.

BLT could investigate grants or funding options to assist with this update.

Motion to City Council:

The Parks, Trees, and Trails Committee recommends to City Council that the Parks Master Plan and Trails Master Plans be revised, as funding is available a request is made for City

Council to review options for funding these updates.

Cindy Kerby seconded to recommend the motion to the City Council. The motion passed unanimously.

- BLT Update – Rod / Jan
 1. Beaton Lane Update – Rush stated there is new development taking place on Beaton Lane and there is a possibility a new sewer main installation will be necessary; we do not want to complete the lights until we know more.
 2. Gills Landing Project – BLT has provided funds to the project and is researching additional funding. Construction is anticipated to begin in 2024. BLT would like PTT committee support that this is a necessary project. Motion by Cindy Kerby from the Parks, Trees, and Trails Committee for Build Lebanon Trails to move forward and apply for the grants to allow this project to happen, the motion passed unanimously. Build Lebanon Trails welcomes letters of support from the committee or members.
 3. River Park to Bromil (Trail #4, Section2) – BLT would like the PTT Committee to make a recommendation of support on this project as outlined in the Trails Master Plan, before moving forward with hiring a surveyor. Motion by Cindy Kerby from the Parks, Trees, and Trails Committee for Build Lebanon Trails to move forward with this project as outlined in the Trails Master Plan, the motion passed unanimously.
- General Discussion

Ziedrich asked if there is a schedule for placing play equipment at Porter Park, City Staff stated there was no plan for equipment to be installed. Funding can be looked for to obtain equipment.

Next Scheduled Meeting

September 20, 2023 (2:00 – 3:30 p.m.)

Adjournment

Barnett Adjourned the meeting at 3:30 p.m.



City of Lebanon
Planning Commission
Meeting Minutes
September 20, 2023

Members Present: Chair Don Robertson, Vice-Chair Gerig-Knurowski, Commissioners Dave McClain, Tina Breshears, Karisten Baxter, Marcellus Angellford, Mike Miller and Don Fountain.

Staff Present: Community Development Director Kelly Hart, and City Attorney Tre Kennedy.

1. CALL TO ORDER/FLAG SALUTE

Chairman Robertson called the meeting of the Lebanon Planning Commission to order at 6:00pm at the Santiam Travel Station. The meeting was also provided on Zoom for a virtual platform, and live streamed on YouTube.

Chair Robertson announced that he will likely be missing the October and November meetings, therefore is having Vice-Chair Gerig-Knurowski lead the meeting.

2. ROLL CALL

Roll call was taken. Commissioners Malloy was excused.

3. APPROVAL OF MEETING MINUTES

July 19, 2023 – approved as written

August 16, 2023 – approved as written

4. CITIZEN COMMENTS – None

5. COMMISSION REVIEW

A. Planning File CPMA-23-02, ZMA-23-02, MLP-23-04 – A Partition to separate 1.33 acres from the 5.68 acre parcel, and to change the comprehensive plan and zoning map designation from Mixed Use to Highway Commercial. (12S 02W 23B, Tax Lot 100)

City Attorney Kennedy read the hearing procedures into the record. Vice-Chair Gerig-Knurowski asked whether any commissioners had a conflict of interest, bias, or ex-

parte communication. Seeing none, Vice-Chair Gerig-Knurowski opened the public hearing and asked for staff to provide a report.

Director Hart provided a summary of the report, the overall decision criteria and the proposed actions to be taken by the Planning Commission.

Vice-Chair Gerig-Knurowski asked if there were any comments or questions from the commission. Seeing none, the public hearing was opened for comment.

Laura LaRoque, Udell Engineering and Raul Woerner, CSA Planning spoke as applicant representatives. Laura LaRoque identified herself as available for questions. Raul Woerner identified a general overview of the project and supporting of the findings written and indicated a question on the conditions.

Vice-Chair Gerig-Knurowski opened the hearing to the public to speak in favor or against the project. Seeing none, Raul Woerner was offered opportunity to ask question of the condition.

Raul Woerner identified that the condition regarding the irrevocable petition requirement was confusing and requested clarification of the condition as part of the final order. Staff and commission discussed the purpose of the condition and proposed clarifying language as part of a motion.

After the discussion, the public hearing was closed. All discussion was left to the commission and staff.

Chair Robertson asked staff to explain the purpose of the irrevocable petition. Director Hart identified that the irrevocable petition was to ensure the property owners participated in the future required land dedication associated with the extension and build out of Burdell Blvd. The design for Burdell Blvd. is not yet complete, so it is not possible to identify the exact amount of land required at this point to clearly condition. Therefore, the irrevocable petition process is utilized to insure the property owner is aware of the requirement and will dedicate the necessary land at such time when the final design of Burdell Blvd. is completed.

Vice-Chair Gerig-Knurowski asked whether there were any additional questions. Seeing no questions or concerns, Vice-Chair Gerig-Knurowski asked for a motion.

Commissioner Baxter moved to recommend approval of the Comprehensive Plan and Zoning Map Amendments, with modification to Condition No. 5 to clarify the requirements of the Irrevocable Petition, and to approve the Minor Land Partition.

Commissioner McClain seconded the motion.

The motion passed 8-0.

B. A-23-04 – Proposed annexation of a 0.33 acre parcel and assign the initial zoning of Residential Mixed Density (Z-RM). (12S 02W 23B, Tax Lot 100)

Vice-Chair Gerig-Knurowski opened the public hearing, had the City Attorney read

into the record the raise it or waive it provisions for basis of appeal, and asked whether there was any conflicts of interest, bias, or ex parte communication to report. Seeing none, Staff was asked to provide a report.

Director Hart provided a summary of the report, and how the project met the decision criteria.

Vice-Chair Gerig-Knurowski asked if there were any comments or questions from the commission. Seeing none, the public hearing was opened for comment. No public comments were provided. The hearing was closed.

Seeing no questions or concerns, Vice-Chair Gerig-Knurowski asked for a motion.

Commissioner Fountain moved to recommend approval of the annexation to the City Council based on the findings written in the staff report.

Commissioner Breshears seconded the motion.

The motion passed 8-0.

C. A-23-05 – Proposed annexation of two parcels totaling 0.31 acres and assigning the initial zoning of Residential Mixed Density (Z-RM). (12S 02W 10CD, Tax Lots 3400 and 3500, 645 and 685 W B Street)

Vice-Chair Gerig-Knurowski opened the public hearing, had the City Attorney read into the record the raise it or waive it provisions for basis of appeal, and asked whether there was any conflicts of interest, bias, or ex parte communication to report. Seeing none, Staff was asked to provide a report.

Director Hart provided a summary of the report, and how the project met the decision criteria.

Vice-Chair Gerig-Knurowski asked if there were any comments or questions from the commission. Seeing none, the public hearing was opened for comment.

The Applicant, Rachel Adamec representing Linn County identified availability to answer any questions.

Commissioner McClain asked why the County was annexing property into the City. The applicant responded that the properties were tax foreclosures, the County has cleaned them up, and identified the City zoning would be better for future redevelopment, resulting in the annexation request. After the annexation, the properties would be put up for auction.

Seeing no further discussion, Vice-Chair Gerig-Knurowski closed the public hearing and asked for any further commissioners' comments, or to provide a motion.

Commissioner Breshears moved to recommend approval of annexation based on the written findings in the staff report.

Commissioner Miller seconded the motion.

The motion passed 8-0.

D. A-23-06 & MLP-23-05 – Proposed annexation of a 0.37 acre parcel and assigning the initial zoning of Residential Mixed Density (Z-RM). Upon annexation, the property is proposed to be partitioned into two parcels. (12S 02W 15CD, Tax Lot 3200, 870 Wassom Street)

Vice-Chair Gerig-Knurowski opened the public hearing, had the City Attorney read into the record the raise it or waive it provisions for basis of appeal, and asked whether there was any conflicts of interest, bias, or ex parte communication to report.

Commissioner Baxter disclosed a potential conflict of interest. Their stepparent owns the property management company that manages the subject property. City Attorney Kennedy asked follow-up questions to clarify the relationship and potential conflict. It was concluded that there is a perceived conflict, but not an actual conflict. Commissioner Baxter indicated that she did not have any bias toward the application and would still be able to make a decision based on the decision criteria. City Attorney Kennedy indicated that since the issue was disclosed, it is not a true conflict of interest, it was acceptable to proceed with participation in the meeting.

Director Hart provided a summary of the report, and how the project met the decision criteria.

Vice-Chair Gerig-Knurowski asked if there were any comments or questions from the commission. Seeing none, the public hearing was opened for comment.

Laura LaRoque, Udell Engineering, represented the applicant and was available for any questions. Seeing none, the hearing was opened for public comment.

Cassandra Turner, owner of neighboring property to the south spoke on the application. It was indicated that the purpose for the comment was not to oppose the annexation, but to see if the city could address some of the issues happening on the subject property. Specifically, the septic system has failed, and raw sewage is seeping up out of the ground, creating a noxious smell, causing headaches, and concerns over medically sensitive family members. Additional nuisance issues were identified.

City Attorney Kennedy indicated that the issues identified were not related to the proposed application, or the decision criteria, and that until the property is annexed, the City cannot take action on the property, but code enforcement related issues could be discussed after the public hearing.

Seeing no additional public comments, the applicant was invited up for rebuttal. Laura LaRoque indicated that per county septic rules, a septic system and drain field cannot be within 10-feet of a property line. With the proposed partition, the property line would be closer than 10-feet, therefore, prior to final plat, connection to city sewer and decommissioning of the septic system would be required, addressing the concerns.

Seeing no further comments, the public hearing was closed. Vice-Chair Gerig-Knurowski asked for any further comments on the application, or a motion.

Commissioner McClain moved to recommend the City Council approve the annexation, and the Planning Commission approve the Partition, contingent upon final decision on the annexation.

Commissioner Fountain seconded the motion.

The motion passed 8-0.

6. COMMISSION BUSINESS & COMMENTS

Director Hart identified there are two items on the agenda for October, and potentially an item currently known for the November meeting. Director Hart also announced that the job flier for the City Manager position has been officially posted and recommended the Commission or members of the public to recommend any qualified candidate to apply.

City Attorney Kennedy presented the annual training on conflicts of interest, bias, ex parte communication, and public meeting laws.

Seeing no further comments or questions, Vice-Chair Gerig Knurowski adjourned the meeting.

ADJOURNMENT:

There being no further business, the meeting was adjourned at 7:37 pm.

[Meeting minutes prepared by Kelly Hart, Community Development Director]



Senior Center and LINX Transit

80 Tangent Street
Lebanon OR 97355
(541) 258-4919

www.ci.lebanon.or.us

ADVISORY BOARD MEETING

August 16, 2023

MINUTES

Members Present: Marlene Flyer, Sherry Liest, Linda Meredith, Cody Wack, Joli Root & Kindra Oliver

1. WELCOME:

Kindra opened the meeting at 10:00 a.m.

The group welcomed three new advisory board members; Cody Wack, Joli Root and Linda Meredith. Marlene and Sherry are returning board members. Thank you to all for serving on the board. We currently have two vacancies on the board.

It has been difficult to have a quorum for meetings the last couple of years and appreciate everyone's willingness to volunteer their time.

2. MINUTES:

Minutes from the July 13, 2022, Advisory Board meeting were approved by consensus.

3. REPORTS:

Meal Site:

- The congregate meal site is open. There aren't as many people coming in as before COVID, but hoping to increase numbers.
- Tori is always looking for substitute drivers to deliver meals for Meals-on-Wheels (MOW).
- One of the long-time MOW volunteers will be retiring at the end of the month, so they need more kitchen help each Monday, Tuesday and Wednesday.

Senior Center:

- We have lots of great things happening at the Senior Center and have been adding programming as we have been making a come back from Covid. We endeavor to offer programming in the seven focus areas, to offer a well-rounded array of options for all to enjoy:
 - Lifelong Learning
 - Health and Wellness
 - Fitness & Exercise
 - Games
 - Social & Recreational
 - Arts & Culture
 - Music & Theatre

- This last fiscal year we have been focusing on Evidence Based Programming (EBP's). EBP's are research-based programs that show participants are likely to experience increased balance, strength, flexibility and other health/wellness related benefits when the program is followed as outlined (ie. participating in the classes twice a week, for a 12-week time frame).
- This current fiscal year, the Senior Center has been awarded funding through the Cascades West Council of Governments (COG) Older Americans Act (OAA) funds for the following:
 - Continuance of Evidence Based Programs:
 - Walk with Ease
 - Geri-Fit Strength and Stretch
 - Bingocize
 - Looking at adding Tai Chi, Yoga or additional EBP
 - I-pad program – purchase of ten i-pads and programming
 - Programming focused on isolation and tech education
 - Idea for cyber theft/security and scams to watch out for
 - Brain health games
 - Personal usage at the Senior Center
- Senior Center staff had a meeting with Samaritan Regional Health Education coordinators to assist with promoting EBP's. Samaritan routinely sends referrals for community members to participate in local EBP's.
- One of the areas Senior Center staff would like to improve on is marketing and promoting our programming so the community is aware of what is available, including enhancing our social media presence.
- The group went through an impromptu brainstorming discussion for Senior Center programming ideas, including:
 - American Sign Language (ASL): Rebecca facilitated a couple of classes in 2023, but could look at doing that again. It would be a good idea to hold a few classes, then offer spaces/times for seniors to meet on their own for "conversation groups" to maintain what they've learned along the way.
 - Seniors would like to see bus trips being offered. This request is a bit difficult to act on, as our buses are dedicated to providing public transportation. We can provide transportation within our service area for local outings, which we have been doing on occasion as long as there is room and it doesn't interfere with our regular service.
 - The group would like to see an attorney come in on a regular basis to provide information on wills, trusts, powers of attorney, etc...
 - The group would like to see more informational classes with COMP NW PT school. Kindra noted the Senior Center does have a partnership with the COMP NW Medical School, including the PT school. Several of our seniors are partaking in the PT sessions the school offers. We also have a few first-year medical students committed to volunteering at the SC each year, for their service organization projects.
 - The group would like to see another Progressive event with other senior living facilities. In years past we would travel to the SC and three or four other locations for a progressive meal and entertainment/education, which allowed the community to visit several senior facilities and learn more about what they offer.
 - The group mentioned square dancing, which is open to the public and is very popular.

- The group would like to see more cooking classes. Rebecca and Michelle currently offer a monthly cooking demonstration, which we have a good turn out for. We have heard that people miss not seeing the videos on Facebook, so hoping we can have a volunteer or student edit the videos to get them posted to social media. One of the board members mentioned that Vandy Roadifer has expressed interest to assist with food demos.
- SC staff would like to see OSU Extension Services start up their cooking classes and other community workshops back at the Senior Center. We have been in contact with the group a few times over the last year and plan to reach out again.
- The group thought it would be a good idea to explore a partnership with YMCA to see if we could offer joint programming/activities. Marlene offered to look into it.
- It was mentioned that Oregon State University has student interns available, through their Human Development Division. They are looking for businesses and agencies to match students up with for internship opportunities. Kindra mentioned the Senior Center had a Portland Community College student come forward a few weeks ago and staff is working with them to complete an internship this fall.
- It was mentioned that it would be nice to have more physical well-being programs and classes. Kindra mentioned that Council of Governments (COG) used to have a wellness coordinator that came in on a monthly basis to have group discussions on various well-being topics. Our local COG no longer has that position, but we have a partnership with Lane County COG who offers that type of programming to a multi-county area and they might be willing to hold some at our Senior Center. Rebecca holds a monthly “Best Life” group that gets together to discuss various topics and support one another. We also host a Living Consciously support group that meets at the Senior Center weekly.
- Board member, Cody Wack, noted he works at Chamberlin House and can offer programming and workshops related to dementia and trauma informed care.
- Other topics of interest mentioned were star gazing, skin care, pound class, hygiene and other wellness related topics.

LINX Transit:

- Kindra provided an overview of the LINX Transit service and our three modes of service to the community, which includes demand response (Monday through Saturday, 7:00am to 6:00pm), our LOOP deviated fixed route (Monday through Saturday, 8:00am to 6:00pm) and the Brownsville Connector (Tuesdays and Fridays, 7:00am to 6:00pm).
- We ended the fiscal year with another record number of rides.
 - 47,850 rides for FY22-23, a 58% increase over the prior fiscal year
 - 30,030 rides for FY21-22, a 36% increase over any other fiscal year.
 - Approximately 20,000 rides averaged each fiscal year for 10 years prior
- LINX Transit had hoped to expand our LOOP deviated fixed route service to run separate routes on the east and west sides of Lebanon. We did apply for funding through the Statewide Transportation Improvement Funds (STIF), in hopes of implementing the expansion in FY24/25, but there weren’t enough STIF funds to support provider requests in Linn County.
- Kindra provided an overview of LINX Transit funding for the FY23-25 biennium, including STIF payroll and population funds, 5311 federal funding, 5310 federal funding, federal COVID-related funding (through October 2024), as well as local funds that contribute to the service.
- We received a new vehicle in June 2023, Bus #24. The cameras, radio and log wrap installation are all complete and it has been in service since the end of June.

4. CONTINUING BUSINESS:

- We continue to assess our staffing needs for maintaining services and operations for the Senior Center and LINX Transit. We have made some adjustments the last few years to keep up with the demands of both departments, especially as we continue to experience growth and increased interest on both sides.

5. NEW BUSINESS:

- Senior Center staff will mail monthly newsletters to SC advisory board members.

6. ADJOURNMENT

Council Minutes

LEBANON CITY COUNCIL
Work Session Minutes - DRAFT
Psilocybin Discussion
October 11, 2023

City Council Present Mayor Jackola, Council President Michelle Steinhebel, Councilors Wayne Dykstra, Jeremy Salvage, Kim Ullfers and Dave Workman

Staff Present City Attorney Tre Kennedy, Interim City Manager Ron Whitlatch, City Recorder Julie Fisher, Community Development Director Kelly Hart and IT Director Brent Hurst

Mayor Jackola called the work session to order at 5:00 p.m.

Community Development Director Kelly Hart presented background on Ballot Measure 109 and an overview of State psilocybin facility regulations. Options to consider included: 1) Doing Nothing (after the November 2024 moratorium lapses, manufacturing and service center facilities would be permitted in the City based on State OARs; 2) Opt-Out (prohibit service centers and manufacturing facilities – voter referral); 3) Partial Opt-Out (prohibit one type of facility, adopt Time, Place and Manner (TPM) for other – voter referral); 4) TPM for both facilities (Council action); or 5) Refer Opt-Out to the voters for consideration, but direct staff to prepare a TPM ordinance in preparation for possible vote to allow facilities in the City.

Community Development Director Hart confirmed for Mayor Jackola that psilocybin can be administered via edibles, extracts, and mushrooms with the effects different for each patient. In response to Mayor Jackola's question as to whether this will be like having a medical marijuana card, Community Development Director Hart said that this is not prescribed by a doctor. It is authorized for anyone over age 21, but they must go through the preparation process. There was a brief discussion about safety measures if someone wants to leave before the session is over.

Responding to Mayor Jackola's question, Community Development Director said that she does not know how the 15% tax is allocated, but the City is prohibited from taxing above that amount. Councilor Steinhebel spoke about the risk-reward of the facilities.

Mayor Jackola asked whether approvals would come before Council like liquor licenses do. Community Development Hart said that she would have to check into this, but the City-level licensing requirement would be a land use compatibility statement. There is no service center limit per capita or distance. Mayor Jackola requested that staff provide a map of areas in the allowable zone. In response to Mayor Jackola's question, Community Development Hart said that she has not toured any manufacturing, laboratories, or service centers. As private facilities, tours would have to be requested. There was a brief discussion about whether these would be contracted facilities. City Attorney Kennedy said that liquor stores contract with the State to buy alcohol, but he would not consider them contractors.

Councilors Salvage and Ullfers expressed concern that someone not licensed as a psychologist or therapist can take 120 hours of training and be certified to treat those with PTSD or other mental health issues.

Mayor Jackola stated his concern that the City will be forced to come up with resources which Lebanon does not have, to manage service centers. The consensus of the Council was to refer opt-out to the voters for consideration but have a TPM ordinance prepared in case the vote is to allow these facilities in the City.

ADJOURNMENT

Mayor Jackola adjourned the work session at 5:52 p.m.

[Minutes prepared by Donna Trippett, Deputy City Recorder]

Minutes Approved by the Lebanon City Council on this
8th day of November 2023

Kenneth E. Jackola, Mayor
Michelle Steinhebel, Council President

ATTESTED:

Julie Fisher, City Recorder

LEBANON CITY COUNCIL
MINUTES – Draft
October 11, 2023

Council Present Mayor Kenneth Jackola, Council President Michelle Steinhebel, Councilors Wayne Dykstra, Jeremy Salvage, Kim Ullfers and Dave Workman

Staff Present City Attorney Tré Kennedy, Interim City Manager Ron Whitlatch, City Recorder Julie Fisher, Police Chief Frank Stevenson, Community Development Director Kelly Hart and IT Director Brent Hurst

Media Present Sarah Brown, Lebanon Local

CALL TO ORDER Mayor Jackola called the meeting to order at 6:00 p.m. in the Santiam Travel Station Board Room.

ROLL CALL Councilor Mann was absent.

CONSENT CALENDAR Councilor Steinhebel moved, Councilor Salvage seconded, to approve the Consent Calendar as presented. The motion passed unanimously.

AGENDA	Lebanon City Council Agenda – October 11, 2023
BOARD MINUTES	Planning Commission – July 19 & August 16, 2023
COUNCIL MINUTES	September 13, 2023 Work & Regular Sessions
EASEMENT	Seven Oak Middle School

PUBLIC COMMENTS

Kay Cortez thanked City staff for addressing the neighborhood safety issues she brought forward.

Greg Black spoke about the wrongful tree and vegetation clearing near S. 16th Street. Interim City Manager Whitlatch apologized for City crews clearing the wrong location and said that the fence would be repaired, and the area cleaned up.

Tana Nicholson shared her distress at seeing the land clearing and not receiving quick responses to her messages.

Bill Sullivan spoke about crime and the homeless. Mayor Jackola said that solutions to homelessness and the jail closure are continually being discussed.

PRESENTATION

1) Community Services Consortium

Community Services Consortium (CSC) Executive Director Pegge McGuire gave a presentation [available in the Council packet] on Continuum of Care (CoC) and the formation of a Tri-County Continuum. She requested that Council pass a resolution supporting the creation of the CoC.

Councilor Workman asked what would change with the CoC formation. Ms. McGuire said that as a smaller group of counties, it would give them greater control over what is applied for and allow for more appropriate processes. The drawback is that the group will have to be self-sufficient.

In response to Councilor Salvage's question about whether this would include services outside of housing, Ms. McGuire said that it includes the entire spectrum from keeping people housed through getting people rehoused and providing supportive services.

Councilor Salvage asked if there is a cost to the City for CSC managing the continuum. Ms. McGuire said that the plan is to use incoming funding, but she is unsure about the future. She estimates three people managing the group, but it depends on the continuation of local planning groups.

Councilor Ulfers shared that he has worked with CSC quite a bit and that consolidating into a local group makes sense financially and esoterically.

PUBLIC HEARINGS

2) Annexation A-23-04 (155 Taylor Street)

Mayor Jackola opened the public hearing at 6:37 p.m. City Attorney Kennedy reviewed public hearing quasi-judicial procedures. There were no Councilor declarations of ex parte contact, bias or conflicts of interest and no objections to the notice sent out or to the jurisdiction of this body to hear and consider this case.

Community Development Director Hart presented a request to annex a 0.33-acre parcel at 155 Taylor Street. The staff report provides analysis of the required review criteria and recommended findings on the annexation requirements.

Mayor Jackola asked for the applicant's testimony.

Laura LaRoque, Udell Engineering, the applicant's representative, made herself available for questions.

There being no comments regarding the application, Mayor Jackola closed the public hearing at 6:40 p.m.

City Attorney Kennedy read the title of ORDINANCE BILL NO. 2023-14, ORDINANCE NO. 3009. ***Councilor Workman moved, Councilor Salvage seconded, to APPROVE ORDINANCE BILL NO. 2023-14, ORDINANCE NO. 3009, A BILL FOR AN ORDINANCE ANNEXING AND ZONING PROPERTIES FOLLOWING CONSENT FILED WITH THE CITY COUNCIL BY LANDOWNERS IN SAID AREA PURSUANT TO ORS 222.120 AND ORS 222.170; File A-23-04; MATTHEW MCELVARY. The motion passed unanimously.***

3) Annexation A-23-05 (645 & 685 West B Street)

Mayor Jackola opened the public hearing at 6:42 p.m. City Attorney Kennedy reviewed public hearing quasi-judicial procedures. There were no Councilor declarations of ex parte contact, bias or conflicts of interest and no objections to the notice sent out or to the jurisdiction of this body to hear and consider this case.

Community Development Director Hart presented a request to annex two parcels totaling 0.31 acres at 645 and 685 West B Street. The staff report provides analysis of the required review criteria and recommended findings on the annexation requirements.

There being no testimony or comments regarding the application, Mayor Jackola closed the public hearing at 6:44 p.m.

City Attorney Kennedy read the title of ORDINANCE BILL NO. 2023-16, ORDINANCE NO. 3011. **Councilor Salvage moved, Councilor Workman seconded, to APPROVE ORDINANCE BILL NO. 2023-16, ORDINANCE NO. 3011, A BILL FOR AN ORDINANCE ANNEXING AND ZONING PROPERTIES FOLLOWING CONSENT FILED WITH THE CITY COUNCIL BY LANDOWNERS IN SAID AREA PURSUANT TO ORS 222.120 AND ORS 222.170; File A-23-05; LINN COUNTY, OREGON. The motion passed unanimously.**

4) Annexation A-23-06 (870 Wassom Street)

Mayor Jackola opened the public hearing at 6:45 p.m. City Attorney Kennedy reviewed public hearing quasi-judicial procedures. There were no Councilor declarations of ex parte contact, bias or conflicts of interest and no objections to the notice sent out or to the jurisdiction of this body to hear and consider this case.

Community Development Director Hart presented a request to annex a 0.37-acre parcel at 870 Wassom Street. The staff report provides analysis of the required review criteria and recommended findings on the annexation requirements.

Mayor Jackola asked for the applicant's testimony.

Laura LaRoque, Udell Engineering, the applicant's representative, made herself available for questions.

There being no comments regarding the application, Mayor Jackola closed the public hearing at 6:48 p.m.

City Attorney Kennedy read the title of ORDINANCE BILL NO. 2023-17, ORDINANCE NO. 3012. **Councilor Ulfers moved, Councilor Dykstra seconded, to APPROVE ORDINANCE BILL NO. 2023-17, ORDINANCE NO. 3012, A BILL FOR AN ORDINANCE ANNEXING AND ZONING PROPERTIES FOLLOWING CONSENT FILED WITH THE CITY COUNCIL BY LANDOWNERS IN SAID AREA PURSUANT TO ORS 222.120 AND ORS 222.170; File A-23-06; GARCIA RIGOBERTO. The motion passed unanimously.**

5) Comprehensive Plan Map Amendment and Zone Map (CPMA-23-02 & ZMA-23-02)

Mayor Jackola opened the public hearing at 6:49 p.m. City Attorney Kennedy reviewed public hearing quasi-judicial procedures. There were no Councilor declarations of ex parte contact, bias or conflicts of interest and no objections to the notice sent out or to the jurisdiction of this body to hear and consider this case.

Community Development Director Hart presented a request to amend the Comprehensive Plan Map and Zoning Map designation for a 1.33-acre parcel on the east side of S. Santiam Hwy. The application complies with Lebanon Development Code decision criteria. She pointed out that the typo in section 3 of the ordinance that refers to an annexation rather than a comprehensive plan map amendment boundary will be corrected.

Mayor Jackola asked for the applicant's testimony.

Raul Woerner, CSA Planning, the applicant's representative, said that the change in zoning is to allow for construction of an automated drive-thru car wash.

There being no comments regarding the application, Mayor Jackola closed the public hearing at 6:53 p.m.

City Attorney Kennedy read the title of ORDINANCE BILL NO. 2023-18, ORDINANCE NO. 3013. **Councilor Steinhebel moved, Councilor Ulfers seconded, to APPROVE ORDINANCE BILL NO. 2023-18, ORDINANCE NO. 3013, A BILL FOR AN ORDINANCE AMENDING THE LEBANON COMPREHENSIVE PLAN MAP AND ZONING MAP DESIGNATION TO ESTABLISH THE COMMERCIAL/HIGHWAY COMMERCIAL DESIGNATION FOR THE 1.33-ACRE PROPERTY PART OF (12S-02W-23B, TAX LOT 100);**

REGULAR SESSION

6) Resolution to Support the Creation of the Linn, Benton, and Lincoln Counties Continuum of Care

Community Development Director Hart requested Council approval of a resolution supporting the formation of the Linn, Benton, and Lincoln Continuum of Care.

City Attorney Kennedy read the title of RESOLUTION NO. 2023-16. *Councilor Ullfers moved, Councilor Workman seconded, to APPROVE RESOLUTION NO. 2023-16 A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF LEBANON, OREGON IN SUPPORT OF ESTABLISHING A CONTINUUM OF CARE FOR LINN, BENTON, AND LINCOLN COUNTIES REGION AND REGISTERING WITH THE UNITED STATES DEPARTMENT OF HOUSING AND URBAN DEVELOPMENT IN 2024. The motion passed unanimously.*

7) System Development Fees for Accessory Dwelling Units

Community Development Director Hart reported on current Accessory Dwelling Units (ADUs) Systems Development Charges (SDCs) and reduced-fee options for Council to consider – 1) assess SDCs for ADUs only when new square footage is created; 2) establish a standard reduced fee for ADUs; 3) provide grants/waivers for ADU SDCs; 4) no SDC fee for ADUs; and 5) non-SDC option. She also briefly mentioned Senate Bill 919 that would allow governments to adopt a property tax exemption for newly constructed ADUs.

Interim City Manager Whitlatch explained that SDCs are used mainly for growth and oversizing. It cannot be used for improvements like capital maintenance (street overlays, waterline replacements, sewer line replacements, lateral replacements, park maintenance).

In response to Councilor Workman's question, Community Development Director Hart said that there are less than a handful of ADU applications a year. The SDC costs are a factor but not every property is suitable for developing an ADU.

Council consensus was to bring back a resolution to adopt the following into a new fee structure – zero SDCs when there is no new square footage created and 40% of single-family dwelling SDCs if new square footage is created. This can be revisited if there is a large increase in the number of ADUs.

8) RV Parks (Code Clarification and Discussion on Council's 2018 Determination)

Community Development Director Hart provided history and requested Council direction on privately owned RV parks as a conditional use within the Highway Commercial zone. She clarified that according to State law, RVs are allowed to reside full-time in manufactured home parks.

There was discussion regarding the effects (fire safety, property maintenance issues over time, current RV issues on City streets, little assessed value and tax revenue with increased demands on library, park and police services) this would likely have.

Council consensus was to amend the Development Code to eliminate privately owned camping/temporary stay RV parks within City limits.

9) Update on Protection from the Elements Time, Place and Manner (TPM) Restrictions

Police Chief Stevenson gave an update on protection from the elements time, place and manner (TPM) restrictions, which give them the tools needed when receiving complaints about individuals being where they should not be. The Oak Street sleeping area was used four times; no one has used the Justice Center site. Since these TPM restrictions are working, he does not see the need to confine it to one location.

10) Approval to Award – Old Water Treatment Plant Demolition

Interim City Manager Whitlatch requested that Council award the Decommissioned Water Treatment Plant Site Rehabilitation Project to Staton Companies for \$436,108.00. Since the City does not normally do projects of this type, the Engineer's Estimate was hard to determine; excess funds will be used for future waterline replacement.

The project will include removing and backfilling everything three feet below existing grade. Pipes will be abandoned and filled.

Councilor Ulfers moved, Councilor Steinhebel seconded, to AWARD THE DECOMMISSIONED WATER TREATMENT PLANT SITE REHABILITATION PROJECT TO STATON COMPANIES FOR \$436,108.00. The motion passed unanimously.

11) Additional Signage – Intersection of N. 2nd and Mary Streets

Interim City Manager Whitlatch requested Council approval of a resolution implementing a four-way stop at the intersection of N. 2nd and Mary Streets based on sight distance constraints due to a large generational oak tree.

City Attorney Kennedy read the title of RESOLUTION NO. 2023-17. ***Councilor Ulfers moved, Councilor Workman seconded, to APPROVE RESOLUTION NO. 2023-17 A RESOLUTION INSTALLING TRAFFIC CONTROL DEVICES ON SECOND STREET AT THE INTERSECTION OF MARY STREET WITHIN THE CITY OF LEBANON. The motion passed unanimously.***

12) City Manager's Report – Interim City Manager Whitlatch provided updates:

- The annual big waterline flush will be held on October 30 - November 9, 2023.
- Transit & Senior Services Director Oliver is projecting almost 60,000 LINX rides this year, which is almost 15,000 more than last year.
- Staff is issuing daily maps to the Lebanon Fire District, Linn County Sheriff's Department and Lebanon School District for the Weirich Road/Crowfoot Road/Cascade Drive construction.

ITEMS FROM COUNCIL

Councilor Workman brought up the City's shared Oregon Health Authority post on Measure 110 and said that it sends the wrong message to citizens. Interim City Manager Whitlatch said that the City serves all of Lebanon's citizens and staff looks at information regardless of personal opinion. Serious guidance on policy would be needed if the Council would like to see a change.

There was discussion regarding the City not allowing comments on Facebook. Interim City Manager Whitlatch and City Attorney Kennedy said that the City does want to hear from citizens, but this would require monitoring and comments are subject to public records retention. Interim City Manager Whitlatch recommended calling or using email submissions, as they are checked daily. Mayor Jackola agreed with not allowing comments because City staff and Council are easily accessible.

Councilor Dykstra asked what power the Council has in controlling rent in the City. Community Development Director Hart said that cities cannot institute any further rent control measures beyond what the State establishes. The Fair Housing Council provides aid and services for those having issues.

PUBLIC/PRESS COMMENTS

Bill Sullivan commented about local landlords.

Interim City Manager Whitlatch confirmed for Mr. Sullivan that the Accelator tank will be removed during the water treatment plant demolition.

NEXT SCHEDULED COUNCIL MEETING

- November 8, 2023 Regular Session (6:00 p.m.)

Mayor Jackola called for a ten-minute recess. The Executive Session was called to order at 7:54 p.m.

Council Present Mayor Kenneth Jackola, Council President Michelle Steinhebel, Councilors Wayne Dykstra, Jeremy Salvage, Kim Ulfers and Dave Workman

Staff Present City Attorney Tre Kennedy, Interim City Manager Ron Whitlatch, City Recorder Julie Fisher and Human Resources Director Angela Solesbee.

Media Present Sarah Brown, Lebanon Local

The Executive Session announcement was read.

EXECUTIVE SESSION

Executive Sessions are closed to the public due to the highly confidential nature of the subject. It is unlawful to discuss anything outside of the Executive Session. Final action/decisions are to be made in open session. Per ORS 192.660(2)(a) To consider the employment of a public officer, employee, staff member or individual agent (City Manager).

No Council decisions were made in Executive Session, which adjourned at 8:44 p.m.

[Minutes prepared by Donna Trippett, Deputy City Recorder]

Minutes Approved by the Lebanon City Council on this 8th day of November 2023.

Kenneth E. Jackola, Mayor
Michelle Steinhebel, Council President

ATTESTED:

Julie Fisher, City Recorder

Presentation

Annual Risk Report

Darrin Godfrey, HUB International Northwest LLC



City of Lebanon

Risk Management Report November 8, 2023

Presented by:



**Darrin Godfrey
Senior Vice President, Commercial Insurance**

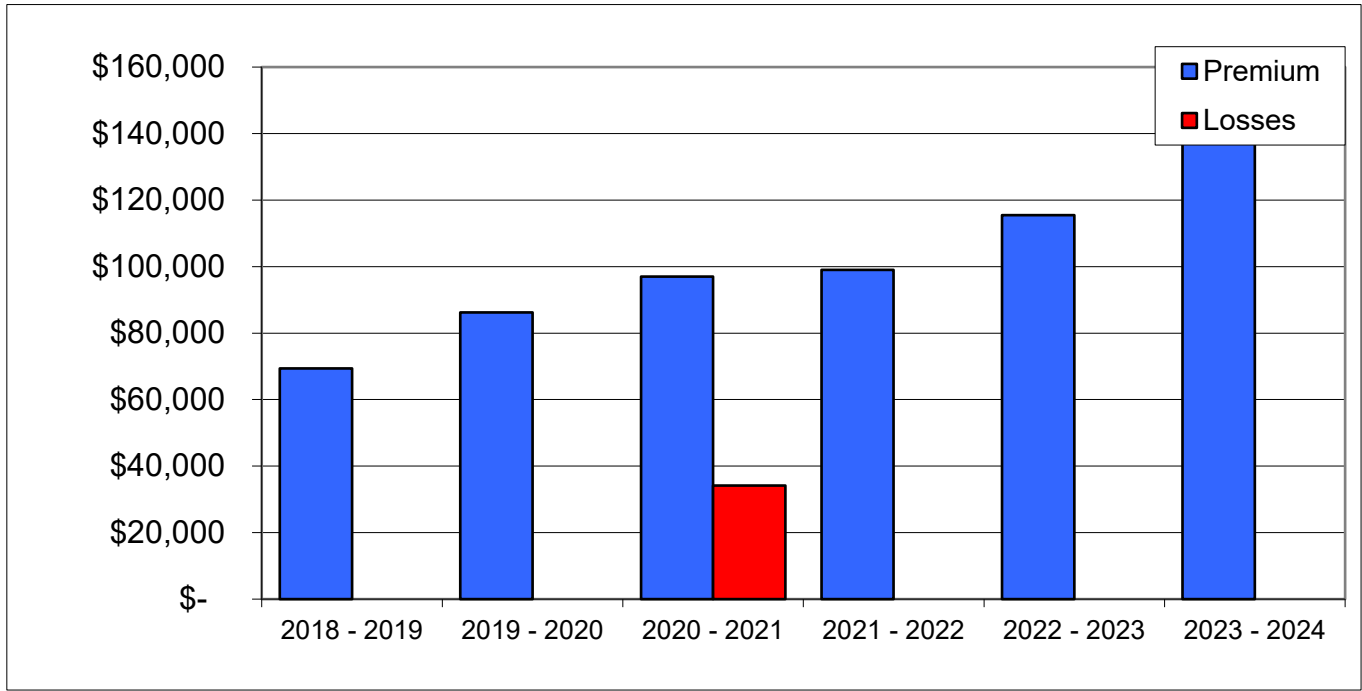
**Mike Godfrey, CIC
Senior Vice President, Workers Compensation Practice Leader**

Schedule of Property and Liability Insurance Coverage / Premium History

Coverage		2018 - 2019	2019 - 2020	2020 - 2021	2021 - 2022	2022 - 2023	2023 - 2024
PROPERTY	Company	CIS	CIS	CIS	CIS	CIS	CIS
Bldgs. & Equipment	Policy #	18PLEB	19PLEB	20PLEB	21PLEB	22PLEB	23PLEB
(Includes \$10 Million Earthquake \$5 Million Flood)	Limits	\$ 80,106,649	\$ 112,400,131	\$ 115,683,129	\$ 109,687,977	\$ 121,388,175	\$ 133,899,809
	Premium	\$ 69,404	\$ 86,227	\$ 96,962	\$ 98,986	\$ 115,516	\$ 148,699
	Deductible	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000
	Losses	\$ -	\$ -	\$ 34,191	\$ -	\$ -	\$ -
				Excludes EQ on City Hall	Excludes EQ on City Hall	Excludes EQ on City Hall	Excludes EQ on City Hall
				Debris Removal Only - Old Water Treatment Plant 1155 S 2nd St.	Debris Removal Only - Old Water Treatment Plant 1155 S 2nd St.	Debris Removal Only - Old Water Treatment Plant 1155 S 2nd St.	Debris Removal Only - Old Water Treatment Plant 1155 S 2nd St.
MOBILE EQUIPMENT	Company	CIS	CIS	CIS	CIS	CIS	CIS
	Policy #	18PLEB	19PLEB	20PLEB	21PLEB	22PLEB	23PLEB
	Limits	\$ 1,012,474	\$ 1,148,960	\$ 1,148,960	\$ 1,410,203	\$ 1,410,203	\$ 1,410,203
	Premium	Included	Included	Included	Included	Included	Included
	Deductible	\$ 1,000	\$ 1,000	\$ 1,000	\$ 1,000	\$ 1,000	\$ 1,000
	Losses	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
BOILER & MACHINERY	Company	CIS	CIS	CIS	CIS	CIS	CIS
	Policy #	18BLEB	19BLEB	20BLEB	21BLEB	22BLEB	23BLEB
	Limits	\$ 80,106,649	\$ 100,000,000	\$ 100,000,000	\$ 100,000,000	\$ 100,000,000	\$ 100,000,000
	Premium	Included	Included	Included	Included	Included	Included
	Deductible	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000
	Losses	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
Earthquake On City Hall	Company				Lloyd's of London	Lloyd's of London	Lloyd's of London
	Policy #				B0507EP2121475	B0507EP2230455	B0507EP2341770
	Limits				\$ 5,500,000	\$ 5,500,000	\$ 6,205,281
	Premium				\$ 14,542	\$ 16,122	\$ 19,831
	Deductible				5% \$25k Min	5% \$25k Min	5% \$25k Min
	Losses				\$ -	\$ -	\$ -
Excess Crime	Company	CIS	CIS	CIS	CIS	CIS	CIS
(\$50,000 provided in property coverage)	Policy #	18ECLEB	19ECLEB	20ECLEB	21ECLEB	22ECLEB	23ECLEB
	Limits	\$ 250,000	\$ 250,000	\$ 500,000	\$ 500,000	\$ 500,000	\$ 500,000
	Premium	\$ 643	\$ 815	\$ 1,143	\$ 1,143	\$ 1,181	\$ 1,214
	Deductible	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000
	Losses	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
TORT LIABILITY	Company	CIS	CIS	CIS	CIS	CIS	CIS
	Policy #	18LLEB	19LLEB	20LLEB	21LLEB	22LLEB	23LLEB
	Limits	\$ 5,000,000	\$ 5,000,000	\$ 5,000,000	\$ 5,000,000	\$ 5,000,000	\$ 5,000,000
	Premium	\$ 107,371	\$ 126,277	\$ 174,122	\$ 189,964	\$ 237,253	\$ 230,573
	Deductible	\$25,000 Annual Aggregate	\$25,000 Annual Aggregate	\$2,500 Per Occurrence	\$5,000 Per Occurrence	\$5,000 Per Occurrence	\$5,000 Per Occurrence
	Losses	\$ 517,016	\$ 362,423	\$ 8,000	\$ -	\$ -	\$ -
AUTO LIABILITY	Company	CIS	CIS	CIS	CIS	CIS	CIS
	Policy #	18LLEB	19LLEB	20LLEB	21LLEB	22LLEB	23LLEB
	Limits	\$ 5,000,000	\$ 5,000,000	\$ 5,000,000	\$ 5,000,000	\$ 5,000,000	\$ 5,000,000
	Premium	\$ 24,330	\$ 26,390	\$ 36,738	\$ 40,159	\$ 51,740	\$ 56,445
	Deductible	Included in Tort	Included in Tort	\$2,500 Per Occurrence	\$5,000 Per Occurrence	\$5,000 Per Occurrence	\$5,000 Per Occurrence
	Losses	\$ 13,902	\$ 114,400	\$ 2,753	\$ -	\$ -	\$ 8,600
AUTO PHYS DAMAGE	Company	CIS	CIS	CIS	CIS	CIS	CIS
	Policy #	18APDLEB	19APDLEB	20APDLEB	21APDLEB	22APDLEB	23APDLEB
	Premium	\$ 17,224	\$ 20,975	\$ 27,553	\$ 28,475	\$ 31,826	\$ 35,645
	Deductible	\$500 / \$500	\$500 / \$500	\$500 / \$500	\$500 / \$500	\$500 / \$500	\$500 / \$500
	Losses	\$ 14,055	\$ 22	\$ 2,770	\$ -	\$ -	\$ 7,500
Cyber Liability	Company	CIS	CIS	CIS	CIS	CIS	CIS
Tiers 1 & 2 (effective 07/01/22)	Policy #	18ECLLEB	19ECLLEB	20ECLLEB	21ECLLEB	22CYBLEB	23CYBLEB
	Limits	\$ 950,000	\$ 950,000	\$ 950,000	\$ 950,000	\$ 250,000	\$ 250,000
	Premium	\$ 2,592	\$ 2,880	\$ 3,485	\$ 4,179	\$ 4,072	\$ 4,072
	Deductible	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000
	Losses	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
Excess Cyber Liability	Company					CIS	CIS
Tier 3	Policy #					22ECYLEB	23CYBLEB
	Limits	Not Applicable	Not Applicable	Not Applicable	Not Applicable	\$ 750,000	\$ 750,000
	Premium					\$ 6,210	\$ 6,521
	Deductible					\$ 5,000	\$ 5,000
	Losses					\$ -	\$ -
VOLUNTEER	Company	Hartford Life	HUB International	Hartford Life	Hartford Life	Hartford Life	Hartford Life

ACCIDENT POLICY	Policy #	52SR702382	52SR702382	52SR702382	52SR702382	52SR702382	52SR702382
	Limits	\$ 10,000	\$ 10,000	\$ 10,000	\$ 10,000	\$ 10,000	\$ 10,000
	Premium	\$ 340	\$ 340	\$ 340	\$ 340	\$ 340	\$ 340
	Deductible	None	None	None	None	None	None
	Losses	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
TANK POLLUTION LIABILITY	Company	Great American	Mid-Continent	Mid-Continent	Mid-Continent	Mid-Continent	Mid-Continent
	Policy #	BTA 9990979-04	04-Top-000003730	04-Top-000011373	04-TOP-000020430	04-TOP-000030118	04-TOP-000030118
	Limits	\$ 1,000,000	\$ 1,000,000	\$ 1,000,000	\$ 1,000,000	\$ 1,000,000	\$ 1,000,000
	Premium	\$ 825	\$ 500	\$ 500	\$ 500	\$ 550	\$ 550
	Deductible	\$ 10,000	\$ 10,000	\$ 10,000	\$ 10,000	\$ 10,000	\$ 10,000
	Losses	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
PUBLIC OFFICIAL BOND CITY MANAGER	Company	Hartford Fire	Hartford Fire	Not Renewed	Not Renewed	Not Renewed	Not Renewed
	Policy #	52BSBHF6641	52BSBHF6641	Included in Crime	Included in Crime	Included in Crime	Included in Crime
	Limits	\$ 300,000	\$ 300,000				
	Premium	\$ 920	\$ 148				
	Losses	\$ -	\$ -				
PUBLIC OFFICIAL BOND FINANCE DIRECTOR	Company	Hartford Fire	Hartford Fire	Not Renewed	Not Renewed	Not Renewed	Not Renewed
	Policy #	52BSBHX9696	52BSBHX9696	Included in Crime	Included in Crime	Included in Crime	Included in Crime
	Limits	\$ 300,000	\$ 300,000				
	Premium	\$ 1,150	\$ 1,150				
	Losses	\$ -	\$ -				
Total Premiums		\$ 224,799	\$ 265,700	\$ 340,844	\$ 378,288	\$ 464,810	\$ 502,999
Total Claim Costs		\$ 544,973	\$ 476,845	\$ 47,715	\$ -	\$ -	\$ 16,100

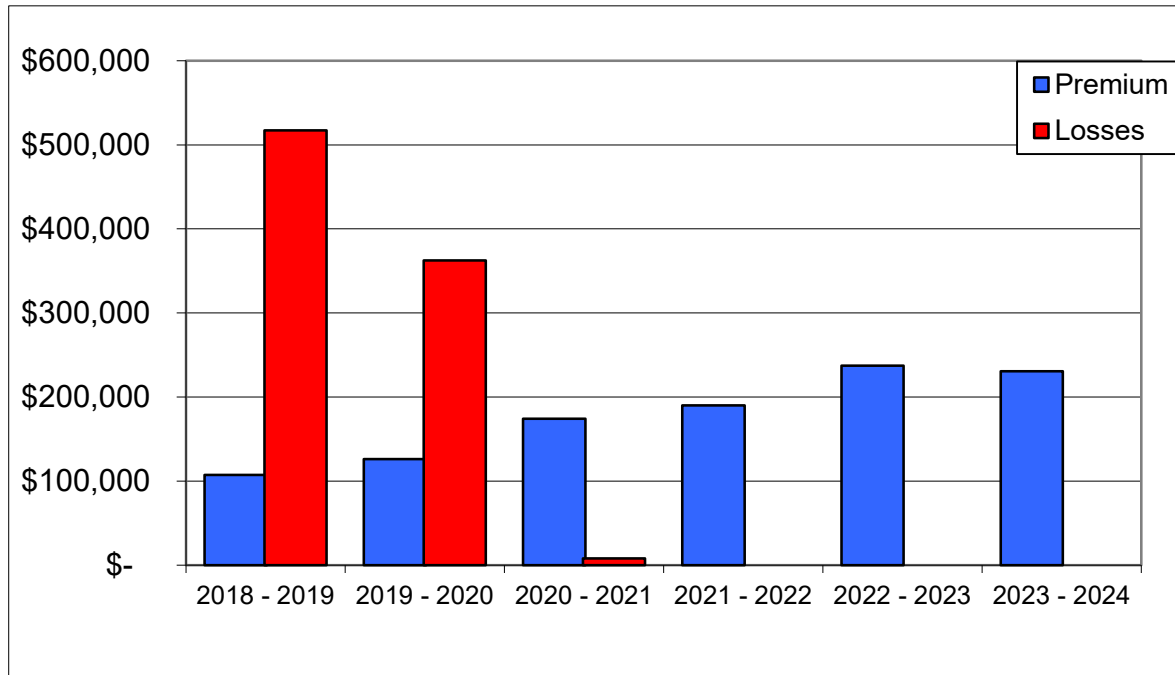
Property 7/01/18 to 09/27/23



Year	2018 - 2019	2019 - 2020	2020 - 2021	2021 - 2022	2022 - 2023	2023 - 2024
Premium	\$ 69,404	\$ 86,227	\$ 96,962	\$ 98,986	\$ 115,516	\$ 148,699
Losses	\$ -	\$ -	\$ 34,191	\$ -	\$ -	\$ -
Deductible	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000	\$ 5,000

Premiums shown include Mobile Equipment, Boiler & Machinery and Excess Earthquake.

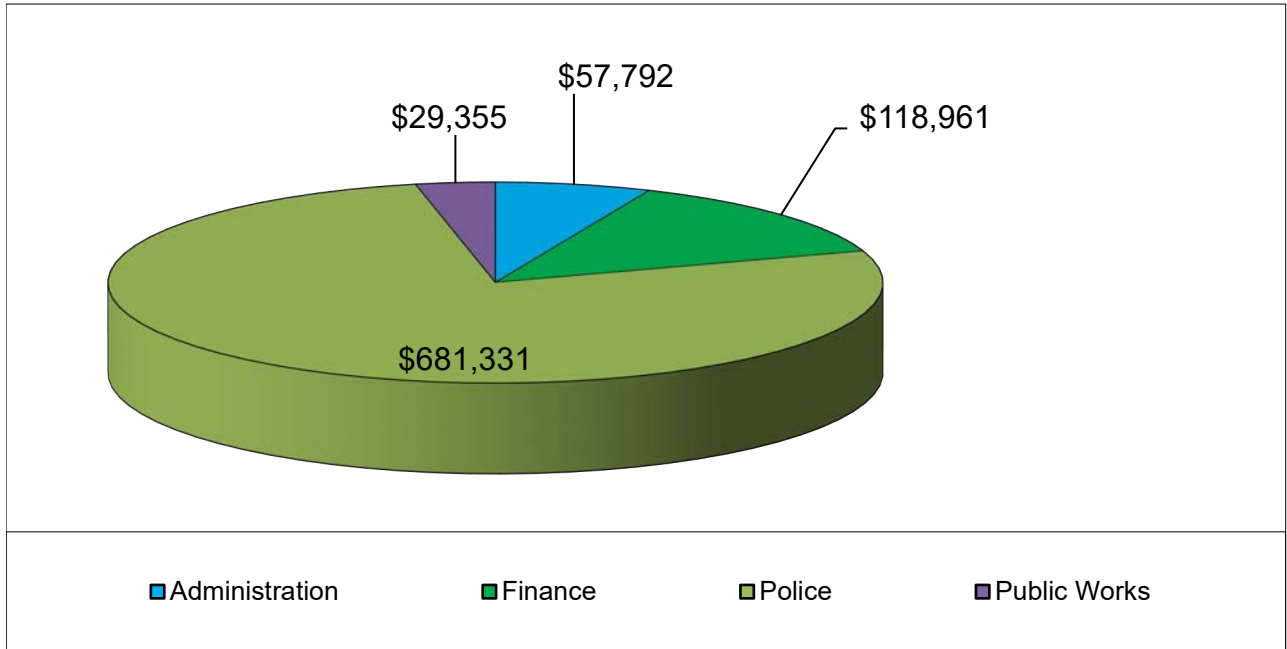
Tort Liability 7/01/18 to 09/27/23



Year	2018 - 2019	2019 - 2020	2020 - 2021	2021 - 2022	2022 - 2023	2023 - 2024
Premium	\$ 107,371	\$ 126,277	\$ 174,122	\$ 189,964	\$ 237,253	\$ 230,573
Losses	\$ 517,016	\$ 362,423	\$ 8,000	\$ -	\$ -	\$ -
Deductible	\$25,000 Annual Aggregate	\$25,000 Annual Aggregate	\$2,500 Per Occurrence	\$5,000 Per Occurrence	\$5,000 Per Occurrence	\$5,000 Per Occurrence

Loss amounts shown include loss adjustment expense (defense costs) and reserves.

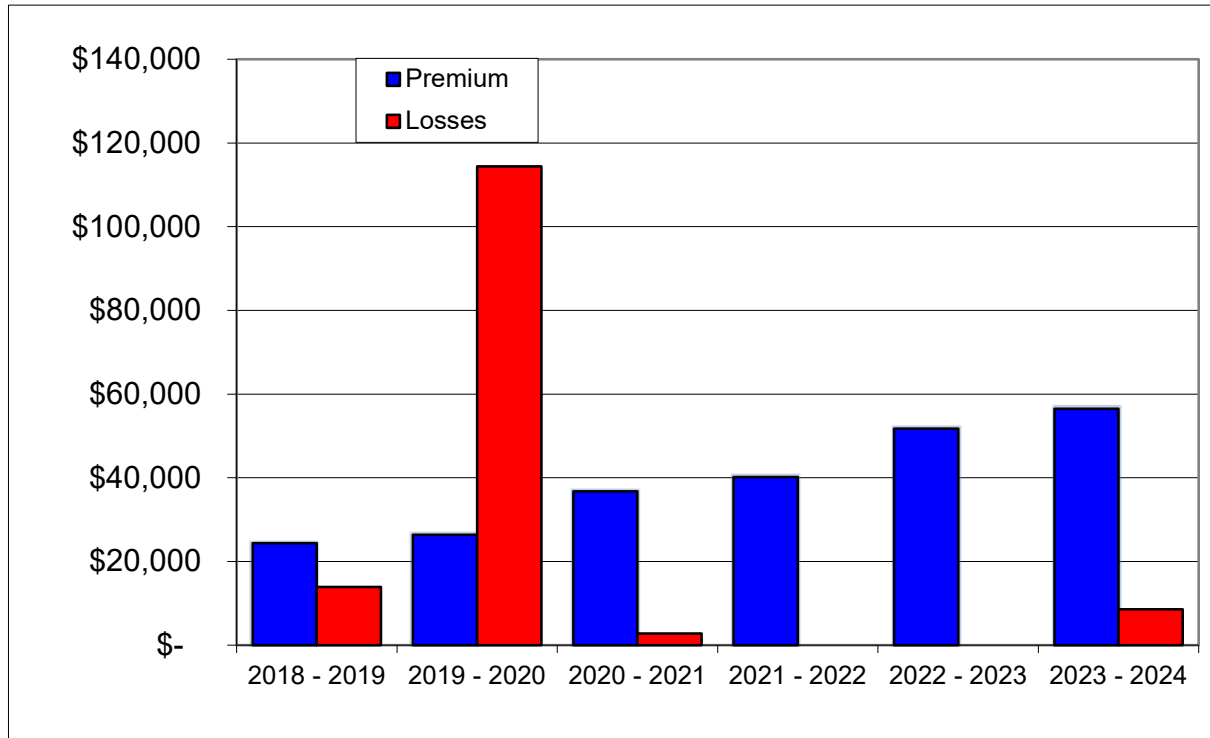
Tort Liability - By Department 7/01/18 to 09/27/23



<u>Department</u>	<u>Incurred \$</u>	<u>%</u>
Administration	\$ 57,792	6.51%
Finance	\$ 118,961	13.40%
Police	\$ 681,331	76.77%
Public Works	\$ 29,355	3.31%
	<u>\$ 887,439</u>	<u>100.00%</u>

Loss amounts shown include loss adjustment expense (defense costs) and reserves.

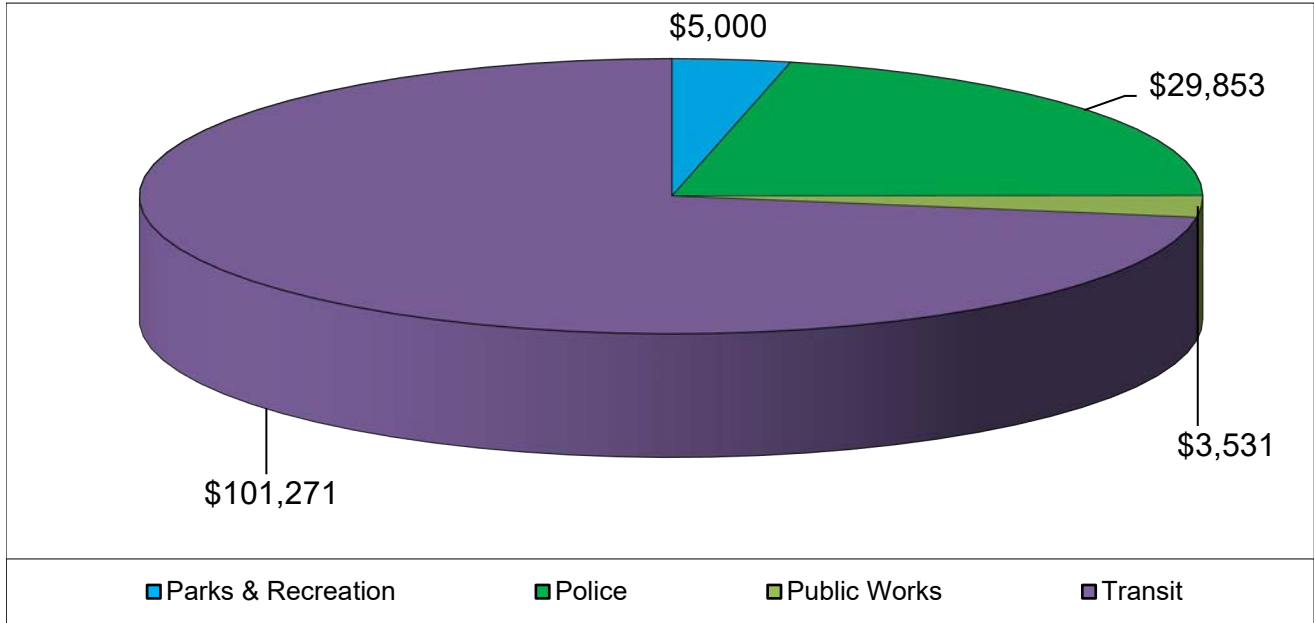
Auto Liability 7/01/18 to 09/27/23



Year	2018 - 2019	2019 - 2020	2020 - 2021	2021 - 2022	2022 - 2023	2023 - 2024
Premium	\$ 24,330	\$ 26,390	\$ 36,738	\$ 40,159	\$ 51,740	\$ 56,445
Losses	\$ 13,902	\$ 114,400	\$ 2,753	\$ -	\$ -	\$ 8,600
Deductible	Included in Tort	Included in Tort	\$2,500 Per Occurrence	\$5,000 Per Occurrence	\$5,000 Per Occurrence	\$5,000 Per Occurrence

Loss amounts shown include loss adjustment expense (defense costs) and reserves.

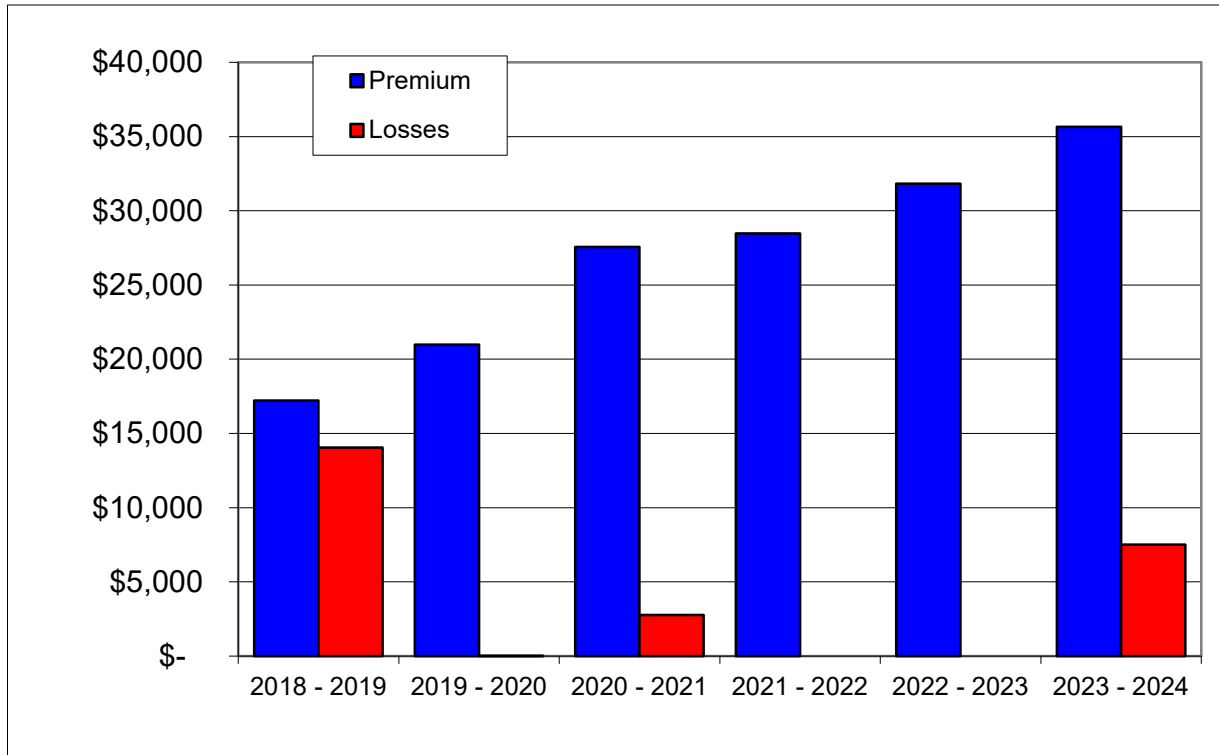
Auto Liability - By Department 7/01/18 to 09/27/23



Department	Incurred \$	%
Parks & Recreation	\$ 5,000	3.58%
Police	\$ 29,853	21.38%
Public Works	\$ 3,531	2.53%
Transit	\$ 101,271	72.52%
	\$ 139,655	100.00%

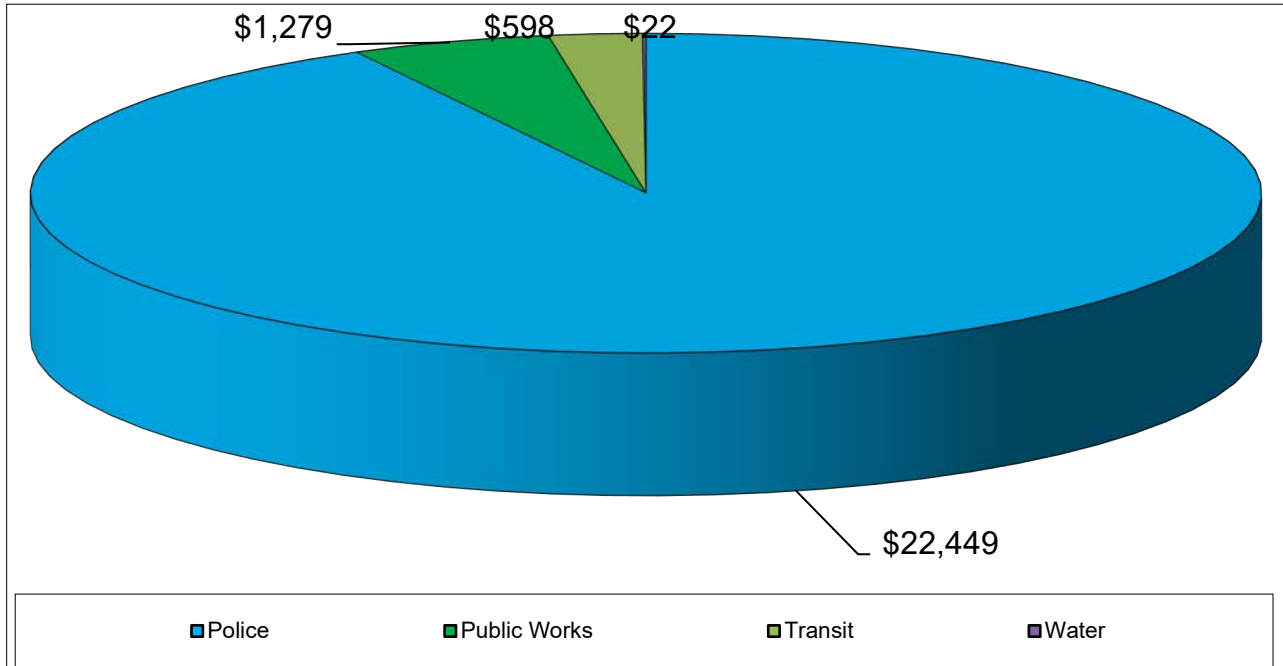
Loss amounts shown include loss adjustment expense (defense costs) and reserves.

Auto Physical Damage 7/01/18 to 09/27/23



Year	2018 - 2019	2019 - 2020	2020 - 2021	2021 - 2022	2022 - 2023	2023 - 2024
Premium	\$ 17,224	\$ 20,975	\$ 27,553	\$ 28,475	\$ 31,826	\$ 35,645
Losses	\$ 14,055	\$ 22	\$ 2,770	\$ -	\$ -	\$ 7,500
Deductible	Various	Various	Various	Various	Various	Various

Auto Physical Damage - By Department 7/01/18 to 09/27/23



Loss amounts shown include loss adjustment expense (defense costs) and reserves.

Property and Liability Insurance Claim Costs

7/01/2018 to 09/27/2023

Valued as of 09/27/2023

Property and Liability Claim Costs Total by Year

Fiscal Year	Number of Claims	Incurred Claim Costs	Average Cost Per Claim
2018 - 2019	16	\$ 544,973	\$ 34,061
2019 - 2020	17	\$ 476,845	\$ 28,050
2020 - 2021	9	\$ 47,715	\$ 5,302
2021 - 2022	3	\$ -	\$ -
2022 - 2023	1	\$ -	\$ -
2023 - 2024	3	\$ 16,100	\$ 5,367
Five Year Total	59	\$ 1,153,416	\$ 19,549
Annual Average	12	\$ 230,683	

Incurred Claim Costs included paid expenses plus any amount reserved for future expenses or settlements related to the original claim.

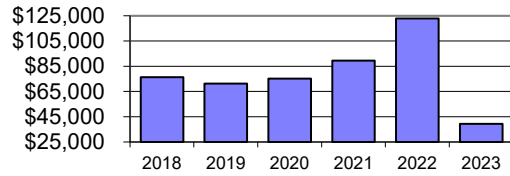
If the claim is pending due to litigation, more dollars may be paid or reserved in the future.

Property and Liability Claim Costs by Type of Claim

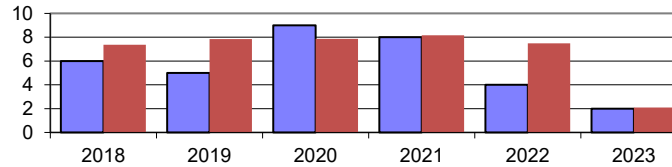
Fiscal Year	Auto Liability		Auto Physical Damage		General Liability		Property		Total Costs	Total # of Claims
	Incurred \$	#	Incurred \$	#	Incurred \$	#	Incurred \$	#		
2018 - 2019	\$ 13,902	4	\$ 14,055	4	\$ 517,016	7	\$ -	1	\$ 544,973	16
2019 - 2020	\$ 114,400	4	\$ 22	3	\$ 362,423	10	\$ -	0	\$ 476,845	17
2020 - 2021	\$ 2,753	2	\$ 2,770	2	\$ 8,000	4	\$ 34,191	1	\$ 47,715	9
2021 - 2022	\$ -	1	\$ -	0	\$ -	2	\$ -	0	\$ -	3
2022 - 2023	\$ -	1	\$ -	0	\$ -	0	\$ -	0	\$ -	1
2023 - 2024	\$ 8,600	2	\$ 7,500	1	\$ -	0	\$ -	0	\$ 16,100	3
Total	\$ 139,655	14	\$ 24,348	10	\$ 887,439	23	\$ 34,191	2	\$ 1,085,633	49
Annual average	\$ 27,931	3	\$ 4,870	2	\$ 177,488	5	\$ 6,838	0	\$ 217,127	10
Ave. per claim	\$ 9,975		\$ 2,435		\$ 38,584		\$ 17,096		\$ 22,156	

Workers' Compensation 07/01/2018 to 10/13/2023

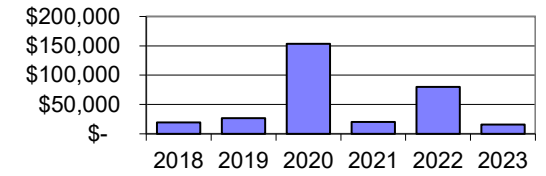
Standard Premium



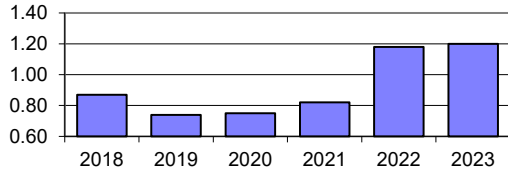
of Claims vs. Expected # of Claims



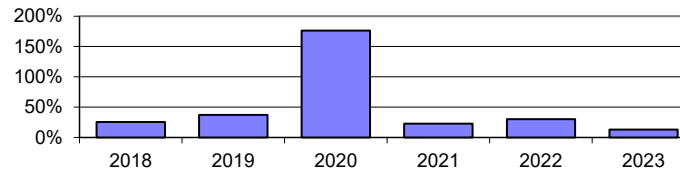
Incurred Costs



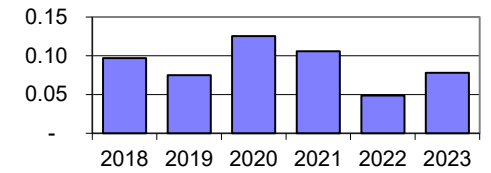
Experience Mod



Pd / Std Loss Ratio



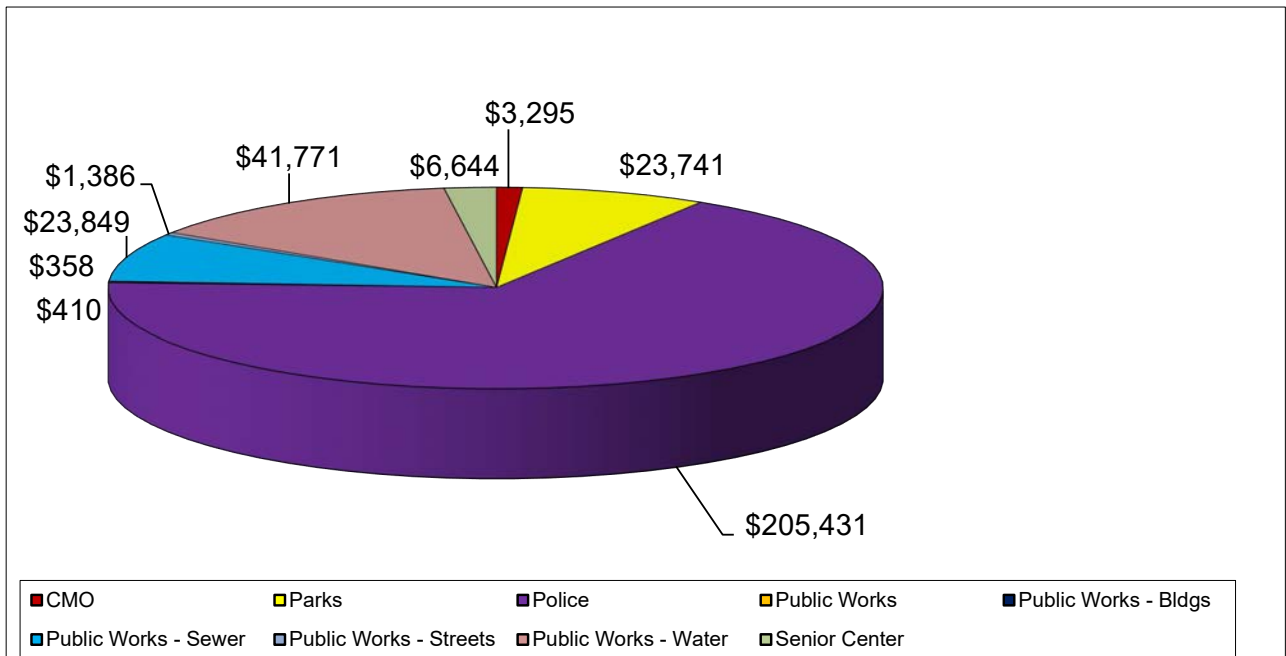
Claim Frequency



Insurer	Policy Start Date	Policy End Date	ER Mod	Std Premium	Payroll	Claim Count	Expected Claim Count	TL Claim Count	Expected TL Claim Count	TL Day Count	Incurred Losses	Paid Losses	Inc / Std Loss Ratio	Pd / Std Loss Ratio	Claim Freq Rate
SAIF	7/1/2018	6/30/2019	0.87	\$ 76,389	\$ 6,190,898	6	7.37	0	2	0	\$ 19,487	\$ 19,487	26%	26%	0.10
SAIF	7/1/2019	6/30/2020	0.74	\$ 71,315	\$ 6,659,041	5	7.84	1	2.26	5	\$ 26,628	\$ 26,628	37%	37%	0.08
SAIF	7/1/2020	6/30/2021	0.75	\$ 75,122	\$ 7,172,451	9	7.87	6	2.26	194	\$ 153,572	\$ 132,470	204%	176%	0.13
SAIF	7/1/2021	6/30/2022	0.82	\$ 89,513	\$ 7,552,125	8	8.17	2	2.33	6	\$ 20,430	\$ 20,430	23%	23%	0.11
SAIF	7/1/2022	6/30/2023	1.18	\$ 122,885	\$ 8,211,735	4	7.49	2	2.12	91	\$ 80,033	\$ 37,306	65%	30%	0.05
SAIF*	7/1/2023	6/30/2024	1.20	\$ 39,357	\$ 2,560,247	2	2.09	1	0.58	1	\$ 15,805	\$ 5,086	40%	13%	0.08

*Std Premium and Payroll for 2023-2024 is prorated.

Workers' Comp - By Department 7/01/18 to 10/13/2023



Department	Incurred \$	%
CMO	\$ 3,295	1.07%
Parks	\$ 23,741	7.74%
Police	\$ 205,431	66.94%
Public Works	\$ 410	0.13%
Public Works - Bldgs	\$ 358	0.12%
Public Works - Sewer	\$ 23,849	7.77%
Public Works - Streets	\$ 1,386	0.45%
Public Works - Water	\$ 41,771	13.61%
Senior Center	\$ 6,644	2.16%
Total	\$ 306,885	100.00%

Loss amounts shown include reserves.

Presentation

Republic Services

Julie Jackson
Municipal & Community Relations Manager



Sustainability in Action

October 1, 2023

Kenneth Jackola, Mayor
Lebanon City Council
Lebanon City Staff

Dear Mayor Jackola, Council and Staff,

This report provides information necessary to calculate the annual Refuse Rate Index (RRI) effective January 1, 2024. As stated in the Rate Modification and Rate Adjustment Statement this calculation is based on the Consumer Price Index, fuel cost and disposal costs. Based on these factors, the calculated adjustment for January 1, 2024, is 1% as shown below:

Republic Services - City of Lebanon RRI

	Index: June 30, 2022	Index: June 30, 2023	% Change	Weight	Refuse Index
CPI - West Size Class B/C All Urban Consumers	178.52	187.354	4.9%	65%	3.2%
CPI - West Coast Ultra Low Sulfur Diesel	6.48	4.415	-31.9%	10%	-3.2%
Coffin Butte Disposal Rate (4.0% cap)	\$ 52.50	\$ 56.00	4.0%	25%	1.0%
				Rate Adjustment %	1.0%
					100.0%
				Adjustment Factor	101.0%

As expected, the reduction in fuel prices and a more moderate economy are driving this modest increase. Residential customers will see increases from .25 to .41 monthly, depending on cart size.

Last year at this time we spoke to the Council about a shortage of drivers but are pleased to now be fully staffed and are seeing a normal rate of turnover. Innovations in the way our drivers run their route continues to improve the job. An example is the tablet drivers now use to run the route, keep track of which carts are not out and track carts that are contaminated.



Sustainability in Action

In 2023, we agreed to a rate for low-income assistance. That program is now available to individuals who qualify for the City's assistance program.

Please feel free to contact me with any questions.

Best Regards,

Julie Jackson
Municipal Manager
Republic Services
jjackson6@republicservices.com
541-936-1334

Presentation

Lebanon Chamber of Commerce

Rebecca Grizzle, Director

10 October 2023



Dear Members of the City Council,

As the Chamber of Commerce for Lebanon, we are privileged to serve as stewards of its economic vitality and prosperity. In light of the current economic climate and the evolving needs of our community, we find it necessary to request an amendment to our existing contract with the city.

Our current contract, as agreed upon, allocates \$35,000 annually for the operation of the Visitors Center and \$50,000 for the administration of tourism grants. While we have strived to efficiently manage these funds, we are faced with significant cost increases that make it increasingly challenging to fulfill our mission effectively and efficiently.

To continue providing the level of service our community deserves and demands, we kindly request an adjustment to our contract terms as follows:

1. An increase in funding for the operation of the Visitors Center from \$35,000 to \$42,000 per year.
2. An decrease in funding for the administration of tourism grants from \$50,000 to \$43,000 per year.

It is crucial to emphasize that these changes will not result in any additional financial burden for the city. The total funding allocated to the Chamber of Commerce will remain the same. Rather, this amendment will allow us the operational flexibility needed to manage these responsibilities effectively and, in turn, better serve our community.

We believe that these amendments will enable us to fulfill our mission to the best of our abilities and adapt to the changing economic landscape of our city.

We look forward to discussing this request further and working together to ensure the prosperity and vitality of our wonderful city.

A handwritten signature in black ink, appearing to read "Rebecca Grizzle". The signature is fluid and cursive, with a large initial "R" and "G".

Rebecca Grizzle

President CEO

Agenda Item 1



925 S. Main Street
Lebanon, Oregon 97355

TEL: 541.258.4906
cdc@ci.lebanon.or.us
www.ci.lebanon.or.us

MEMORANDUM

Community Development

To: Mayor Jackola and City Council
From: Kelly Hart, Community Development Director
Subject: Comprehensive Plan Map and Zoning Map Amendments
Planning File No.: CPMA-23-03, ZMA-23-03
Applicant: Sona Athwal

Date: October 19, 2023

This is a request to amend the Comprehensive Plan Map and Zoning Map designation for a 1.11 acre parcel located on the northwest corner of Airport Road and Stoltz Hill Road. The site address is 911 Airport Road. The County Assessor Map places the parcel within Township 12 South; Range 2 West; Section 15BD; Tax Lot 1000.

The property is located within the Lebanon Urban Growth Boundary (UGB) and city limits and is currently designated Residential Mixed Density in the Lebanon Comprehensive Plan and Zoning Maps. The Applicant is requesting to amend the Comprehensive Plan Map and Zoning Map designation to Mixed Use (C/Z-MU).

The Planning Commission conducted a public hearing on October 18, 2023, and found the application complied with the decision criteria contained in the Lebanon Development Code, voted to recommend City Council approval of the application. The Planning Commission record as been included as an attachment to this agenda item which includes the basis for the decision. Included under Planning Commission consideration was an Administrative Review and Conditional Use Permit to allow for the development and operation of a convenience store and gas station. The Planning Commission approved the development applications, contingent upon approval of the Comprehensive Plan and Zoning Map Amendments.

Included for review is the proposed Ordinance including the findings in support of the application as approved by the Planning Commission and the Planning Commission agenda packet including the entire application record.

It is the recommendation of the Planning Commission that the City Council approve the application for the Comprehensive Plan Map and Zoning Map Amendments to designate the property in the Comprehensive Plan as Mixed Use (C-MU) and the Zoning designation of Mixed Use (Z-MU).

A BILL FOR AN ORDINANCE AMENDING THE) ORDINANCE BILL NO. 2023-20
LEBANON COMPREHENSIVE PLAN MAP AND)
ZONING MAP DESIGNATION TO ESTABLISH)
THE MIXED USE DESIGNATION FOR THE 1.11)
ACRE PROPERTY(12S-02W-15BD, TAX LOT 1000)) ORDINANCE NO. 3015
File CPMA-23-03, ZMA-23-03; Sona Athwal)

WHEREAS, the City of Lebanon has received a submission by written request to amend the Comprehensive Plan Map and Zoning Map designation from Residential Mixed Density (C/Z-RM) to Mixed Use (C/Z-MU) for the real property herein described in Exhibit “A”; and

WHEREAS, on October 18, 2023, the Planning Commission for the City of Lebanon conducted a hearing on Planning File No. CPMA-23-03, ZMA-23-03, AR-23-04 and CU-23-03, making findings recommending establishment of the Mixed Use (C/Z-MU) comprehensive plan and zoning map designation; and

WHEREAS, after conducting the hearing and considering all objections or remonstrance regarding the proposed map amendments, and further considering the recommendation of the Lebanon Planning Commission, the City Council finds that this Comprehensive Plan and Zoning Map amendment is in the best interest of the City.

NOW, THEREFORE, the City of Lebanon ordains as follows:

Section 1. Findings. In addition to the findings referred to above and the Planning Commission record, the City Council further adopts and finds those matters contained in Exhibit “B” which is incorporated herein by this reference as if fully set forth at this point.

Section 2. Comprehensive Plan Map and Zoning Map Amendments. Based upon the findings adopted herein, the Lebanon Comprehensive Plan Map and Zoning Map are hereby amended, such that the property herein described in Exhibit “A” shall be designated Mixed Use (C/Z-MU).

Passed by the Lebanon City Council by a vote of _____ for and _____ against and approved by the Mayor this 8th day of November 2023.

CITY OF LEBANON, OREGON

Kenneth E. Jackola, Mayor
Michelle Steinhebel, Council President

Attested:

Julie Fisher, City Recorder

EXHIBIT "A"
PROPERTY SUBJECT TO COMPREHENSIVE PLAN MAP AND ZONING
AMENDMENT

911 Airport Road, Lebanon, OR 97355

The County Assessor Map places the parcel within Township 12 South; Range 2 West;
Section 15BD; Tax Lot 1000

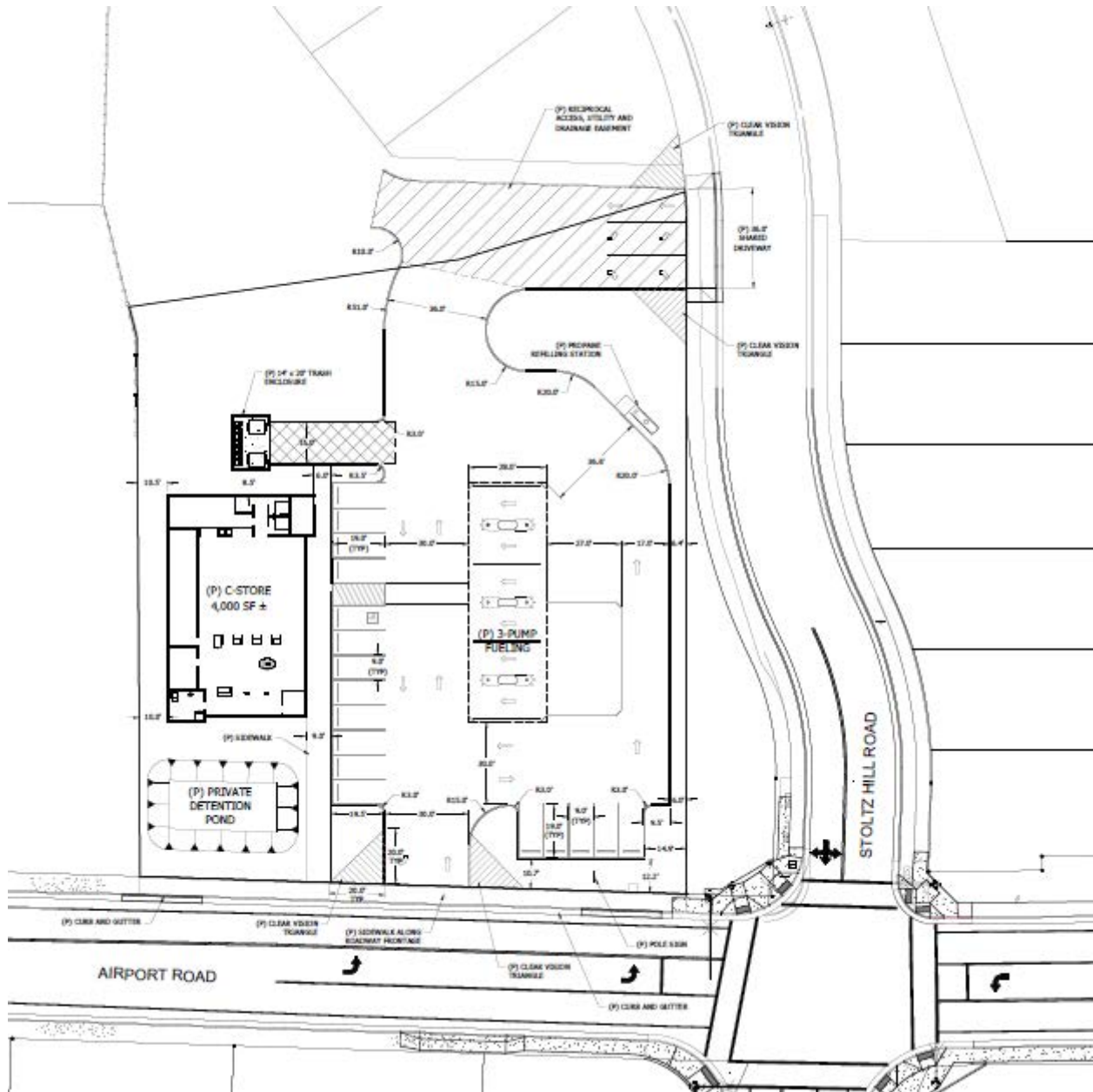


EXHIBIT B
LEBANON CITY COUNCIL FINDINGS

I. NATURE OF THE APPLICATION

This matter comes before the Lebanon City Council on the application of Sona Athwal with consideration of two applications: (1) Comprehensive Plan Map Amendment and (2) Zoning Map Amendment to change the property designation from Residential Mixed Density to Mixed Use.

II. GENERAL INFORMATION

A. Site Location

The subject property is located on the northwest corner of Airport Road and Stoltz Hill Road. The site address is 911 Airport Road. The County Assessor Map places the parcel within Township 12 South; Range 2 West; Section 15BD; Tax Lot 1000.

B. Adjacent Zoning and Land Uses

The property is in a generally developed neighborhood. To the north, west and east are properties within city limits, zoned Residential Mixed Density and improved or planned for improvement of single-family attached, detached, and duplex units, and at the northern terminus of Stoltz Hill Road is an apartment complex. To the south are properties within city limits and county, including single-family detached dwellings and a convenience store at the southwest corner.

C. Applicant Proposal

The Applicant is requesting authorization to amend the comprehensive plan and zoning map designations from Residential Mixed Density (C/Z-RM) to Mixed Use (C/Z-MU) to allow for the application of a Conditional Use Permit to develop and operate a gas station, and an Administrative Review to build a convenience store. The Planning Commission reviewed and approved the Administrative Review and Conditional Use Permit, contingent upon approval of the designation amendments.

III. PUBLIC HEARING

A. Planning Commission Action

On November 8, 2023 the Lebanon City Council held a public hearing on this application. At the hearing, Planning File CPMA-23-03 and ZMA-23-03 were made a part of the record. The City noticed the hearing pursuant to Chapter 16.20 of the Lebanon Development Code. No objection was raised as to jurisdiction, evidence or testimony presented at the hearing. At the end of the hearing, the Planning Commission deliberated on the issue and voted to recommend the City Council approve the proposed Comprehensive Plan and Zoning Map Amendments. The Commission found the proposal consistent with the applicable decision criteria. The Planning Commission also voted to approve the Administrative Review and Conditional Use Permit (contingent on City Council approval of CPMA-23-03 and ZMA-23-03) applications, finding the proposals consistent with the applicable decision criteria.

B. City Council Action

On October 18, 2023 the Lebanon City Council held a public hearing on this application. At the hearing, Planning File CPMA-23-03 and ZMA-23-03 were made a part of the record. The City noticed the hearing pursuant to Chapter 16.20 of the Lebanon Development Code. No objection was raised as to jurisdiction, evidence or testimony presented at the hearing. At the end of the hearing, the City Council deliberated on the issue and voted to approve the proposed Comprehensive Plan and Zoning Map Amendments. The Council found the proposal consistent with the applicable decision criteria.

IV. FINDINGS OF FACT-GENERAL

The Lebanon City Council, after careful consideration of the testimony and evidence in the record, adopts the following General Findings of Fact:

- A. The applicant is Sona Athwal.
- B. The subject property is located on the northwest corner of Airport Road and Stoltz Hill Road. The site address is 911 Airport Road. The County Assessor Map places the parcel within Township 12 South; Range 2 West; Section 15BD; Tax Lot 1000.
- C. The total area contains 1.11 acres.
- D. The subject parcel may be accessed from Airport Road and Stoltz Hill Road. The site is currently improved with a single-family dwelling, which would be razed to accommodate the development.
- E. The land is currently located within city limits and designated Residential Mixed Density (C/Z-RM).
- F. The property is in a generally developed neighborhood. To the north, west and east are properties within city limits, zoned Residential Mixed Density and improved or planned for improvement of single-family attached, detached, and duplex units, and at the northern terminus of Stoltz Hill Road is an apartment complex. To the south are properties within city limits and county, including single-family detached dwellings and a convenience store at the southwest corner.
- G. The Applicant is requesting authorization amend the comprehensive plan and zoning map designations from Residential Mixed Density (C/Z-RM) to Mixed Use (C/Z-MU) to allow for the application of a Conditional Use Permit to develop and operate a gas station, and an Administrative Review to build a convenience store.
- H. The Community Development Department contacted the Department of Land Conservation and Development, affected agencies and area property owners regarding the application. No comments were submitted.
- I. The decision to approve or deny shall be based on criteria contained in the Lebanon Development Code, Chapter 16.28 – Comprehensive Plan Map, Zoning Map, and Urban Growth Boundary Amendments, and Chapter 16.20 – Review and Decision Making Procedures, and Chapter 16.21 – Conditional Uses.

V. REVIEW CRITERIA AND FINDINGS –
COMPREHENSIVE PLAN AND ZONING MAP AMENDMENTS

The Applicant is requesting consideration of a comprehensive plan and zoning map amendment to change the designation from Residential Mixed Density to Mixed Use. Section 16.27.050 of the Lebanon Development Code identifies the criteria in which to consider amendments to the Comprehensive Plan Map and Zoning Map. The criteria are the same for each amendment, as such, the findings have been combined:

1. All proposed amendments to the Comprehensive Plan Map or to the Zoning Map shall be consistent with the City of Lebanon’s adopted Comprehensive Plan and Facility Plans. The City’s Facility plans, including the Transportation System Plan (TSP), are based on the future site service demands according to the Comprehensive Plan Map designation and associated zoning.

The Comprehensive Plan consists of ten Chapters with each Chapter addressing specific land use issues. The applicable policies in each Chapter are reviewed below:

- a. Chapter 1: Introduction - The introductory Chapter describes the Comprehensive Plan, its relationship to the Statewide Land Use Goals, the Citizen Involvement program and key terminology. Goals and policies relate to the organization of the Plan, the continued need for citizen involvement and the relationship of the Plan to State law and implementing codes. These goals and policies are incorporated in the Development Code criteria to determine the appropriateness of a Plan and/or zone change.
- b. Chapter 2: Natural Environment – The Chapter address goals and policies related to the City’s natural environment.

FINDING: The subject property is not located within a designated flood hazard area, does not contain any identified natural or historic resource, and is not otherwise in an area subject to increased risk to other natural hazards. Therefore, none of the goals or policies in Chapter 2 are found to be directly implicated by this application. Upon any development of the site, the property owner is responsible for complying with state law, including compliance with the State Historic Preservation Office (SHPO).

- c. Chapter 3: Urbanization – This Chapter provides the basic framework for future urban development within the City. It is found the following policies apply:

G-1: Providing for an orderly and efficient transition from rural to urban land use.

P-3: Support a flexible phased program for the orderly extension of water, wastewater, storm drainage and transportation services in response to land development proposals.

P-10: Review all development proposals to ensure that public facilities are available and have adequate capacity to accommodate the proposed development, or that such facilities and their capacities can be made available through appropriate extensions and/or enhancements concurrent with or prior to proposed developments.

P-30: Manage its Urban Growth Boundary and the lands within so as to make available

sufficient land for the various uses to ensure choices in the marketplace, through implementation of land use regulations and land use policies.

P-31: Manage its Urban Growth Boundary and the lands within so as to encourage development within urban areas before conversion of urbanizable areas, through implementation of land use regulations and land use policies.

FINDING: "Urbanizable Land" is defined in the Comprehensive Plan Glossary. Generally, Urbanizable Lands are those unincorporated areas in the UGB for which a city and county coordinate on assuring will remain available for expansion of the city over the planning period. Mutually adopted urban growth boundary agreements typically govern how and when urbanizable land can be converted to urban use. The subject property, however, is already in the municipal boundary and currently zoned for full urban development under the Residential Mixed Density zone and has previously been developed to urbanized standards. Accordingly, it is found that the subject property is already urban land by existing zoning rather than urbanizable land to be converted to urban use for which Policies P-30 and P-31 would apply. Those policies are therefore not implicated in this case.

- d. Chapter 4: Land Use – This Chapter details the goals and policies to assure the City provides different types of land within City limits that are suitable for a variety of uses. It is found the following policies apply:

P-1: Recognize that the Comprehensive Plan land use designations or categories shall determine zoning.

P-6: Require that changes to the Comprehensive Plan Map be consistent with the policies of the Comprehensive Plan, State law, and any adopted intergovernmental agreements.

P-12: Ensure that the Zoning Map reflects and implements the Comprehensive Plan Map.

P-27: Allow future commercial and mixed-use centers within the Mixed Density Residential Zone based upon an approved development plan in conformance with the Planned Development standards and procedures of the Zoning Ordinance, provided access is from a designated highway or arterial and urban services are available.

P-40: Encourage a mix of commercial and residential uses within individual buildings, lots, and neighborhoods, in order to promote a compact, pedestrian friendly environment. Industrial uses should be allowed to mix with residential and commercial uses where there is limited potential for nuisance or jeopardy to the public health, safety, and welfare.

FINDING: The review process, evaluation, and determination of compliance with the Comprehensive Plan as part of the review process addresses compliance with Comprehensive Plan policies (P-6). The State effectively acknowledged the Comprehensive Plan, therefore, conformance with these policies assures conformance with state law. Compliance with Statewide Goals will be reviewed in another finding. For the subject property, the site is currently limited for residential opportunity and

development, and the change to the Mixed-Use designation would allow for a greater opportunity of a mix of commercial and residential uses within the neighborhood (P-40).

- e. Chapter 5: Economic Development – This Chapter addresses population growth and economic development as well as those trends affecting both.

P-1: Ensure that Lebanon has enough land zoned to provide industrial and commercial opportunities, periodically monitoring land development, and updating the land use map to ensure that there is enough land in each designation to accommodate expected growth.

A-1: Monitor development in commercial and industrial areas. As development or rezoning occurs, evaluate whether the city continues to have enough commercial and industrial land to support employment growth

A-4: : Evaluate opportunities to rezone land to allow for more and smaller scale commercial uses around Airport Road and Grant Street.

FINDING: The 2023 Economic Opportunity Analysis (EOA) completed a buildable lands inventory that determined the city had sufficient land for commercial, industrial, and mixed use lands. However, the opportunities analysis identified Airport Road and Grant Street as high quality transportation corridors that could provide opportunities to support more commercial development. Action Item #4 in the adopted Policies and recommended actions memo from ECONorthwest as part of the EOA, specifically identifies evaluating opportunities to rezone land to allow for more smaller scale commercial uses around Airport Road. The rezoning of the subject property to provide commercial activity directly aligns with this goal.

- f. Chapter 6: Housing – This Chapter establishes the City’s Goals and Policies related to Housing.

FINDING: This proposed amendment would change the comprehensive plan and zoning map designation from residential mixed density to mixed use. The amendment would result in a reduction of land designated for housing opportunities by 1.11 acres. In the 2019 Housing Needs Analysis, the City identified the projected housing needs over a 20-year planning period, and conducted a buildable land inventory to identify whether there was sufficient land in the urban growth boundary to accommodate the housing need. Based on the results of the analysis, a total of 2,503 housing units are identified as the 20-year housing need, and the city has 240 acres of low-density land surplus, 0 acres of medium density land surplus, and 63 acres of high-density land surplus. A such, the reduction of 1.11 acres of land that may more readily accommodate housing would not result in an impact to the City’s ability to meet the long range housing needs for the community.

Chapter 7: Community Friendly Development & Preservation of Historic Resources - This Chapter focuses on policies creating a built environment suitable for the needs of a diverse population through a variety of uses scaled for the pedestrian, and capable of accommodating the automobile and mass transit. In addition, the Chapter focuses on historical preservation. The focus on Community Friendly design is associated with infill development, promotion of mixed-use opportunities, and development specific

standards. In addition, there are no historical sites on the property, as such, the historical preservation goals are not applicable.

G-6: Developing sidewalks, crosswalks, and multi-use paths that not only meet ADA standards, but also enhance pedestrian and bicycle friendly environment throughout the community.

FINDING: As part of the development, improvements to the sidewalk along Airport Road would be completed, including a multiuse path to the east of the development, enhancing the walkability of the neighborhood. In addition, a signal is to be installed at the intersection of Airport Road and Stoltz Hill Road, increasing the safety of pedestrian and bicycle movement through the neighborhood.

- g. Chapter 8: Transportation – This Chapter addresses the transportation needs of the City with an emphasis of creating a variety of transportation options for pedestrians, bicyclists, vehicles and mass transit. It is found the following policies apply:

P-12: The transportation system shall be managed to reduce existing traffic congestion and facilitate the safe, efficient movement of people and commodities within the community.

FINDING: The subject site is located to the northwest of the West Airport Road and Stoltz Hill Road intersection at 911 West Airport Road. West Airport Road and Stoltz Hill Road are classified in the 2018 Lebanon Transportation System Plan (TSP) as a Minor Arterial and Collector streets, respectively. A future street project (no. D36) is identified in the 2018 Transportation System Plan (TSP). Upon its completion, the project will realign Stoltz Hill Road and add an intersection traffic signal. The proposed amendments will not impact the planned intersection improvement project. Additionally, Section 6.12.010(B)(1) includes thresholds for when Traffic Impact Studies (TIS) and Traffic Impact Analysis (TIA) are warranted, which is typically in association with a development proposal. A TIA based on the proposed development was found to be warranted. The TIA concluded a number of off-site improvements required, which have been conditioned as part of the concurrent development proposal. With the completion of the TIA and conditioning of the off-site improvements, it is concluded that the proposal complies with Chapter 8.

- h. Chapter 9: Public Facilities and Service - The City is required by State law to plan and develop a timely, orderly, and efficient arrangement of public facilities and services to serve as a framework for urban development. Goals and policies in this Chapter address those requirements. It is found the following policies apply:

General Policies

P-8: Review all development proposals to ensure that public facilities are available and have adequate capacity to accommodate the proposed development, or that such facilities and their capacities can be made available through appropriate extensions and/or enhancements concurrent with or prior to proposed developments. (Duplicated in Chapter 3, Urbanization)

P-9: Require that new developments are either served by existing and/or proposed public infrastructure improvements, and/or are served by privately funded

infrastructure extensions and improvements. (Duplicated in Chapter 3, Urbanization)

P-10: Consider impacts on key City-provided urban utility services (water, storm drainage, wastewater, and streets) and any other community facilities that are identified by service providers as substantially impacted by the proposal before development proposals, or rezoning applications are approved.

FINDING: City water service and storm drainage are available in Airport Road along the frontage of the subject property. City Sewer service is available in Stoltz Hill Road along the frontage of the subject property. These facilities would be able to accommodate any development opportunity of the subject property. As such, there is sufficient utility infrastructure to service the site, and therefore, the proposal complies with Chapter 9.

i. Chapter 10: Plan Implementation, Amendment, and Land Use Planning Coordination – This Chapter establishes procedures for amending the Comprehensive Plan Map and Zoning Map. Specific applicable policies include:

P-1: The City Council may amend the Comprehensive Plan and/or Map after referral to the Planning Commission public hearing, for action, review, revisions, and recommendations.

P-2: Changes to the Plan and/or Map shall be made by ordinance after public hearings as prescribed by state law and local ordinances.

P-3: Changes in the Plan and/or Map shall be incorporated directly into the document at the appropriate place. A list of all amendments with date of passage shall then become part of the document until the next comprehensive update of the entire Comprehensive Plan.

P-4: An amendment to the Comprehensive Plan and/or Map may be considered when one or more of the following conditions exist:

- a. Updated data demonstrates significantly different trends than previous data;
- b. New data reflects new or previously undisclosed public needs;
- c. New community attitudes represent a significant departure from previous attitudes as reflected by the Planning Commission or City Council;
- d. Statutory changes significantly affect the applicability or appropriateness of existing plan policies.

P-5: Residents, property owners, their authorized agents, the Planning Commission, the City Council, or City staff may initiate a Comprehensive Plan amendment. In order to obtain a Comprehensive Plan and/or Map amendment, the applicants shall have the burden of proof that all of the following conditions exist:

- a. There is a need for the proposed change;
- b. The identified need can best be served by granting the change requested;
- c. The proposed change complies with the Statewide Planning Goals; and,
- d. The proposed change is consistent with all other provisions of the City's Comprehensive Plan.

FINDING: Policies P-1 through P-3 relate to amendment procedures and do not

function as approval criteria. Regarding P-4, a number of recent changes to city policy and updates to portions of the comprehensive plan provide new data and community attitude as reflected by the Planning Commission and City Council. As part of the 2018 update to the City's Transportation System Plan, the transportation bypass system which would have routed significant traffic around the city to the southern portion from Hwy 34 was removed from the plan. With the bypass no longer considered, the existing transportation system allows travelers to bypass the northern portion of the city from Hwy 34 using Denny School Road which turns into Airport Road to connect to Hwy 20. Airport Road is a significant transportation corridor for access in and out of the city to avoid the northern traffic congestion formed by the Hwy 20 couplet. As such, with the change in TSP, and Airport Road being a significant transportation corridor, amending the land use designation to allow for additional commercial and vehicle service related uses may be appropriate.

In addition, in 2021, the Planning Commission considered the development code as it related to gas (fueling) stations and determined it would be appropriate to expand the opportunities for gas stations beyond the previous zoning limited to Highway Commercial. Specifically, the Planning Commission identified that intersections of roadways with classifications of arterial or arterial and collector are appropriate locations as they are designed to accommodate the traffic volume and circulation. Ultimately, an amendment to the development code was approved by the Planning Commission and City Council expanding opportunities for gas stations in the city.

Finally, in September 2023, the City adopted an Economic Opportunity Analysis (EOA) and updated the comprehensive plan goals, policies, and actions items. One action item states, "evaluate opportunities to rezone land to allow for more smaller scale commercial uses around Airport Road and Grant Street." This action item, with consideration of Airport Road as a significant transportation corridor further identifies a new community attitude that represents a significant departure from previous attitudes as reflected by the Planning Commission and City Council, establishing the ability to amend the comprehensive plan.

Regarding P-5a.b., the proposed land use designation amendment is needed and is best served by granting the amendment to be able to accommodate the EOA action item to allow for rezoning on Airport Road to offer smaller scale commercial uses. The subject property, being located at the intersection of an arterial and collector road, with the intersection soon to be signalized, increases the commercial viability of the property, identifying the site as well suited to accomplish the EOA goals.

Findings here above and below further establish that the change complies with the Statewide Planning Goals and the proposed change is consistent with all other provisions of the City's Comprehensive Plan.

Compliance with the Statewide Goals (P-5.c) is noted as follows:

Goal 1, Citizen Involvement: Public hearings will be held before the Planning Commission on October 18, 2023 and the City Council on November 8, 2023. This is consistent with City procedures, state law, and the intent of the Goal.

Goal 2, Land Use Planning: The proposal does not involve exceptions to the Statewide Goals. Adoption actions are consistent with the acknowledged Comprehensive Plan

and Development Code.

Goal 3, Agricultural Lands: This Goal does not apply, as the land is not designated farmland.

Goal 4, Forest Lands: This Goal does not apply, as the land is not designated forestland.

Goal 5, Open Spaces, Scenic and Historic Areas, and Natural Resources: The site has not been designated for open space, a scenic area, or known for having natural resources. As such, this goal is not applicable. Upon any development of the site, the property owner is responsible for complying with state law, including compliance with the State Historic Preservation Office (SHPO).

Goal 6, Air, Water and Land Resource Quality: the map designation would change from a residential mixed density designation to mixed use. With the designation change, there are properties in the vicinity with the new designation, and the mixed use zone maintains similar land use types that are permissible in the residential mixed density designation, with an expanded opportunity for additional commercial activities and industrial activities. The use proposed associated with the requested zone change does not include idling vehicles impacting emissions, the development would be graded to ensure historical drainage patterns are maintained to avoid flooding issues meeting water resource considerations, and the land is not suitable for quality farming or timber and is an urbanizable property, therefore, there should be no significant impacts on air, water or resource quality than would otherwise occur.

Goal 7, Natural Hazards: The site is not located within any designated flood plain, does not contain steep slopes or contain any other identified natural hazard.

Goal 8, Recreational Needs: The proposed map amendments do not reduce the land designated for recreational needs, nor create uses which would adversely impact recreational opportunities.

Goal 9, Economic Development: The goal is to provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare, and prosperity of Oregon's citizens. The proposed change will be result in an increase in employment lands along Airport Road, which is in accordance with the City's adopted Economic Opportunities Analysis. LCDC implementing rules at OAR Chapter 660, Division 9, are not implicated by changes of less than two acres of employment land designations.

Goal 10, Housing: This proposed amendment would change the comprehensive plan and zoning map designation from residential mixed density to mixed-use. Residential uses are permissible in the mixed-use zone; however, the concurrent development proposal does not include residential uses. The amendment would result in a reduction of land designated for housing opportunities by 1.11 acres. In the 2019 Housing Needs Analysis, the City identified the projected housing needs over a 20-year planning period, and conducted a buildable land inventory to identify whether there was sufficient land in the urban growth boundary to accommodate the housing need. Based on the results of the analysis, a total of 2,503 housing units are identified as the 20-year housing need, and the city has 240 acres of low-density land surplus, 0 acres

of medium density land surplus, and 63 acres of high-density land surplus. As such, the reduction of 1.11 acres of land that may more readily accommodate housing would not result in an impact to the City's ability to meet the long range housing needs for the community. As such, this proposal would comply with Goal 10.

Goal 11, Public Facilities and Services: The goal is to plan and develop a timely, orderly and efficient arrangement of public facilities and services to serve as a framework for urban and rural land. The subject property is urban land that is already fully served by urban public facilities and services.

Goal 12, Transportation: Previous findings indicate the map amendments will not significantly affect planned transportation improvements.

Goal 13, Energy Conservation: The map amendments are neutral regarding energy conservation.

Goal 14, Urbanization: Previous findings indicate the change will still allow the city to meet housing needs of the community, as well as not impact the ability to provide industrial opportunities, and expands commercial opportunity along Airport Road, which has been specifically identified as a goal in the Economic Opportunity Analysis. The land is already within the urban growth boundary and city limits, thus eligible for urbanization.

Goals 15 to 19, Willamette River Greenway, Estuarine Resources, Coastal Shores, Beaches and Dunes, Ocean Resources: The proposals do not involve land within the Willamette Greenway or coastal areas.

Finally, all previous findings indicate the proposal complies with the applicable policies of the City's Comprehensive Plan (P-5.d).

policies of the City's Comprehensive Plan (P-5.d).

1. Facility plans need to be consistent with the Comprehensive Plan Map as well as the text, and changes to the Map may necessitate changes to a facility plan. For example, changing a Comprehensive Plan Map designation to a higher intensity use may require an amendment to the TSP, sanitary sewer, or potable water master plans.

FINDINGS: The property is currently designated as Residential Mixed Density, and the proposal includes a revised designation to Mixed Use. The Mixed-Use designation allows for a mix of uses including commercial, residential, and light industrial. The property is approximately 1.12 acres. The property is currently utilized as a residential development, and potential uses for the property under the Mixed Use designation would be similar commercial uses, or small residential uses. Based on the size of the property, and the similar uses permitted and feasible on the property, the existing facility plans would not require amendment.

In addition, as part of the Transportation System Plan (TSP), there is a designated intersection improvement project to align Stoltz Hill Road and to add a traffic signal. The proposed map amendments would not result in a change to the TSP project or necessitate amendments to existing facility plans as the allowed and proposed uses are in line with the

existing designation in terms of demand on facilities and would not impact the planned transportation project from moving forward.

2. Applicants proposing amendments to the Zoning Map must request a City Zoning Classification that is consistent with the Comprehensive Plan Map designation for a subject property. If an applicant requests a City Zoning Classification that is not consistent with the Comprehensive Plan Map, the zoning requested shall not be granted until the Comprehensive Plan Map is first appropriately amended to reflect concurrence. (See the Annexation Zoning Matrix, Table 16.26-1.) Such an amendment requires a separate application, hearing and decision; this process may occur concurrently with the Zoning Map Amendment hearing.

FINDING: The proposal is to amend the Comprehensive Plan Map and Zoning Map. With the Comprehensive Plan Map amendment to the Mixed Use designation, the required zoning designation based on Table 16.26-1 of the Lebanon Development Code is Mixed Use, which is consistent with the proposal by the applicant. As such, the project complies with this decision criteria as the proposed zoning map amendment to Mixed Use is occurring with a concurrent comprehensive plan map amendment to Mixed Use.

3. Section 16.27.080.B states that if proposed amendments to the Comprehensive Plan Map or Zoning Map do not comply with the Comprehensive Plan, the Comprehensive Plan must first be amended so that the proposed Map amendment will be consistent with and accurately implement the Plan.

FINDING: Previous findings indicate the proposal may comply with the applicable Comprehensive Plan policies and does not require amendments to the Plan text. Therefore, this provision does not apply.

VII. CONCLUSION

The City Council concludes the proposed Comprehensive Plan and Zoning Map Amendments, complies with the applicable decision criteria.



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MEMORANDUM

Community Development

To: Lebanon Planning Commission Date: October 9, 2023
From: Kelly Hart, Community Development Director
Subject: Comprehensive Plan and Zoning Map Amendments, Administrative Review and Conditional Use Permit CPMA-23-03, ZMA-23-03, AR-23-04 and CU-23-03

I. BACKGROUND

The subject property is located on the northwest corner of Airport Road and Stoltz Hill Road. The site address is 911 Airport Road. The County Assessor Map places the parcel within Township 12 South; Range 2 West; Section 15BD; Tax Lot 1000. The property is currently within city limits and is designated in the comprehensive plan and zoned Residential Mixed Density (C/Z-RM).

The property is in a generally developed neighborhood. To the north, west and east are properties within city limits, zoned Residential Mixed Density and improved or planned for improvement of single-family attached, detached, and duplex units, and at the northern terminus of Stoltz Hill Road is an apartment complex. To the south are properties within city limits and county, including single-family detached dwellings and a convenience store at the southwest corner.

The site is currently developed with a single-family dwelling with property frontage on both Airport Road and the north leg of Stoltz Hill Road. The proposal would be to demolish the existing improvements and redevelop the property for the operation of a convenience store and gas station.

The project proposal consists of four applications are under consideration: (1) Comprehensive Plan Map Amendment and (2) Zoning Map Amendment to change the property designation from Residential Mixed Density to Mixed Use, (3) Administrative Review to construct a convenience store and (4) Conditional Use Permit for the construction and operation of a gas station.

The Comprehensive Plan and Zoning Map Amendment are legislative actions subject to consideration by the Planning Commission and City Council. The Administrative Review and Conditional Use permit are quasi-judicial hearings, subject to Planning Commission decision, but the conditional use permit is contingent upon approval of the comprehensive plan and zoning map amendments.

II. CURRENT REPORT

DEVELOPMENT AND USE PROPOSAL: The project proposal includes the demolition of existing improvements, and construction and operation of a convenience store and gas station. The permissible zones to operate a gas station include the Highway Commercial (Z-HCM), Neighborhood Mixed Use (Z-NMU), Industrial (Z-IND) and Mixed Use (Z-MU) zones. As such, to operate a gas station, a comprehensive plan and zoning map amendment is required. The applicant has proposed the Mixed Use zone as there are other properties within the vicinity of the site to the west that are zoned Mixed Use, and the Mixed Use zone is consistent with the overall character of the Airport Road corridor, which maintains a mix of public, residential, commercial, and industrial uses. The convenience store use is permissible in the current Residential Mixed Density zone, or the Mixed Use zone, subject to an administrative review.

For the Mixed Use zone, gas stations may be permitted on parcels with a minimum of 12,000 square feet, and at an intersection with the minimum of an Arterial (minor or principal) and Collector roadway classification. The subject property is 1.11 acres and is located at the northwest corner of the intersection of Airport Road and Stoltz Hill Road. Per the City's adopted Transportation System Plan (TSP), Airport Road is a Minor Arterial and Stoltz Hill Road at the intersection is a Collector Road. The north leg of Stoltz Hill Road was built after the TSP. However, it meets the standards for constrained roadway design for Collector Roads per the TSP. With a curb-to-curb width of 34-feet, this provides sufficient width for two 12-foot travel lanes, and parking on one side of the street. Per the constrained roadway design standards, the minimum lane width with shared roadway bike facilities is 12-feet, with 4.5 ft landscape strips, and 5-ft sidewalks. The northern leg of Stoltz Hill Road meets these design options for a constrained Collector per the TSP, and the typical street design standards found in Table 16.13.030-2 (Typical Street Design Standards) in Section 16.13.030 of the Lebanon Development Code (LDC).

The subject property is a corner lot, with frontage on both Stoltz Hill Road, and Airport Road. Per Chapter 16.32 of the LDC, a corner lot is "a lot at least two adjacent sides of which abut streets other than alleys, provided that the angle of the intersection of the adjacent streets does not exceed 135 degrees." For this parcel, the southern and eastern property lines abut the two intersecting streets, creating a corner lot. There is a small tract of land located along a portion of the eastern property line at the intersection, but it is identified as a tract (i.e., parcel held by the homeowner's association for open space, drainage, excess land that is not for development) associated with the neighboring subdivision. The existence of the tract, although at the exact intersection of the streets, does not diminish the subject property designation as a corner lot based on the definition as the subject parcel abuts the two streets. As such, the subject property is eligible for a gas station, upon comprehensive plan and zone change, based on the property being a corner lot, at the intersection of an arterial and collector roadway and over 12,000 square feet.

The development standards found in the Chapter 16.08 (Commercial Land Uses) zone shall be applied to commercial development in the Mixed Use zone. The convenience store development is permissible in the Neighborhood Commercial Zone (Z-NCM) and is consistent with the buffering and use impacts authorized in the Z-NCM zone. As such, the NCM zone standards are applied for the setbacks and building height for the convenience store. The gas station use

is authorized in the Highway Commercial (Z-HCM) zone, and therefore, the development standards for the gas station portion of the development shall be applied based on the Z-HCM zone.

For setbacks, with the zoning application as described above, the front setback (Airport Road) shall be 10-feet (20-feet abutting residential), streetside setback (Stoltz Hill Road) shall be 10-feet (20-feet abutting residential), the interior side (western property line) setback shall be 10-feet abutting residential, and the rear (northern property line) setback shall be 20-feet. The maximum building height authorized for the convenience store is 25-feet, and 45-feet for the gas station; and the minimum landscape area is the required front and street side setbacks. However, Section 16.14.040.A.2 authorizes parking areas (inclusive of drive aisles) to be within the front and streetside setbacks, if separated by at least a five-foot landscaped buffer outside the clear vision triangle.

The convenience store building is proposed to be setback 10-feet from the western interior side property line, approximately 65-feet from the front property lines, and approximately 80-feet from the rear property line. The fueling pump canopy for the gas station use is setback approximately 60-feet from the front property line, and 50-feet from the street side property line. Where the parking areas are proposed in the setbacks, a minimum 6.4-foot landscape planter is provided. The overall height of the convenience store and fuel canopy is 17-feet, which is less than the maximum authorized. With approximately 6,480 square feet of building coverage, the 15% lot coverage is within permissible limits.

For parking, there is no defined parking for a gas station. Per Table 16.14.070-1 in Section 16.14.070 of the LDC, retail/grocery stores require four parking spaces per 1,000 square feet of building area. The proposed convenience store is 4,000 square feet, requiring 16 parking spaces, and four bicycle parking spaces. A total of 17 parking spaces are proposed. The customers for the fueling station would generally park at the fueling pump island or share the parking areas for the convenience store, meeting the parking requirement.

SITE ACCESS AND TRAFFIC CONSIDERATIONS: The site is proposed to be accessed by two driveways, one on Airport Road, and one on Stoltz Hill Road. The Stoltz Hill Road access is located on the northern most corner of the property and is proposed to share driveway access with two flag lots associated with the residential subdivision to the north. A reciprocal access easement was recorded as part of the final plat between the properties during the re-plat for the subdivision through application S-23-01. The driveway is proposed to be improved to commercial driveway standard and include lane width to provide a dedicated right and left turn out of the driveway onto Stoltz Hill Road. The Airport Road driveway is proposed to be located near the center of the property width on Airport Road approximately 115 feet from the intersection. With the proximity to the intersection, the traffic movements would be restricted to only allow a “right-in, right-out” traffic movement.

The site access and traffic circulation were reviewed in the Traffic Impact Analysis (TIA) by Kittelson & Associates. The following are the required off-site improvements to address traffic impacts:

- The planned signalization project should be completed prior to the proposed convenience store/fueling facility being issued a certificate of occupancy.

- Construct a non-traversable raised median barrier along north edge of the eastbound Airport Road left-turn lane from the Stoltz Hill Road intersection to approximately 50 feet west of the proposed site driveway.
- In conjunction with the planned near-term signalization of the Airport Road/Stoltz Hill Road intersection to be delivered by the City of Lebanon, it is recommended that the signalization project include the following design elements pending availability of adequate right-of-way along the west side of Stoltz Hill Road:
 - Incorporate a separate left-turn lane with at least 50 feet of storage and a shared through/right-turn lane on the northbound Stoltz Hill Road approach to Airport Road.

With the off-site improvements identified in the TIA and required operation of the traffic signal prior to use occupancy, the site access and circulation would operate at an acceptable industry standard.

CONDITIONAL USE PERMIT MITIGATION: As part of the conditional use permit review, the use is evaluated regarding traffic, noise, vibration, exhaust/emissions, light glare, erosion, odor, dust, visibility, safety, and aesthetic considerations. Site design or conditions of development and use are incorporated to address any mitigation needs to ensure compatibility of operations with the surrounding uses.

- Access and traffic have been previously discussed and conditions have been incorporated.
- Noise generation is anticipated to be minimal with the loudest noise generation occurring from the delivery of products to the convenience store. The convenience store use is not subject to review of the conditional use permit. Noise generated from the fueling station use is anticipated to be customer conversation and vehicle traffic. There are no anticipated noise mitigation measures required. The overall property operation would be subject to the citywide noise ordinance.
- Vibration, erosion, and dust generation is not anticipated once the site is developed. During the periods of construction, intermittent vibrations may occur, and dust may be generated. Construction best management practices are required at all construction sites in the city to mitigate construction dust, off-site impacts, noise, erosion, and vibration.
- Exhaust and Emissions are expected to be higher during the construction period with a higher propensity for truck idling associated with construction equipment. This is a temporary impact. The convenience store and gas station are not anticipated to produce significant exhaust and emissions beyond other commercial uses. Both uses involve customers entering the site with vehicles, internal circulation, and parking. The vehicles are turned off during times which the customers are utilizing the site services. These actions are similar to other commercial uses that do not include a drive-up window. No anticipated mitigation measures are required for the exhaust and emissions.
- Light glare and visibility are items of consideration for the gas station use. With the canopy structure illuminating an outdoor area, the site will generate more light from the property than uses that do not include outdoor uses. In addition, the vehicle headlights while moving through the property would extend past property lines. To address lighting impacts, Section 16.06.10.A.3 of the LDC requires canopy lighting fixtures to be fully within the canopy structure and not extend below the canopy ceiling, and to meet the

exterior lighting standards in Section 16.19.050 which require exterior lighting to be designed, constructed, located, shielded, and directed in such a manner so as to not face directly, shine, reflect, or glare onto adjacent residences, streets and other land uses, and the lumens should not exceed 0.1 at the property line. A photometric map has been conditioned as part of the building plan submittal to verify the lighting design meets these requirements. In addition, landscaping conditions have been incorporated to require mature shrubs to be planted along the southern and eastern property lines, or other design treatment, outside the vision clearance areas, to block headlight lights from extending beyond the property line. Finally, per the applicant narrative, the site would be closed between 11:00pm and 5:30am, limiting nighttime lighting impacts.

- Visibility and Safety considerations include ensuring the vision clearance and site distance triangles remain clear of any visual impairments. The business owner is also encouraged to coordinate with the Police Department to discuss community policing through environment design (CPTED) concepts and on-site security measures.
- Aesthetic considerations include landscape improvements and lighting considerations. The City does not have any design standards, and the site is over one acre and not subject to infill design evaluation. High screen landscaping has been conditioned for the western and northern property boundary when adjacent to residential dwellings, and specific landscape treatments are required for the front and streetside landscape areas to address lighting mitigation.

Based on the nuisance considerations, as identified, several conditions of development and use have been included to appropriately mitigate the use.

COMPREHENSIVE PLAN AND ZONING MAP AMENDMENTS: The subject property is currently designated and zoned C/Z-RM (Residential Mixed Density). The applicant is proposing to amend the designation to Mixed Use. Generally speaking, per the comprehensive plan, a map amendment may be considered when one or more of the following conditions exist:

1. Updated data demonstrates significantly different trends than previous data
2. New data reflects new or previously undisclosed public needs
3. New community attitudes represent a significant departure from previous attitudes as reflected by the Planning Commission or City Council
4. Statutory changes significantly affect the applicability or appropriateness of existing plan policies.

A number of recent changes to city policy and updates to portions of the comprehensive plan provide new data and community attitude as reflected by the Planning Commission and City Council. As part of the 2018 update to the City's Transportation System Plan, the transportation bypass system which would have routed significant traffic around the city to the southern portion from Hwy 34 was removed from the plan. With the bypass no longer considered, the existing transportation system allows travelers to bypass the northern portion of the city from Hwy 34 using Denny School Road which turns into Airport Road to connect to Hwy 20. Airport Road is a significant transportation corridor for access in and out of the city to avoid the northern traffic congestion formed by the Hwy 20 couplet. As such, with the change in TSP, and Airport Road being a significant transportation corridor, amending the land use designation to allow for additional commercial and vehicle service related uses may be appropriate.

In addition, in 2021, the Planning Commission considered the development code as it related to gas (fueling) stations and determined it would be appropriate to expand the opportunities for gas stations beyond the previous zoning limited to Highway Commercial. Specifically, the Planning Commission identified that intersections of roadways with classifications of arterial or arterial and collector are appropriate locations as they are designed to accommodate the traffic volume and circulation. Ultimately, an amendment to the development code was approved by the Planning Commission and City Council expanding opportunities for gas stations in the city.

Finally, in September 2023, the City adopted an Economic Opportunity Analysis and updated the comprehensive plan goals, policies, and actions items. One action item states, “evaluate opportunities to rezone land to allow for more smaller scale commercial uses around Airport Road and Grant Street.” This action item, with consideration of Airport Road as a significant transportation corridor further identifies a new community attitude that represents a significant departure from previous attitudes as reflected by the Planning Commission and City Council, establishing the ability to amend the comprehensive plan.

Chapter 16.27 of the Lebanon Development Code identifies the requirements for consideration of an amendment to the Comprehensive Plan Map. In order to approve an amendment to the map designation, the amendment shall be consistent with the City’s adopted Comprehensive Plan and Facility Plans, including the Transportation System Plan. The sections below provide a detailed analysis for how the proposed amendment meets the criteria.

III. DECISION CRITERIA AND RECOMMENDED FINDINGS – COMPREHENSIVE PLAN AND ZONING MAP AMENDMENTS

Section 16.27.050 of the Lebanon Development Code identifies the criteria in which to consider amendments to the Comprehensive Plan Map and Zoning Map. The criteria are the same for each amendment, as such, the findings have been combined.

- a. All proposed amendments to the Comprehensive Plan Map or to the Zoning Map shall be consistent with the City of Lebanon’s adopted Comprehensive Plan and Facility Plans. The City’s Facility plans, including the Transportation System Plan (TSP), are based on the future site service demands according to the Comprehensive Plan Map designation and associated zoning.

The Comprehensive Plan consists of ten Chapters with each Chapter addressing specific land use issues. The applicable policies in each Chapter are reviewed below:

- a. Chapter 1: Introduction - The introductory Chapter describes the Comprehensive Plan, its relationship to the Statewide Land Use Goals, the Citizen Involvement program and key terminology. Goals and policies relate to the organization of the Plan, the continued need for citizen involvement and the relationship of the Plan to State law and implementing codes. These goals and policies are incorporated in the Development Code criteria to determine the appropriateness of a Plan and/or zone change.

- b. Chapter 2: Natural Environment – The Chapter address goals and policies related to the City’s natural environment.

RECOMMENDED FINDING: The subject property is not located within a designated flood hazard area, does not contain any identified natural or historic resource, and is not otherwise in an area subject to increased risk to other natural hazards. Therefore, none of the goals or policies in Chapter 2 are found to be directly implicated by this application. Upon any development of the site, the property owner is responsible for complying with state law, including compliance with the State Historic Preservation Office (SHPO).

- c. Chapter 3: Urbanization – This Chapter provides the basic framework for future urban development within the City. It is found the following policies apply:

G-1: Providing for an orderly and efficient transition from rural to urban land use.

P-3: Support a flexible phased program for the orderly extension of water, wastewater, storm drainage and transportation services in response to land development proposals.

P-10: Review all development proposals to ensure that public facilities are available and have adequate capacity to accommodate the proposed development, or that such facilities and their capacities can be made available through appropriate extensions and/or enhancements concurrent with or prior to proposed developments.

P-30: Manage its Urban Growth Boundary and the lands within so as to make available sufficient land for the various uses to ensure choices in the marketplace, through implementation of land use regulations and land use policies.

P-31: Manage its Urban Growth Boundary and the lands within so as to encourage development within urban areas before conversion of urbanizable areas, through implementation of land use regulations and land use policies.

RECOMMENDED FINDING: "Urbanizable Land" is defined in the Comprehensive Plan Glossary. Generally, Urbanizable Lands are those unincorporated areas in the UGB for which a city and county coordinate on assuring will remain available for expansion of the city over the planning period. Mutually adopted urban growth boundary agreements typically govern how and when urbanizable land can be converted to urban use. The subject property, however, is already in the municipal boundary and currently zoned for full urban development under the Residential Mixed Density zone and has previously been developed to urbanized standards. Accordingly, it is found that the subject property is already urban land by existing zoning rather than urbanizable land to be converted to urban use for which Policies P-30 and P-31 would apply. Those policies are therefore not implicated in this case.

- d. Chapter 4: Land Use – This Chapter details the goals and policies to assure the City provides different types of land within City limits that are suitable for a variety of uses. It is found the following policies apply:

P-1: Recognize that the Comprehensive Plan land use designations or categories shall determine zoning.

P-6: Require that changes to the Comprehensive Plan Map be consistent with the policies of the Comprehensive Plan, State law, and any adopted intergovernmental agreements.

P-12: Ensure that the Zoning Map reflects and implements the Comprehensive Plan Map.

P-27: Allow future commercial and mixed-use centers within the Mixed Density Residential Zone based upon an approved development plan in conformance with the Planned Development standards and procedures of the Zoning Ordinance, provided access is from a designated highway or arterial and urban services are available.

P-40: Encourage a mix of commercial and residential uses within individual buildings, lots, and neighborhoods, in order to promote a compact, pedestrian friendly environment. Industrial uses should be allowed to mix with residential and commercial uses where there is limited potential for nuisance or jeopardy to the public health, safety, and welfare.

RECOMMENDED FINDING: The review process, evaluation, and determination of compliance with the Comprehensive Plan as part of the review process addresses compliance with Comprehensive Plan policies (P-6). The State effectively acknowledged the Comprehensive Plan, therefore, conformance with these policies assures conformance with state law. Compliance with Statewide Goals will be reviewed in another finding. For the subject property, the site is currently limited for residential opportunity and development, and the change to the Mixed-Use designation would allow for a greater opportunity of a mix of commercial and residential uses within the neighborhood (P-40).

- e. Chapter 5: Economic Development – This Chapter addresses population growth and economic development as well as those trends affecting both.

P-1: Ensure that Lebanon has enough land zoned to provide industrial and commercial opportunities, periodically monitoring land development, and updating the land use map to ensure that there is enough land in each designation to accommodate expected growth.

A-1: Monitor development in commercial and industrial areas. As development or rezoning occurs, evaluate whether the city continues to have enough commercial and industrial land to support employment growth

A-4: : Evaluate opportunities to rezone land to allow for more and smaller scale commercial uses around Airport Road and Grant Street.

RECOMMENDED FINDING: The 2023 Economic Opportunity Analysis (EOA) completed a buildable lands inventory that determined the city had sufficient land for commercial, industrial, and mixed use lands. However, the opportunities analysis identified Airport Road and Grant Street as high quality transportation corridors that could provide opportunities to support more commercial development. Action Item #4 in the adopted Policies and recommended actions memo from ECONorthwest as part of the EOA, specifically identifies evaluating opportunities to rezone land to allow for more smaller scale commercial uses around Airport Road. The rezoning of the subject property to provide commercial activity directly aligns with this goal.

- f. Chapter 6: Housing – This Chapter establishes the City’s Goals and Policies related to Housing.

RECOMMENDED FINDING: This proposed amendment would change the comprehensive plan and zoning map designation from residential mixed density to mixed use. The amendment would result in a reduction of land designated for housing opportunities by 1.11 acres. In the 2019 Housing Needs Analysis, the City identified the projected housing needs over a 20-year planning period, and conducted a buildable land inventory to identify whether there was sufficient land in the urban growth boundary to accommodate the housing need. Based on the results of the analysis, a total of 2,503 housing units are identified as the 20-year housing need, and the city has 240 acres of low-density land surplus, 0 acres of medium density land surplus, and 63 acres of high-density land surplus. A such, the reduction of 1.11 acres of land that may more readily accommodate housing would not result in an impact to the City’s ability to meet the long range housing needs for the community.

Chapter 7: Community Friendly Development & Preservation of Historic Resources - This Chapter focuses on policies creating a built environment suitable for the needs of a diverse population through a variety of uses scaled for the pedestrian, and capable of accommodating the automobile and mass transit. In addition, the Chapter focuses on historical preservation. The focus on Community Friendly design is associated with infill development, promotion of mixed-use opportunities, and development specific standards. In addition, there are no historical sites on the property, as such, the historical preservation goals are not applicable.

G-6: Developing sidewalks, crosswalks, and multi-use paths that not only meet ADA standards, but also enhance pedestrian and bicycle friendly environment throughout the community.

RECOMMENDED FINDING: As part of the development, improvements to the sidewalk along Airport Road would be completed, including a multiuse path to the east of the development, enhancing the walkability of the neighborhood. In addition, a signal is to be installed at the intersection of Airport Road and Stoltz Hill Road, increasing the safety of pedestrian and bicycle movement through the neighborhood.

- g. Chapter 8: Transportation – This Chapter addresses the transportation needs of the City with an emphasis of creating a variety of transportation options for pedestrians, bicyclists, vehicles and mass transit. It is found the following policies apply:

P-12: The transportation system shall be managed to reduce existing traffic congestion and facilitate the safe, efficient movement of people and commodities within the community.

RECOMMENDED FINDING: The subject site is located to the northwest of the West Airport Road and Stoltz Hill Road intersection at 911 West Airport Road. West Airport Road and Stoltz Hill Road are classified in the 2018 Lebanon Transportation System Plan (TSP) as a Minor Arterial and Collector streets, respectively. A future street project (no. D36) is identified in the 2018 Transportation System Plan (TSP). Upon its completion, the project will realign Stoltz Hill Road and add an intersection traffic signal. The proposed amendments will not impact the planned intersection improvement project. Additionally, Section 6.12.010(B)(1) includes thresholds for when Traffic Impact Studies (TIS) and Traffic Impact Analysis (TIA) are warranted, which is typically in association with a development proposal. A TIA based on the proposed development was found to be warranted . The TIA concluded a number of off-site improvements required, which have been conditioned as part of the concurrent development proposal. With the completion of the TIA and conditioning of the off-site improvements, it is concluded that the proposal complies with Chapter 8.

- h. Chapter 9: Public Facilities and Service - The City is required by State law to plan and develop a timely, orderly, and efficient arrangement of public facilities and services to serve as a framework for urban development. Goals and policies in this Chapter address those requirements. It is found the following policies apply:

General Policies

P-8: Review all development proposals to ensure that public facilities are available and have adequate capacity to accommodate the proposed development, or that such facilities and their capacities can be made available through appropriate extensions and/or enhancements concurrent with or prior to proposed developments. (Duplicated in Chapter 3, Urbanization)

P-9: Require that new developments are either served by existing and/or proposed public infrastructure improvements, and/or are served by privately funded infrastructure extensions and improvements. (Duplicated in Chapter 3, Urbanization)

P-10: Consider impacts on key City-provided urban utility services (water, storm drainage, wastewater, and streets) and any other community facilities that are identified by service providers as substantially impacted by the proposal before-development proposals, or rezoning applications are approved.

RECOMMENDED FINDING: City water service and storm drainage are available in Airport Road along the frontage of the subject property. City Sewer service is available in Stoltz Hill Road along the frontage of the subject property. These facilities would be able to accommodate any development opportunity of the subject property. As such, there is sufficient utility infrastructure to service the site, and therefore, the proposal complies with Chapter 9.

- i. Chapter 10: Plan Implementation, Amendment, and Land Use Planning Coordination – This Chapter establishes procedures for amending the Comprehensive Plan Map and Zoning Map. Specific applicable policies include:

P-1: The City Council may amend the Comprehensive Plan and/or Map after referral to the Planning Commission public hearing, for action, review, revisions, and recommendations.

P-2: Changes to the Plan and/or Map shall be made by ordinance after public hearings as prescribed by state law and local ordinances.

P-3: Changes in the Plan and/or Map shall be incorporated directly into the document at the appropriate place. A list of all amendments with date of passage shall then become part of the document until the next comprehensive update of the entire Comprehensive Plan.

P-4: An amendment to the Comprehensive Plan and/or Map may be considered when one or more of the following conditions exist:

- a. Updated data demonstrates significantly different trends than previous data;
- b. New data reflects new or previously undisclosed public needs;
- c. New community attitudes represent a significant departure from previous attitudes as reflected by the Planning Commission or City Council;
- d. Statutory changes significantly affect the applicability or appropriateness of existing plan policies.

P-5: Residents, property owners, their authorized agents, the Planning Commission, the City Council, or City staff may initiate a Comprehensive Plan amendment. In order to obtain a Comprehensive Plan and/or Map amendment, the applicants shall have the burden of proof that all of the following conditions exist:

- a. There is a need for the proposed change;
- b. The identified need can best be served by granting the change requested;
- c. The proposed change complies with the Statewide Planning Goals; and,
- d. The proposed change is consistent with all other provisions of the City's Comprehensive Plan.

RECOMMENDED FINDING: Policies P-1 through P-3 relate to amendment procedures and do not function as approval criteria. Regarding P-4, a

number of recent changes to city policy and updates to portions of the comprehensive plan provide new data and community attitude as reflected by the Planning Commission and City Council. As part of the 2018 update to the City's Transportation System Plan, the transportation bypass system which would have routed significant traffic around the city to the southern portion from Hwy 34 was removed from the plan. With the bypass no longer considered, the existing transportation system allows travelers to bypass the northern portion of the city from Hwy 34 using Denny School Road which turns into Airport Road to connect to Hwy 20. Airport Road is a significant transportation corridor for access in and out of the city to avoid the northern traffic congestion formed by the Hwy 20 couplet. As such, with the change in TSP, and Airport Road being a significant transportation corridor, amending the land use designation to allow for additional commercial and vehicle service related uses may be appropriate.

In addition, in 2021, the Planning Commission considered the development code as it related to gas (fueling) stations and determined it would be appropriate to expand the opportunities for gas stations beyond the previous zoning limited to Highway Commercial. Specifically, the Planning Commission identified that intersections of roadways with classifications of arterial or arterial and collector are appropriate locations as they are designed to accommodate the traffic volume and circulation. Ultimately, an amendment to the development code was approved by the Planning Commission and City Council expanding opportunities for gas stations in the city.

Finally, in September 2023, the City adopted an Economic Opportunity Analysis (EOA) and updated the comprehensive plan goals, policies, and actions items. One action item states, "evaluate opportunities to rezone land to allow for more smaller scale commercial uses around Airport Road and Grant Street." This action item, with consideration of Airport Road as a significant transportation corridor further identifies a new community attitude that represents a significant departure from previous attitudes as reflected by the Planning Commission and City Council, establishing the ability to amend the comprehensive plan.

Regarding P-5a.b., the proposed land use designation amendment is needed and is best served by granting the amendment to be able to accommodate the EOA action item to allow for rezoning on Airport Road to offer smaller scale commercial uses. The subject property, being located at the intersection of an arterial and collector road, with the intersection soon to be signalized, increases the commercial viability of the property, identifying the site as well suited to accomplish the EOA goals. Findings here above and below further establish that the change complies with the Statewide Planning Goals and the proposed change is consistent with all other provisions of the City's Comprehensive Plan.

Compliance with the Statewide Goals (P-5.c) is noted as follows:

Goal 1, Citizen Involvement: Public hearings will be held before the Planning Commission on October 18, 2023 and the City Council on November 8, 2023. This is consistent with City procedures, state law, and the intent of the Goal.

Goal 2, Land Use Planning: The proposal does not involve exceptions to the Statewide Goals. Adoption actions are consistent with the acknowledged Comprehensive Plan and Development Code.

Goal 3, Agricultural Lands: This Goal does not apply, as the land is not designated farmland.

Goal 4, Forest Lands: This Goal does not apply, as the land is not designated forestland.

Goal 5, Open Spaces, Scenic and Historic Areas, and Natural Resources: The site has not been designated for open space, a scenic area, or known for having natural resources. As such, this goal is not applicable. Upon any development of the site, the property owner is responsible for complying with state law, including compliance with the State Historic Preservation Office (SHPO).

Goal 6, Air, Water and Land Resource Quality: the map designation would change from a residential mixed density designation to mixed use. With the designation change, there are properties in the vicinity with the new designation, and the mixed use zone maintains similar land use types that are permissible in the residential mixed density designation, with an expanded opportunity for additional commercial activities and industrial activities. The use proposed associated with the requested zone change does not include idling vehicles impacting emissions, the development would be graded to ensure historical drainage patterns are maintained to avoid flooding issues meeting water resource considerations, and the land is not suitable for quality farming or timber and is an urbanizable property, therefore, there should be no significant impacts on air, water or resource quality than would otherwise occur.

Goal 7, Natural Hazards: The site is not located within any designated flood plain, does not contain steep slopes or contain any other identified natural hazard.

Goal 8, Recreational Needs: The proposed map amendments do not reduce the land designated for recreational needs, nor create uses which would adversely impact recreational opportunities.

Goal 9, Economic Development: The goal is to provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare, and prosperity of Oregon's citizens. The proposed change will be result in an increase in employment lands along Airport Road, which is in accordance with the City's adopted Economic Opportunities Analysis. LCDC implementing rules at

OAR Chapter 660, Division 9, are not implicated by changes of less than two acres of employment land designations.

Goal 10, Housing: This proposed amendment would change the comprehensive plan and zoning map designation from residential mixed density to mixed-use. Residential uses are permissible in the mixed-use zone; however the concurrent development proposal does not include residential uses. The amendment would result in a reduction of land designated for housing opportunities by 1.11 acres. In the 2019 Housing Needs Analysis, the City identified the projected housing needs over a 20-year planning period, and conducted a buildable land inventory to identify whether there was sufficient land in the urban growth boundary to accommodate the housing need. Based on the results of the analysis, a total of 2,503 housing units are identified as the 20-year housing need, and the city has 240 acres of low-density land surplus, 0 acres of medium density land surplus, and 63 acres of high-density land surplus. As such, the reduction of 1.11 acres of land that may more readily accommodate housing would not result in an impact to the City's ability to meet the long range housing needs for the community. As such, this proposal would comply with Goal 10.

Goal 11, Public Facilities and Services: The goal is to plan and develop a timely, orderly and efficient arrangement of public facilities and services to serve as a framework for urban and rural land. The subject property is urban land that is already fully served by urban public facilities and services.

Goal 12, Transportation: Previous findings indicate the map amendments will not significantly affect planned transportation improvements.

Goal 13, Energy Conservation: The map amendments are neutral regarding energy conservation.

Goal 14, Urbanization: Previous findings indicate the change will still allow the city to meet housing needs of the community, as well as not impact the ability to provide industrial opportunities, and expands commercial opportunity along Airport Road, which has been specifically identified as a goal in the Economic Opportunity Analysis. The land is already within the urban growth boundary and city limits, thus eligible for urbanization.

Goals 15 to 19, Willamette River Greenway, Estuarine Resources, Coastal Shores, Beaches and Dunes, Ocean Resources: The proposals do not involve land within the Willamette Greenway or coastal areas.

Finally, all previous findings indicate the proposal complies with the applicable policies of the City's Comprehensive Plan (P-5.d).

- b. Facility plans need to be consistent with the Comprehensive Plan Map as well as the text, and changes to the Map may necessitate changes to a facility plan. For example,

changing a Comprehensive Plan Map designation to a higher intensity use may require an amendment to the TSP, sanitary sewer, or potable water master plans.

RECOMMENDED FINDINGS: The property is currently designated as Residential Mixed Density, and the proposal includes a revised designation to Mixed Use. The Mixed-Use designation allows for a mix of uses including commercial, residential, and light industrial. The property is approximately 1.12 acres. The property is currently utilized as a residential development, and potential uses for the property under the Mixed Use designation would be similar commercial uses, or small residential uses. Based on the size of the property, and the similar uses permitted and feasible on the property, the existing facility plans would not require amendment.

In addition, as part of the Transportation System Plan (TSP), there is a designated intersection improvement project to align Stoltz Hill Road and to add a traffic signal. The proposed map amendments would not result in a change to the TSP project or necessitate amendments to existing facility plans as the allowed and proposed uses are in line with the existing designation in terms of demand on facilities and would not impact the planned transportation project from moving forward.

- c. Applicants proposing amendments to the Zoning Map must request a City Zoning Classification that is consistent with the Comprehensive Plan Map designation for a subject property. If an applicant requests a City Zoning Classification that is not consistent with the Comprehensive Plan Map, the zoning requested shall not be granted until the Comprehensive Plan Map is first appropriately amended to reflect concurrence. (See the Annexation Zoning Matrix, Table 16.26-1.) Such an amendment requires a separate application, hearing and decision; this process may occur concurrently with the Zoning Map Amendment hearing.

RECOMMENDED FINDING: The proposal is to amend the Comprehensive Plan Map and Zoning Map. With the Comprehensive Plan Map amendment to the Mixed Use designation, the required zoning designation based on Table 16.26-1 of the Lebanon Development Code is Mixed Use, which is consistent with the proposal by the applicant. As such, the project complies with this decision criteria as the proposed zoning map amendment to Mixed Use is occurring with a concurrent comprehensive plan map amendment to Mixed Use.

- d. Section 16.27.080.B states that if proposed amendments to the Comprehensive Plan Map or Zoning Map do not comply with the Comprehensive Plan, the Comprehensive Plan must first be amended so that the proposed Map amendment will be consistent with and accurately implement the Plan.

RECOMMENDED FINDING: Previous findings indicate the proposal may comply with the applicable Comprehensive Plan policies and does not require amendments to the Plan text. Therefore, this provision does not apply.

IV. CONCLUSION AND RECOMMENDATION –

COMPREHENSIVE PLAN AND ZONING MAP AMENDMENTS

Staff finds the proposal complies with the decision criteria for a Comprehensive Plan and Zoning Map Amendment and recommends the Planning Commission to recommend the City Council approve the proposal.

V. DECISION CRITERIA AND RECOMMENDED FINDINGS – ADMINISTRATIVE REVIEW AND CONDITIONAL USE

The Applicant is requesting consideration of an Administrative Review and Conditional Use Permit for the development of a convenience store and a gas station.

Below is an analysis of the review criteria (Section 16.20.040.D of the LDC) and recommended findings for the ADMINISTRATIVE REVIEW application:

1. The proposal shall conform to use, height limits, setbacks, and similar development requirements of the underlying zone.

RECOMMENDED FINDING: The development standards found in the Chapter 16.08 (Commercial Land Uses) zone shall be applied to commercial development in the Mixed Use zone. The convenience store development is permissible in the Neighborhood Commercial Zone (Z-NCM) and is consistent with the buffering and use impacts authorized in the Z-NCM zone. As such, the NCM zone standards are applied for the setbacks and building height for the convenience store. The gas station use is authorized in the Highway Commercial (Z-HCM) zone, and therefore, the development standards for the gas station portion of the development shall be applied based on the Z-HCM zone.

For setbacks, with the zoning application as described above, the front setback (Airport Road) shall be 10-feet (20-feet abutting residential), streetside setback (Stoltz Hill Road) shall be 10-feet (20-feet abutting residential), the interior side (western property line) setback shall be 10-feet abutting residential, and the rear (northern property line) setback shall be 20-feet. The maximum building height authorized for the convenience store is 25-feet, and 45-feet for the gas station; and the minimum landscape area is the required front and street side setbacks. However, Section 16.14.040.A.2 authorizes parking areas (inclusive of drive aisles) to be within the front and streetside setbacks, if separated by at least a five-foot landscaped buffer outside the clear vision triangle.

The convenience store building is proposed to be setback 10-feet from the western interior side property line, approximately 65-feet from the front property lines, and approximately 80-feet from the rear property line. The fueling pump canopy for the gas station use is setback approximately 60-feet from the front property line, and 50-feet from the street side property line. Where the parking areas are proposed in the setbacks, a minimum 6.4-foot landscape planter is provided. The overall height of the convenience store and fuel canopy is 17-feet, which is less than the maximum authorized. With approximately 6,480 square feet of building coverage, the 15% lot coverage is within permissible limits. As such, this criterion has been satisfied.

2. The proposal shall comply with applicable access and street improvement requirements in Chapters 16.12 and 16.13, respectively.

RECOMMENDED FINDING: Per the City's adopted Transportation System Plan (TSP), Airport Road is a Minor Arterial and Stoltz Hill Road at the intersection is a Collector Road. The north leg of Stoltz Hill Road was built after the TSP. However, it meets the standards for constrained roadway design for Collector Roads per the TSP. With a curb-to-curb width of 34-feet, this provides sufficient width for two 12-foot travel lanes, and parking on one side of the street. Per the constrained roadway design standards, the minimum lane width with shared roadway bike facilities is 12-feet, with 4.5 ft landscape strips, and 5-ft sidewalks. The northern leg of Stoltz Hill Road meets these design options for a constrained Collector per the TSP, and the typical street design standards found in Table 16.13.030-2 (Typical Street Design Standards) in Section 16.13.030 of the Lebanon Development Code (LDC).

The site is to be accessed by two driveways, one on Airport Road, and one on Stoltz Hill Road. The Stoltz Hill Road access is located on the northern most corner of the property and is proposed to share driveway access with two flag lots associated with the residential subdivision to the north. A reciprocal access easement was recorded as part of the final plat between the properties during the re-plat for the subdivision through application S-23-01. The driveway is proposed to be improved to commercial driveway standard and include lane width to provide a dedicated right and left turn out of the driveway onto Stoltz Hill Road. The Airport Road driveway is proposed to be located near the center of the property width on Airport Road approximately 115 feet from the intersection. With the proximity to the intersection, the traffic movements would be restricted to only allow a "right-in, right-out" traffic movement.

The site access and traffic circulation were reviewed in the Traffic Impact Analysis (TIA) by Kittelson & Associates. The following are the required off-site improvements to address traffic impacts:

- The planned signalization project should be completed prior to the proposed convenience store/fueling facility being issued a certificate of occupancy.
- Construct a non-traversable raised median barrier along north edge of the eastbound Airport Road left-turn lane from the Stoltz Hill Road intersection to approximately 50 feet west of the proposed site driveway.
- In conjunction with the planned near-term signalization of the Airport Road/Stoltz Hill Road intersection to be delivered by the City of Lebanon, it is recommended that the signalization project include the following design elements pending availability of adequate right-of-way along the west side of Stoltz Hill Road:
 - Incorporate a separate left-turn lane with at least 50 feet of storage and a shared through/right-turn lane on the northbound Stoltz Hill Road approach to Airport Road.

With the off-site improvements identified in the TIA and required operation of the traffic signal prior to use occupancy, the site access and circulation would operate at an

acceptable industry standard. With the conditions of development, this criterion has been met.

3. The proposal shall comply with applicable parking requirements in Chapter 16.14.

RECOMMENDED FINDING: There is no defined parking for a gas station. Per Table 16.14.070-1 in Section 16.14.070 of the LDC, retail/grocery stores require four parking spaces per 1,000 square feet of building area. The proposed convenience store is 4,000 square feet, requiring 16 parking spaces, and four bicycle parking spaces. A total of 17 parking spaces are proposed, including four bicycle parking rack spaces. The customers for the fueling station would generally park at the fueling pump island or share the parking areas for the convenience store, meeting the parking requirement. As such, this criterion has been met.

4. The proposal shall comply with applicable screening and landscaping provisions in Chapter 16.15.

RECOMMENDED FINDING: Chapter 16.15 of the development code requires the front and street side setbacks to be landscaped, when not used for parking and drive aisle, at which point, a 5-foot landscape buffer is required. Conditions of development have been included to require landscaping in the required setbacks to be planted prior to certificate of occupancy. With these conditions, this criterion has been met.

5. Any required public facility improvements shall comply with provisions in Chapter 16.16.

RECOMMENDED FINDING: For utilities, there are public facilities in Stoltz Hill Road that are sized to accommodate build out, including commercial development on the street. There is an 8-inch sewer main, an 8-inch water main, and a 15-inch storm drain available for connection to the site. All public street facilities are built out and do not require expansion, except as identified in the TIA. With the mitigation measures of the TIA included as conditions of development, this criterion has been met.

6. Where applicable, the proposal shall comply with development requirements within identified hazard areas and/or overlay zones.

RECOMMENDED FINDING: There are no areas of steep slopes on the subject property. The applicable Flood Insurance Rate Map for the subject site is 41043C0566G, dated September 29, 2010. Based on this FIRM, the entirety of the subject property is located outside of the Special Flood Hazard Area (SFHA), otherwise known as the 100-year floodplain. The subject property is greater than 75 feet from the top of the bank of the South Santiam River, Oak Creek, or Cheadle Lake waterbodies. The subject property is also located outside of the designated boundaries of the special transportation area.

The site is located in the conical surface area of the Airport Overlay Zone, but the proposed subdivision does not include development that exceeds the height limit or cause interference with the aircraft using the airport. The subject property is located outside all other overlay zones or special districts. As such, the criterion is met.

7. The proposal shall comply with the supplementary zone regulations contained in Chapter 16.19 or elsewhere in the Development Code.

RECOMMENDED FINDING: Chapter 16.19 identifies exterior lighting standards to ensure the lighting associated with new development does not adversely impact neighboring properties. Conditions of development have been incorporated to require a photometric plan to identify compliance with this section. As such, this criterion has been met.

Below is an analysis of the review criteria (Section 16.21.060 of the LDC) and recommended findings for the CONDITIONAL USE PERMIT application:

1. The application complies with all of the applicable provisions of the underlying Land Use Zones (LDC Chapters 16.09.110), including: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other special standards as may be required for certain land uses.

RECOMMENDED FINDING: Findings 1-5 associated with the Administrative Review criteria are hereby incorporated as reference. This criterion has been met.

2. The site size, dimensions, location, topography and access are adequate for the needs of the proposed use, considering the proposed building mass, parking, traffic, noise, vibration, exhaust/emissions, light, glare, erosion, odor, dust, visibility, safety, and aesthetic considerations.

RECOMMENDED FINDING: The proposed use is an open storage facility for the storage of boats, vehicles, trailers, and recreational vehicles.

- Site Size, Dimensions, and Topography. The site is ±1.11-acres in size and rectangularly shaped. The site is relatively flat with no steep slopes, wetlands, floodplains, or other impediment to the full use of the property.
- Access. The site is to be accessed by two driveways, one on Airport Road, and one on Stoltz Hill Road. The Stoltz Hill Road access is located on the northern most corner of the property and is proposed to share driveway access with two flag lots associated with the residential subdivision to the north. A reciprocal access easement was recorded as part of the final plat between the properties during the re-plat for the subdivision through application S-23-01. The driveway is proposed to be improved to commercial driveway standard and include lane width to provide a dedicated right and left turn out of the driveway onto Stoltz Hill Road. The Airport Road driveway is proposed to be located near the center of the property width on Airport Road approximately 115 feet from the intersection. With the proximity to the intersection, the traffic movements would be restricted to only allow a “right-in, right-out” traffic movement.
- Building Mass: Mass refers to the size or physical bulk of a building, and can be understood as the actual size, or size relative to context. The two proposed structures on-site would be a 4,000 square foot convenience store, and a pump island canopy. Both structures are proposed to be approximately 17-feet in height, consistent with the one and two-story structures in the vicinity.

- Vehicle Parking – There is no defined parking for a gas station. Per Table 16.14.070-1 in Section 16.14.070 of the LDC, retail/grocery stores require four parking spaces per 1,000 square feet of building area. The proposed convenience store is 4,000 square feet, requiring 16 parking spaces, and four bicycle parking spaces. A total of 17 parking spaces are proposed, including four bicycle parking rack spaces. The customers for the fueling station would generally park at the fueling pump island or share the parking areas for the convenience store, meeting the parking requirement.
- Noise generation is anticipated to be minimal with the loudest noise generation occurring from the delivery of products to the convenience store. The convenience store use is not subject to review of the conditional use permit. Noise generated from the fueling station use is anticipated to be customer conversation and vehicle traffic. There are no anticipated noise mitigation measures required. The overall property operation would be subject to the citywide noise ordinance.
- Vibration, erosion, and dust generation is not anticipated once the site is developed. During the periods of construction, intermittent vibrations may occur, and dust may be generated. Construction best management practices are required at all construction sites in the city to mitigate construction dust, off-site impacts, noise, erosion, and vibration.
- Exhaust and Emissions are expected to be higher during the construction period with a higher propensity for truck idling associated with construction equipment. This is a temporary impact. The convenience store and gas station are not anticipated to produce significant exhaust and emissions beyond other commercial uses. Both uses involve customers entering the site with vehicles, internal circulation, and parking. The vehicles are turned off during times which the customers are utilizing the site services. These actions are similar to other commercial uses that do not include a drive-up window. No anticipated mitigation measures are required for the exhaust and emissions.
- Light glare and visibility are items of consideration for the gas station use. With the canopy structure illuminating an outdoor area, the site will generate more light from the property than uses that do not include outdoor uses. In addition, the vehicle headlights while moving through the property would extend past property lines. To address lighting impacts, Section 16.06.10.A.3 of the LDC requires canopy lighting fixtures to be fully within the canopy structure and not extend below the canopy ceiling, and to meet the exterior lighting standards in Section 16.19.050 which require exterior lighting to be designed, constructed, located, shielded, and directed in such a manner so as to not face directly, shine, reflect, or glare onto adjacent residences, streets and other land uses, and the lumens should not exceed 0.1 at the property line. A photometric map has been conditioned as part of the building plan submittal to verify the lighting design meets these requirements. In addition, landscaping conditions have been incorporated to require mature shrubs to be planted along the southern and eastern property lines, or other design treatment, outside the vision clearance areas, to block headlight lights from extending beyond the property line. Finally, per the applicant narrative, the site would be closed between 11:00pm and 5:30am, limiting nighttime lighting impacts.
- Visibility and Safety considerations include ensuring the vision clearance and site distance triangles remain clear of any visual impairments. The business owner is also encouraged to coordinate with the Police Department to discuss community policing through environment design (CPTED) concepts and on-site security measures.
- Aesthetic considerations include landscape improvements and lighting considerations. The City does not have any design standards, and the site is over one acre and not

subject to infill design evaluation. High screen landscaping has been conditioned for the western and northern property boundary when adjacent to residential dwellings, and specific landscape treatments are required for the front and streetside landscape areas to address lighting mitigation.

- Screening of Refuse Containers – LDC 16.15.020.2.f.2 requires that refuse containers and disposal areas be screened from view by placement of a sight-obscuring fence, wall, or hedge at least six feet tall. As shown on the site plan, a trash enclosure is provided; therefore, this standard is met.

3. The negative impacts of the proposed use on adjacent properties and on the public can be mitigated through application of other Code standards, or other reasonable conditions of approval.

RECOMMENDED FINDING: There are no known negative impacts of the proposed use on adjacent properties or the public. The site is within the vicinity of a wide array of land uses including, residential, commercial, and industrial development. According to LDC 16.06.050, the proposed gas station is permitted via Conditional Use approval. The previous decision criteria and findings identified items of possible negative impact on the adjacent properties and code standards and conditions of development included to mitigate the potential impacts. The specific language for decision criteria number 2 for the conditional use permit is incorporated by reference. Therefore, this criterion has been met with the established conditions of approval and application of the development code.

4. All required public facilities have adequate capacity to serve the proposal.

RECOMMENDED FINDING: For utilities, there are public facilities in Stoltz Hill Road that are sized to accommodate build out, including commercial development on the street. There is an 8-inch sewer main, an 8-inch water main, and a 15-inch storm drain available for connection to the site. All public street facilities are built out and do not require expansion, except as identified in the TIA.

Police Protection: The proposed development is served by the City of Lebanon Police Department. No comments or concerns were provided by the City of Lebanon Police Department as part of this application.

Fire Safety: The Fire District has reviewed the proposed development and provided conditions for development, which have been incorporated into the record.

With the mitigation measures of the TIA included as conditions of development, this criterion has been met.

5. Existing conditions of approval required as part of a prior land use decision shall be met.

RECOMMENDED FINDING: Subdivision S-23-01 included a replat of the subdivision to the north of the subject property and included a property line adjustment between the subdivision and the subject property, including an establishment of an access easement for two flag-lot

parcels and the subject property. All conditions of approval for the subdivision as they relate to the access and maintenance of the easement remain in full force and effect.

6. The applicant shall be required to upgrade any existing development that does not comply with the applicable land use district standards, in conformance with Chapter 16.30, Non-conforming Use and Development.

RECOMMENDED FINDING: There are no legal non-conforming conditions in the site area. As such, this criterion has been met.

7. The application complies with all of the Community Development Standards in LDC Chapters 16.12-16.19

RECOMMENDED FINDING: Findings pertaining to Chapters 16.12 – 16.16, and 16.19 are provided under previous criteria above and incorporated herein by reference. Chapters 16.17 and 16.18 regarding wireless communication facilities and signage are not applicable as none are proposed with this application. With the conditions incorporated, this criterion has been met.

VI. PUBLIC NOTIFICATION AND COMMENTS

A public notification for this project was issued on September 27, 2023 for initial consideration by the Planning Commission on October 18, 2023. All public comment received on this application will be presented to the Planning Commission at the scheduled public hearing.

VII. CONCLUSION AND RECOMMENDED CONDITIONS FOR DEVELOPMENT – ADMINISTRATIVE REVIEW AND CONDITIONAL USE PERMIT

Staff finds the proposal complies with the decision criteria for an Administrative Review and Conditional Use Permit, and recommends approval of the application subject to the adoption of the following Conditions of Development:

1. The Planning Department conditions include, but may not be limited to:
 - a. Full sight-obscuring fencing shall be installed along the western project boundary line. Fencing shall be a minimum 6-feet in height to a maximum 8-feet in height outside the front setback area.
 - b. High screen landscape treatment shall be provided along the western and northern property boundary, outside any vision clearance areas.
 - c. Landscaping or screening treatment shall be provided along the southern, eastern, and northern property boundary and parking area. Shrubs, landscaping, or other treatment as proposed by property owner and approved by the Planning Official shall be installed in a manner which provides immediate screening cover to headlight height of a passenger vehicle at time of planting or installation.
 - d. All landscaping as identified on the site plan shall be maintained, clear of trash and debris.

- e. A photometric plan shall be submitted for review and approval prior to certificate of occupancy that demonstrates a maximum of 0.1 lumens at all property boundaries.
 - f. A minimum of 16 vehicle parking and 4 bicycle parking spaces shall be maintained on-site at all times.
 - g. All product deliveries to the convenient store must occur on-site. Delivery trucks may not temporarily park or idle on Stoltz Hill Road or Airport Road.
 - h. Site deliveries should be attempted to occur during daytime hours. At no time, should delivery or other on-site activities violate the city's adopted noise ordinance.
2. All requirements of the Lebanon Fire District shall be met, including but not limited to:
- a. Plans shall be submitted for review and approval by the Lebanon Fire Marshal that demonstrates full compliance with the Oregon Fire Code and local amendments. Lebanon Fire Marshal approval shall be obtained prior to issuance of building permits.
3. The Engineering Department conditions include, but may not be limited to:
- a. This proposal includes installation of new public improvements. All public improvements shall:
 - i. conform to the latest "City of Lebanon Standards for Public Improvements."
 - ii. require completion of a Drawing Review Application and a Public Improvements Permit prior to beginning construction.
 - iii. be designed by a professional engineer registered in the State of Oregon.
 - b. An engineered site plan shall be submitted for review and approval for the site. The site plan must be submitted with an Application for Site Plan Review and associated fee. The site plan shall detail all site improvements necessary for the proposed development together with a grading and drainage plan.
 - c. All elevations shown on plans submitted to the City must be on the NAVD 88 vertical datum to provide compatibility with the City computer aided mapping system.
 - d. All private, onsite utilities must be reviewed and approved by the City Building Official.

Transportation

- e. Provide verification of Republic Services approval of location and turn around access to garbage and recycling containers.
- f. Reimbursement payment to the city for completion of the Traffic Impact Study. (Balance \$5069.75)
- g. Include in the engineered drawings all traffic recommendations outlined in the Traffic Impact Study for review and approval.
- h. Provide an illumination plan to demonstrate site lighting shall not glare or shine onto adjacent public streets or neighboring properties.

Water

- i. Identify any on-site wells on the engineered drawings. Wells must be capped and abandoned according to state and county regulations prior to connection to the public water system.
- j. The number and location of fire hydrants shall installed in conformance with the Oregon Fire Code and by the Lebanon Fire Marshal. All new hydrants must be operational and accepted by the City prior to storage of combustible materials on site.

Storm Drainage

- k. The drainage system and grading plan shall be designed so as not to adversely impact drainage to or from adjacent properties. Storm drainage facilities must be designed and constructed to ensure historical rates of site discharge are not exceeded. Storm drain capacity shall be determined by the Rational Method for a 10-year event with a 15-minute minimum durations time as per City of Lebanon Storm Water Master Plan. A detailed design including engineering calculations shall be submitted as part of site plan review.
- l. With the engineering drawings, provide a grading plan for the sites that indicates existing and proposed elevations. Drainage improvements (ditches and or piping) may be required at the site boundaries to prevent adverse impacts. The engineering drawings must provide a detailed design (including calculations) of the drainage improvements and mitigation of any impacts to adjacent properties.
- m. Provide a construction erosion prevention plan.
- n. Provide verification of Oregon DEQ NPDES permit issuance and all conditions of permit issuance prior to construction.

Sewer System

- o. Identify any on-site septic systems on the engineering drawings. Septic systems must be abandoned according to the city and county regulations.

Landscaping

- p. Any landscaping proposed in the public right of way shall have a maximum mature height of no more than 24 inches above the street grade and at least 3 feet from any fire hydrant. All landscaping proposed in the yard setback areas adjacent to public streets shall have a maximum mature height of no more than 36 inches above the street grade.
4. Compliance with these conditions, and the applicable requirements of the Lebanon Development Code, City Public Works Standards, City Engineering Standards, Lebanon Fire District requirements and adopted building code, shall be the sole responsibility of the property developer.

VIII. PLANNING COMMISSION RECOMMENDED ACTIONS – COMPREHENSIVE PLAN AND ZONING MAP AMENDMENTS

- 1. Recommend the City Council approve the proposed Comprehensive Plan Map Amendment (CPMA-23-03) and Zoning Map Amendment (ZMA-23-03) to modify the

Comprehensive Plan Map and Zoning Map designation from Residential Mixed Density (C/Z-RM) to Mixed Use (C/Z-MU); or

2. Recommend the City Council approve the proposed Comprehensive Plan Map Amendment (CPMA-23-03) and Zoning Map Amendment (ZMA-23-03) to modify the Comprehensive Plan Map and Zoning Map designation from Residential Mixed Density (C/Z-RM) to Mixed Use (C/Z-MU), adopting modified findings for the decision criteria and conditions of development; or
3. Recommend the City Council deny the proposed Comprehensive Plan Map Amendment (CPMA-23-03) and Zoning Map Amendment (ZMA-23-03) to modify the Comprehensive Plan Map and Zoning Map designation from Residential Mixed Density (C/Z-RM) to Mixed Use (C/Z-MU), specifying reasons why the proposal fails to comply with the decision criteria; and
4. Direct staff to prepare an Order of Recommendation for the Chair or Vice Chair's signature incorporating the adopted findings as approved by the Planning Commission.

IX. PLANNING COMMISSION RECOMMENDED ACTIONS – ADMINISTRATIVE REVIEW AND CONDITIONAL USE PERMIT

1. Approve the proposed Administrative Review (AR-23-04) and Conditional Use Permit (CU-23-03), subject to approval of the comprehensive plan and zoning map amendments, adopting the written findings for the decision criteria contained in the staff report with the conditions of development; or
2. Approve the proposed Administrative Review (AR-23-04) and Conditional Use Permit (CU-23-03), subject to approval of the comprehensive plan and zoning map amendments, adopting modified findings for the decision criteria and conditions of development; or
3. Deny the proposed Administrative Review (AR-23-04) and Conditional Use Permit (CU-23-03), specifying reasons why the proposal fails to comply with the decision criteria; and
4. Direct staff to prepare an Order of Decision for the Chair or Vice Chair's signature incorporating the adopted findings as approved by the Planning Commission.



NOTICE OF PUBLIC HEARING

LEBANON PLANNING COMMISSION

NOTICE IS HEREBY GIVEN that a public hearing will be held before the Lebanon Planning Commission on **October 18, 2023 at 6:00 p.m.** in the Santiam Travel Station located at 750 S 3rd Street, to afford interested persons and the general public an opportunity to be heard and give testimony concerning the following matter:

Planning Case No.:	A-23-04, CPMA-23-03, CU-23-03, & ZMA-23-03
Applicant:	Sona Athwal
Location:	911 W Airport Rd
Map & Tax Lot No.:	12S02W15BD01000
Request:	Administrative Review, Comprehensive Plan Map Amendment, Conditional Use & Zone Map Amendment
Decision Criteria:	Lebanon Development Code Chapters: 16.05, 16.20, 16.26 & 16.27

Request: The applicant is requesting a Comprehensive Plan Map and Zone Map Amendment to change the designations from Residential Mixed Density to Mixed Use. The applicant is also requesting Administrative Review and Conditional Use approval to construct a new convenience store and gas station.

Providing Comments: The City will be accepting public comment on this item in a number of ways to afford interested persons and the general public an opportunity to give testimony on the subject matter. Written and verbal testimony will be accepted upon issuance of this notice, **until 5:00pm on Tuesday, October 17, 2023.** Written testimony may be emailed to kelly.hart@lebanonoregon.gov or mailed to the City of Lebanon at 925 S. Main Street, Lebanon, OR 97355, or delivered and dropped in the white mailbox in front of City Hall.



The public is invited to either participate in person at the Santiam Travel Station or watch the meeting virtually on **October 18, 2023.**

If you wish to address the Commission under Public Comments or for a Public Hearing, click: <https://zoom.us/meeting/register/tJ0vdOuhrzkvGdM7HGeune-lsnzIPYFADCzS> to register in advance for the meeting. You will receive a confirmation email containing information about joining the meeting. Attendees will need to register to receive the link to the meeting.

Please register **ONLY** if you wish to address the Commission. If you want to watch or listen to the meeting, please click this link to do so on YouTube: <https://youtube.com/live/3wsfbypsdhU>

The agenda and application materials will be available for review on the City's website at <https://www.lebanonoregon.gov/meetings> seven days prior to the hearing.

**COMPREHENSIVE PLAN MAP AND ZONE MAP AMENDMENTS;
CONDITIONAL USE (CLASS 3); ADMINISTRATIVE REVIEW (CLASS 1)
APPLICATIONS**

Submitted to: City of Lebanon
Planning Department
925 S. Main Street
Lebanon, Oregon 97355

Applicants/Property Owners: Sona Athwal
2515 Geary Street SE
Albany, OR 97322
Sona Athwal athwal1@yahoo.com / (530) 662-2100

Applicants/Property Owners: Sranna Jaswant Et Al
2342 Kokanee Way
Lebanon, OR 97355

Applicant's Representative: Udell Engineering and Land Surveying, LLC
63 E. Ash Street
Lebanon, OR 97355

Contact: Laura LaRoque
Email: laura@udelleng.com
Phone: (541) 990-8661

Site Location: 911 West Airport Road

Linn County Assessor's Map No.: 12S-02W-15BD Tax Lots 1000

Site Size: ±1.02-acres

Existing Land Use: Single-Family Dwelling

Zone Designation: Residential Mixed Density (Z-RM) with AS-OZ overlay

Comprehensive Plan Designation: Residential Mixed Density (C-RM)

Surrounding Zoning: North: Residential Mixed Density (Z-RM)
South: Z-RM/UGA-UGM-10
East: Z-RM
West: Z-RM

Surrounding Uses: North: Residential
South: Residential/Commercial
East: Residential
West: Residential



22-042 Athwal
2010 Stoltz Hill Road
CP, ZC, CU, ADMIN.,

August 21, 2023
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I. Executive Summary

The subject property, 911 West Airport Road (Linn County Tax Assessor's Map No. 12S-02W-15BD Tax Lot 1000) is located northwest of the Stoltz Hill Road and West Airport Road intersection.

The subject property is currently improved with an existing single-family dwelling with associated site improvements. All existing improvements are proposed to be razed for redevelopment of a new convenience store, three-pump gas station, and associated site improvements.

Under consideration are the following applications:

1. Comprehensive Plan Map Amendment to change the designation of the subject property from Residential Mixed Density (C-RM) to Mixed Use (C-MU).
2. Zone Map Amendment to change the zone designation of the subject property from Residential Mixed Use (Z-RM) to Mixed Use (Z-MU).
3. Conditional Use (Class III) approval of a three-pump gas station.
4. Administrative Review (Class II) of a 4,074 square foot convenience store with associated parking and site improvements.

I. Code Analysis – Chapter 16.27

1. Chapter 16.27 contains requirements for map amendments, including both the Comprehensive Plan map and Zoning map. Section 16.27.010 addresses the Chapter's purpose while Section 16.27.020 establishes the authority to request map amendments.

FINDING: The Plan and Zoning maps may be amended over time and the property owner/applicant has the authority to request change in a property's Plan and Zoning map. Therefore, this proposal conforms to these two Sections.

2. The Planning Commission cannot consider a Plan or Zone map amendment within the one-year period immediately following a previous denial (Section 16.27.040).

FINDING: A denial of a Plan or Zone map amendment has not been issued. Therefore, this section is not applicable.

3. Sections 16.27.050 and 16.28.090 establish the decision criteria for reviewing a Plan map or Zone map amendment.

FINDING: Sections 16.27.050(A) and 16.28.090 contain the relevant criteria, which are reviewed in the following sections.

4. Section 16.27.060 describes the application process and submittal requirements. Subsection "A" states the request requires hearings before the Planning Commission and City Council. The Commission provides a recommendation to the Council and the Council makes the final decision. Subsection "B" establishes the application requirements.

FINDING: The City is obligated to follow the requirements under Section 16.27.060(A) and does so via the public hearing process. The application must be deemed complete, in this case, pursuant to provisions in Section 16.27.060(B) prior to commencing the public hearing process.

(Note: Chapter 16.27 does not include Section 16.27.070.)

5. Section 16.28.090 establishes the decision criteria for Plan map and Zone map amendments. This Section states the city may approve a Comprehensive Plan Map or Zoning Map Amendment request if it satisfies all relevant Decision Criteria cited in Section 16.27.050.

FINDING: Subsection "A" contains the relevant criteria, which are reviewed in the following Sections.

II. **Comprehensive Plan and Zone Map Amendment - Code Criteria**

Section 16.27.050 of the Lebanon Development Code (LDC) includes three review criteria that must be met for Comprehensive Plan and Zone Map amendments to be approved. The criteria are the same for each amendment, and as such, the findings have been combined. Code criteria are written in **bold** and are followed by findings and conclusions.

Criterion 1

1. **All proposed amendments to the Comprehensive Plan Map or to the Zoning Map shall be consistent with the City of Lebanon's adopted Comprehensive Plan and Facility Plans. The City's Facility plans, including the 2007 Transportation System Plan (TSP), are based on the future site service demands according to the Comprehensive Plan Map designation and associated zoning.**

FINDING: Through many of the Lebanon Goals and Policies are aspirational and not to be mistaken for, or interpreted as, approval criteria it is required that they be considered. They are to be used as a guide to aid decision makers. Where goals or policies conflict the decision makers need to weigh the evidence and decide which goal or policy the request satisfies.

Relevant Plan Goals and Policies

The proposed Plan map amendment to change Plan designation from RM to MU must satisfy long-range interests of the public as outlined in the Comprehensive Plan's goals and policies.

The following Comprehensive Plan goals and policies are relevant in considering whether the proposed MU designation is more supportive of the Comprehensive Plan, on balance, than the current RM designation. The relevant goals and policies are listed under the relevant Statewide Planning Goals and are shown in **bold** print followed by findings of fact and conclusions.

- a. Chapter 1: Introduction - The introductory Chapter describes the Comprehensive Plan, its relationship to the Statewide Land Use Goals, the Citizen Involvement program and key terminology.

Goals

G-1: Encouraging development in a planned and considered manner consistent with the community's general health, safety, and welfare.

G-6: Achieving public interest, access, understanding and support of the planning process and the goals toward which the process is directed.

G-7: Involving the citizens of the City in all phases of the planning process and to encourage effective neighborhood participation.

Policies

P-8: The adopted Comprehensive Plan Map, Comprehensive Plan, and implementing ordinances shall be reviewed periodically and may be revised and amended as needed to reflect changing needs and conditions within the planning area, as well as to address deficiencies in any needed land use zone.

P-9: All proposed revisions or amendments to the adopted policies shall be reviewed at public hearings before final action.

P-11: Only those statements that are listed in the section entitled "Policies" in Part Two at the end of each Chapter of this Comprehensive Plan shall have the status of being the official Policies of the City in relation to the subject matter of that chapter.

FINDINGS: Sections 16.28.080, 16.28.090, and 16.20.060 of the Lebanon Development Code (LDC) state a legislative review process with public hearings before the Planning Commission and City Council is required for comprehensive plan and zone map amendments. The review process for a Comprehensive Plan and Zone Map amendment provides an opportunity for citizen involvement. Notice of the public hearings are mailed published in the newspaper and provided to the Department of Land Conservation and Development. Additionally, there are opportunities for the public to review and obtain copies of the application materials and the completed staff report prior to the advertised public hearings. All members of the public have access to provide testimony and ask questions during the public review and hearing process.

- b. Chapter 2: Natural Environment - The Chapter addresses goals and policies related to the City's natural environment.

FINDINGS: The subject property has been previously developed and includes residential development. Designated natural resources are not known to be present on the subject property. There are no map wetlands on the city's local wetland inventory or national wetland inventory. The Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map shows the property outside of special flood hazard areas. Additionally, the property is not identified as a scenic or open space resource, or registered historic resources. Regardless, these goals and policies are incorporated in the Development Code criteria and will be reviewed at the time of development.

- c. Chapter 3: Urbanization - This Chapter provides the basic framework for future urban development within the city. Public Facilities Capability Policies

G-1: Providing for an orderly and efficient transition from rural to urban land use.

P-3: Support a flexible phased program for the orderly extension of water, wastewater, storm drainage and transportation services in response to land development proposals.

P-10: Review all development proposals to ensure that public facilities are available and have adequate capacity to accommodate the proposed development, or that such facilities and their capacities can be made available through appropriate extensions and/or enhancements concurrent with or prior to proposed developments.

P-30: Manage its Urban Growth Boundary and the lands within so as to make available sufficient land for the various uses to ensure choices in the marketplace, through implementation of land use regulations and land use policies.

P-31: Manage its Urban Growth Boundary and the lands within so as to encourage development within urban areas before conversion of urbanizable areas, through implementation of land use regulations and land use policies.

FINDINGS: The proposed amendments do not involve urbanization of any land not currently within the Lebanon Urban Growth Boundary. In addition, the site was previously developed with a residential dwelling, and as such, the land has already been urbanized.

- d. Chapter 4: Land Use- This Chapter details the goals and policies to assure the City provides different types of land within City limits that are suitable for a variety of uses. The following policies apply:

General Policies for Land Use

P-1: Recognize that the Comprehensive Plan land use designations or categories shall determine zoning.

P-6: Require that changes to the Comprehensive Plan Map be consistent with the policies of the Comprehensive Plan, State law, and any adopted intergovernmental agreements.

P-12: Ensure that the Zoning Map reflects and implements the Comprehensive Plan Map.

P-27: Allow future commercial and mixed-use centers within the Mixed Density Residential Zone based upon an approved development plan in conformance with the Planned Development standards and procedures of the Zoning Ordinance, provided access is from a designated highway or arterial and urban services are available.

P-40: Encourage a mix of commercial and residential uses within individual buildings, lots, and neighborhoods, in order to promote a compact, pedestrian friendly environment. Industrial uses should be allowed to mix with residential and commercial uses where there is limited potential for nuisance or jeopardy to the public health, safety, and welfare.

FINDINGS: The review process, evaluation, and determination of compliance with the Comprehensive Plan as part of the review process addresses compliance with Comprehensive Plan policies (P-6). The State effectively acknowledged the Comprehensive Plan, therefore, conformance with these policies assures conformance with state law. Compliance with Statewide Goals will be reviewed in another finding. For the subject property, the site is

currently limited for residential opportunity and development, and the change to the Mixed-Use designation would allow for a greater opportunity of a mix of commercial and residential uses within the neighborhood (P-40).

- e. Chapter 5: Population & Economy- This Chapter addresses population growth and economic development as well as those trends affecting both.

P-1: Monitor changes in demographic information to assure that the type, quantity, and location of services, facilities, vacant lands, and housing remain adequate to meet changing needs.

P-2: Include an analysis of the implications of the most current US Census information for the city in each update of the Comprehensive Plan.

P-5: Designate enough land in a variety of parcel sizes and locations to meet future employment and commercial needs.

FINDINGS: The 2004 Lebanon Urbanization Study, was completed as part of a periodic review work program and included an updated to the Buildable Land Inventories for Residential and Commercial/Industrial lands, an Economic Opportunities Analysis and housing needs analysis, and evaluated whether Lebanon has sufficient buildable land available to meet identified needs through 2025.

In this study, it was found that during the planning horizon of 2003-2025 there would be a deficit/surplus of land under the following plan designations: a 6.7-acres deficit of commercial land, 35.2-acres surplus of general industrial land, 267.9-acres surplus of Special Development land (i.e., mix use), and 1,123-acres surplus of Mixed Density Residential within the Lebanon Urban Growth Boundary basis on a conservative growth estimate.

Notably, at the time of the study there was found to be only 1.4-acres of commercial, 38-acres of general industrial, 142-acres of developable Special Development land (i.e., mix use), and 1,098-acres Mixed Residential land within the Lebanon city limits.

Although the data provided is near the end of the planning horizon reasonable assumptions can be made about the needed land supply under certain designations. Markedly, during the last several years Lebanon has experienced a period of growth in most sectors with the highest in new residential development and moderate redevelopment in commercial/industrial sectors.

Based on the amount of development that has occurred over the planning horizon and the amount of available redevelopable land (i.e., vacant, partially vacant, and unconstrained land) there is a far greater need for the special development/mixed use development designation which permits a variety of commercial and industrial uses. Furthermore, the proposed designation does not limit residential uses as most of the permitted uses under the current designation are still permitted under the proposed designation.

Additionally, the site is adjacent to a wide-ranging of land uses including, residential, commercial, industrial, and institutional development. This indicates a demand for a mix of land uses in this area and the ability for such uses to occur compatibly.

These plan designations are selected based on land supply/demand as well as proximity to major transportation corridors for ease of access, mobility, and visibility. Similarly, the subject site is bordered by West Airport Road and Stoltz Hill Road, which have an arterial street and collector street classification, respectively. This street pattern and proximity to higher volume roadways further suggest that this site is ideally situated for mixed-use development as opposed to residential development which may occur under the current zone designation.

- f. Chapter 6: Housing - This Chapter establishes the City's Goals and Policies related to Housing. The Chapter applies, as it concerns residential zoning.

FINDINGS: The following policies are applicable:

9.1 Residential Compatibility - This subsection considers placement of manufactured homes, location of neighbor commercial uses, and allowances for home occupations. The subject policies apply to the development of the site and are not directly related to the Plan and zone change requests.

9.2 Neighborhood Appearance - This subsection establishes screening policies for above ground utilities, the placement of street trees and fencing/landscaping provisions along collector and arterial streets. These policies apply to site development and do not address the matter of the Plan and zone change.

9.3 Housing Density and Affordability - This subsection allows for the creation of density bonuses, cooperation with various agencies to provide affordable housing, and ensure the Development Code provides the variety and type of housing required to meet the community's needs.

9.4 Housing and Open Space – This subsection notes adequate open space must be included in multifamily projects. The policy applies to the site development and not to the request.

9.5 Housing and Transportation Connectivity – The subsection includes policies on placement of schools in residential area; sidewalk and ADA requirements; placement of bikeways and pedestrian trails; development of local street standards and emergency vehicle access. These policies apply to specific development requirements and not to the Plan and zone change.

9.6 Housing, Public Utilities and Services – Policies call for adequacy of utilities to serve development and undergrounding of all utilities. Further, street lighting is required, street names should be approved by the Fire District and streets should align. Only Policy P-24 regarding public facility provisions directly applies to the request. As noted, services can be available to serve the property.

9.7 Refinement Plans – This subsection allows development of neighborhood refinement plans. This policy section does not apply to the request.

Generally, the policies related to housing are development related and do not directly apply to the request. Any development that would occur on the property would be required to be consistent with the development code. The Development Code was developed to implement the goals of the Comprehensive Plan, and as such, if the Map amendment is approved, any development to occur on the site would be consistent with the Comprehensive Plan and the Zoning Map.

- g. Chapter 7: Community Friendly Development & Preservation of Historic Resources - This Chapter focuses on policies creating a built environment suitable for the needs of a diverse population through a variety of uses scaled for the pedestrian, and capable of accommodating the automobile and mass transit. The following policies apply:

Community Friendly Development Policies

P-9: Encourage mixed uses within individual buildings, neighborhoods, and zoning districts were allowed by planning and building codes, and where there is no or only limited potential for incompatibility or conflict with public health, safety, and welfare.

P-10: Allow limited and appropriately scaled neighborhood commercial services in residential zones with appropriate standards to ensure compatibility.

FINDINGS: According to Table 4-1: City of Lebanon Comprehensive Plan Land Use Categories: The purpose of the C-MU district is as follows: *“To provide lands that possess potential for several types of land use or combinations of different land uses. The intent of this designation is to achieve an environment in which different land uses can co-exist by providing building groupings for privacy, usable and attractive open spaces, and safe circulation, thus promoting the general wellbeing of the residents, businesses, and other occupants. Mixed Development lands are open to all types of development including residential, commercial, and light industrial land uses.”*

As the purpose statement suggests, the MU zoning allows a mixture of residential and commercial development which is consistent with policies P-9 and P-10.

- h. Chapter 8: Transportation- This Chapter addresses the transportation needs of the city with an emphasis of creating a variety of transportation options for pedestrians, bicyclists, vehicles, and mass transit. The following policies apply:

Transportation System Planning Policies

P-12: The transportation system shall be managed to reduce existing traffic congestion and facilitate the safe, efficient movement of people and commodities within the community.

FINDINGS: The subject site is located to the northwest of the West Airport Road and Stoltz Hill Road intersection at 911 West Airport Road. West Airport Road and Stoltz Hill Road are classified in the 2018 Lebanon Transportation System Plan (TSP) as a Minor Arterial and Collector streets, respectively.

A future street project (no. D36) is identified in the 2018 Transportation System Plan (TSP). Upon its completion, the project will realign Stoltz Hill Road and add an intersection traffic signal. The proposed amendments will not impact the planned intersection improvement project.

Additionally, Section 6.12.010(B)(1) includes thresholds for when Traffic Impact Studies (TIS) and Traffic Impact Analysis (TIA) are warranted, which is typically in association with a development proposal. A TIA based on the proposed development was found to be warranted. Therefore, a TIA was completed by Kittelson and Associates is included as an attachment.

- i. Chapter 9: Public Facilities and Service - The City is required by State law to plan and develop a timely, orderly, and efficient arrangement of public facilities and services to serve as a framework for urban development. Goals and policies in this Chapter address those requirements. The following policies are applicable:

General Policies

P-8: Review all development proposals to ensure that public facilities are available and have adequate capacity to accommodate the proposed development, or that such facilities and their capacities can be made available through appropriate extensions and/or enhancements concurrent with or prior to proposed developments. (Duplicated in Chapter 3, Urbanization)

P-9: Require that new developments are either served by existing and/or proposed public infrastructure improvements, and/or are served by privately funded infrastructure extensions and improvements. (Duplicated in Chapter 3, Urbanization)

P-10: Consider impacts on key City-provided urban utility services (water, storm drainage, wastewater, and streets) and any other community facilities that are identified by service providers as substantially impacted by the proposal before-development proposals, or rezoning applications are approved.

FINDINGS: City water service and storm drainage are available in Airport Road along the frontage of the subject property. City Sewer service is available in Stoltz Hill Road along the frontage of the subject property. These facilities would be able to accommodate any development opportunity of the subject property.

- j. Chapter 10: Plan Implementation, Amendment, and Land Use Planning Coordination - This Chapter establishes procedures for amending the Comprehensive Plan Map and Zoning Map. Specific applicable policies include:

P-1: The City Council may amend the Comprehensive Plan and/or Map after referral to the Planning Commission public hearing, for action, review, revisions, and recommendations.

P-2: Changes to the Plan and/or Map shall be made by ordinance after public hearings as prescribed by state law and local ordinances.

P-3: Changes in the Plan and/or Map shall be incorporated directly into the document at the appropriate place. A list of all amendments with date of passage shall then become part of the document until the next comprehensive update of the entire Comprehensive Plan.

P-4: An amendment to the Comprehensive Plan and/or Map may be considered when one or more of the following conditions exist:

- a) Updated data demonstrates significantly different trends than previous data;*
- b) New data reflects new or previously undisclosed public needs;*
- c) New community attitudes represent a significant departure from previous attitudes as reflected by the Planning Commission or City Council;*
- d) Statutory changes significantly affect the applicability or appropriateness of existing plan policies.*

P-5: Residents, property owners, their authorized agents, the Planning Commission, the City Council, or City staff may initiate a Comprehensive Plan amendment. To obtain a Comprehensive Plan and/or Map amendment, the applicants shall have the burden of proof that all the following conditions exist:

- a) There is a need for the proposed change;*
- b) The identified need can best be served by granting the change requested;*
- c) The proposed change complies with the Statewide Planning Goals; and,*
- d) The proposed change is consistent with all other provisions of the City's Comprehensive Plan.*

FINDING: Policies P-1, P-2 and P-3 relate the processing of a Plan text or map amendment. The city is obligated to follow these requirements and does so with the public hearing process.

While the proposed zone change does not prescribe a specific development, it does allow greater options than the Residential Mixed Density designation and zone to meet those needs (P-4.a, P-5.a and P-5.b). Further, the establishment of the MU zone does not preclude development of the site for residential development.

Compliance with the Statewide Goals (P-5.c) is noted as follows:

1. Goal 1, Citizen Involvement: Public hearings will be held before both the Planning Commission and City Council. This is consistent with City procedures and the intent of the Goal.
2. Goal 2, Land Use Planning: The proposal does not involve exceptions to the Statewide Goals. Adoption actions are consistent with the acknowledged Comprehensive Plan and Development Code.
3. Goal 3, Agricultural Lands: This Goal does not apply, as the land is not designated farmland.

4. Goal 4, Forest Lands: This Goal does not apply, as the land is not designated forestland.
5. Goal 5, Open Spaces, Scenic and Historic Areas, and Natural Resources: The was previously fully developed with no known wetlands or historic areas. The site has not been designated for open space, a scenic area, or known for having natural resources. As such, this goal is not applicable. Upon any development of the site, the property owner is responsible for complying with state law, including compliance with the State Historic Preservation Office (SHPO).
6. Goal 6, Air, Water and Land Resource Quality: The map designation would change from a residential designation to mixed use. With the designation change, similar land use types are permissible in the mixed-use designation as the residential, with an expanded opportunity for additional commercial and industrial activities. The industrial uses they may be permissible in the mixed-use designation would be considered light industrial, oriented more toward light manufacturing. However, the size of the subject site would not be conducive to industrial use, but rather commercial or residential use. The applicable uses suited for the site would be similar in terms of air, water, and land resources under the mixed-use designation as the residential designation, therefore, there should be no significant impacts on air, water or resource quality than would otherwise occur.
7. Goal 7, Natural Hazards: The site is not located within any designated flood plain, does not contain steep slopes or contain any other identified natural hazard.
8. Goal 8, Recreational Needs: The proposed map amendments do not create uses which would adversely impact recreational opportunities.
9. Goal 9, Economic Development: The map amendments will allow a greater variety of uses thereby increasing development potential and providing opportunities for residential and commercial uses. Further, the proposed MU zone does not preclude employment-type activities on the site.
10. Goal 10, Housing: This Goal directly applies, as the map amendments create additional opportunities to meet the housing needs of the community. The city has an adopted Comprehensive Plan that projects the housing needs for the city through 2025. In addition, the city commissioned the 2019 Housing Needs Analysis which identified how the city can accommodate the projected housing needs over a 20-year period. The Housing Needs Analysis identified that the city had a surplus of land available to accommodate the projected housing needs. Specifically, a total of 2,503 housing units are identified as the housing need within the City's UGB, including 1,320 low density units, 643 medium density units and 540 high density units. To accommodate the need, a total of 437 acres of buildable land was needed to accommodate the housing demand. The city identified a total of 735 acres available, as such, there is a surplus of available housing. In terms of the surplus, there is 240 acres of low-density surplus, 0 acres of medium density surplus, and 63 acres of high-density surplus. The proposal is

to change the designation from Residential Mixed Density to Mixed Use, which would allow residential development subject to the Residential Mixed Density standards upon annexation. This change would maintain housing opportunities, as such, this proposal would comply with Goal 10.

11. Goal 11, Public Facilities and Services: Previous findings indicate services are available to serve the property and the map amendments will not affect the City's ability to provide necessary public facilities.
12. Goal 12, Transportation: Previous findings indicate the map amendments will not significantly affect planned transportation improvements.
13. Goal 13, Energy Conservation: The map amendments are neutral regarding energy conservation.
14. Goal 14, Urbanization: Previous findings indicate the change will still allow the city to meet housing needs of the community as the proposed MU zone does not preclude residential development.
15. Goals 15 to 19, Willamette River Greenway, Estuarine Resources, Coastal Shores, Beaches and Dunes, Ocean Resources: The proposals do not involve land within the Willamette Greenway or coastal areas.

Finally, all previous findings indicate the proposal complies with the applicable policies of the City's Comprehensive Plan (P-5.d).

Criterion 2

- k. **Section 16.27.050(A)(2) states that facility plans need to be consistent with the Comprehensive Plan Map as well as the text, and changes to the Map may necessitate changes to a facility plan. For example, changing a Comprehensive Plan Map designation to a higher intensity use may require an amendment to the TSP, sanitary sewer, or potable water master plans.**

FINDINGS: The property is currently designated as Residential Mixed Density, and the proposal includes a revised designation to Mixed Use. The Mixed-Use designation allows for a mix of uses including commercial, residential, and light industrial. The property is approximately 1.02 acres. The property is currently utilized as a residential development, and potential uses for the property under the Mixed-Use designation would be similar commercial uses, or small residential uses. Based on the size of the property, and the similar uses permitted and feasible on the property, the existing facility plans would not require amendment.

In addition, as part of the Transportation System Plan (TSP), there is a designated intersection improvement project to align Stoltz Hill Road and to add a traffic signal. The proposed map amendments would not result in a change to the TSP project or necessitate amendments to existing facility plans as the allowed and proposed uses are in line with the existing designation in terms of demand on facilities and would not impact the planned transportation project from moving forward.

- I. Section 16.27.050(A)(3) states applicants proposing amendments to the Zoning Map must request a City Zoning Classification that is consistent with the Comprehensive Plan Map designation for a subject property. If an applicant requests a City Zoning Classification that is not consistent with the Comprehensive Plan Map, the zoning requested shall not be granted until the Comprehensive Plan Map is first appropriately amended to reflect concurrence. (See the Annexation Zoning Matrix, Table 16.26-1.) Such an amendment requires a separate application, hearing, and decision; this process may occur concurrently with the Zoning Map Amendment hearing.

FINDINGS: The proposal is to amend the Comprehensive Plan Map and Zoning Map. With the Comprehensive Plan Map amendment to the Mixed-Use designation, the required zoning designation based on Table 16.26-1 of the Lebanon Development Code is Mixed Use, which is consistent with the proposal by the applicant. As such, the project complies with this decision criteria as the proposed zoning map amendment to Mixed Use is occurring with a concurrent comprehensive plan map amendment to Mixed Use.

Criterion 3

- m. **Section 16.27.050(B) states that if proposed amendments to the Comprehensive Plan Map or Zoning Map do not comply with the Comprehensive Plan, the Comprehensive Plan must first be amended so that the proposed Map amendment will be consistent with and accurately implement the Plan.**

FINDINGS: The Applicant requests approval of a Comprehensive Plan Map amendment to amend the site's Plan designation from C-RM to C-MU with a concurrent Zone Map Amendment to amend the zone designation from the Z-RM to Z-MU. The proposed Z-MU zone designation is consistent with the C-MU plan map designation.

Previous findings indicate the proposal may comply with the applicable Comprehensive Plan policies and does not require amendments to the Plan text. Therefore, this provision does not apply.

- n. Section 16.27.090 establishes requirements for Urban Growth Boundary (UGB) Amendments. This Section does not apply as the property lies entirely within the UGB.

III. Code Analysis – Chapter 16.21

With approval of the Comprehensive Plan Map and Zone Map amendments, the subject property will be zoned Mixed-Use. Per Lebanon Development Code (LDC) 16.06.060, Table 16.06-3, gas stations (filling stations) are permitted in the MU zone with Conditional Use review by the Planning Commission through a quasi-judicial review process.

According to LDC 16.21.050, the Conditional Use application submittal requirements include an application authorization form, site analysis map, proposed development plan, preliminary grading plan, and other information as needed to determine conformance with LDC 16.21.060.

According to LDC 16.20.040.B, the Administrative Review application submittal requirements include an application authorization form, information requested on the application form, and narrative statement.

II. Conditional Use - Review Criteria

LDC 16.21.060 includes the following review criteria that must be met for a conditional use application to be approved. Code criteria are written in **bold** and are followed by findings and conclusions.

Criterion 1

A. The application complies with all of the applicable provision of the underlying Land Use Zones (LDC Chapters 16.05 – 16.11), including building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other special standards as may be required for certain land uses.

1.1 With approval of the Comprehensive Plan Map and Zone Map amendments, the subject property will be zoned Mixed-Use (Z-MU).

1.2 The purpose of the MU zoning district according to Section 16.060.020.A is as follows:

“The purpose of the Mixed-Use Zone is to provide lands that possess potential for several types of land use or combinations of different land uses. The intent of this designation is to achieve an environment in which different land uses can co-exist by providing building groupings for privacy, usable and attractive open spaces, and safe circulation, thus promoting the general wellbeing of the residents, businesses, and other occupants. Effective mixed-use zones not only allow the co-location of various types of uses, but they also promote compatible architectural design and connectivity of buildings to streets and paths.”

1.3 The development standards for the MU zone are found in Table 16.06-7 and are as follows:

- a. There is no minimum lot size for uses other than residential. Therefore, this criterion is not applicable.
- b. Required yards and other site development standards are as required in the Z-RM, Z-NMU, Z-NCM, Z-CCM, Z-HCM, Z-IND, or Z-PU zone. Based on site location and composition the neighborhood commercial standards (Z-NCM) would be most appropriate:

A table of NCM development standards is provided below:

Standard	Neighborhood Commercial Zone (Z-NCM)
Maximum Height	25'
Maximum Building Coverage	60% (less setback area)



Minimum Setbacks

- (a) Front yards and street side yards shall be a minimum of 10'
- (b) Front yards or street side yards abutting a residential zone shall be 10'
- (c) Side yards abutting a residential zone shall be 10'
- (d) Rear yards abutting a residential zone shall be 10'

Building height is the vertical distance from grade to the average height of the height roof surface. According to the Building Elevation (Sheets A2.01 and A2.03), the overall height of the convenience store and fuel canopy is 17 feet, which is less than the maximum 25-foot building height in the NCM zoning district.

The property is in a developed neighborhood and abuts residential zoned property to the north, east, and west and public right-of-way to the south.

A setback is the required distance between a property line and a building, or other feature, in which development is restricted. As shown on the tentative site plan, the commercial structure, parking lot, and travel aisles will be setback at least 69 feet from the north; 6 feet from the east property line; 10-feet from the south property line; 10 feet from the west property line with proposed landscaping along all property boundaries.

Lot coverage is the area of the lot covered by a building, or buildings, expressed as a percentage of the total lot area. The site is ±1.02-acre and will be razed for redevelopment. The preliminary site plan shows ±4,074 square foot convenience store and 2,408 square foot fuel pump canopy at build-out, or 15 percent lot coverage. Therefore, the lot coverage of the site layout is below the most restrictive 60 percent maximum of the NCM zone.

- c. Clear vision area is as provided in Section 16.12.030.H.4. All access points must contain a clear vision area which consist of a triangular area, two sides of which are lot lines measured from the corner intersection of the street lot lines and the third side of which is a line across the corner of the lot joining the non-intersection ends of the other two sides. The clear vision area for corner lots is 20 feet along the right-of-way of each intersecting street. The clear vision area for a street-driveway intersection shall be 10 feet along the driveway from its intersection with the street right-of-way and 20 feet along the street right-of-way at the point of intersection with the driveway. As shown on the site plan included in the record, the clear vision areas meet the respective standards.

Criterion 2

- B. The site size, dimensions, location, topography, and access are adequate for the needs of the proposed use, considering the proposed building mass, parking, traffic, noise, vibration,**



exhaust/emissions, light glare, erosion, odor, dust, visibility, safety, and aesthetic considerations.

- 2.1 “Adequate” is not defined in the Lebanon Development Code but the Merriam Webster’s Collegiate Dictionary, Eleventh Edition, defines “adequate” as “(1) sufficient for a need or requirement.”
- 2.2 Proposed Use. The proposed use is a commercial gas station (filling station) consisting of three-fuel pumps.
- 2.3 Location. The subject property, 911 West Airport Road (Linn County Tax Assessor’s Map No. 12S-02W-15BD Tax Lot 1000) is located northwest of the Stoltz Hill Road and West Airport Road intersection.
- 2.4 Site Size, Dimensions, and Topography. The site is ±1.02-acre in size with a relatively flat with an elevation of 354 feet to 355 feet.
- 2.5 Access. The site has frontage on and access on West Airport Road and Stoltz Hill Road. West Airport Road and Stoltz Hill Road are classified as a minor arterial street and collector street, respectively. The accessway to West Airport Road is proposed to be 30 feet wide and right-in/right-out. The accessway to Stoltz Hill Road is proposed to be 36 feet wide with unrestricted access.
- 2.6 Building Mass: Mass refers to the size or physical bulk of a building, and can be understood as the actual size, or size relative to context.

A canopy is proposed over the fueling stations. The proposed canopy will be approximately 28-feet by 86-feet and 18 feet tall at its highest point.

The following design techniques will decrease any perceived bulk: 1) increased setbacks to property boundaries property; 2) reduced overall height; 3) building orientation.

- 2.7 Vehicle Parking: A parking standard for fueling stations is not found in Table 16.14.070-1: Off-Street Parking Requirements for Motor Vehicles and Bicycles by Types of Use. According to LDC 16.14.020.C, in instances where parking for a use is not specified, the Planning Official bases required parking upon the requirements of a similar use.

Parking demand for the proposed use is low due to the drive-up/drive-off nature of the fueling station business so parking is typically based on the net floor area of any related convenience store which will generate parking demand. Table Sections 16.14.070-1.6.e and 16.14.070-1.1.a of the LDC requires retail stores to have four vehicle parking spaces per 1,000 square foot of net floor area.

The proposed convenience store is approximately 4,074 square feet; therefore, 16 parking spaces are required. A total of 17 spaces are shown in designated parking stalls and the remainder are provided adjacent to the fueling pumps.

- 2.8 Bicycle Parking. A parking standard for a fueling station is not found in Table 16.14.070-1: Off-Street Parking Requirements for Motor Vehicles and Bicycles by Types of Use. Five

percent of required vehicle parking, or 4 spaces, whichever is greater is required for retail stores. A total of four bicycle parking spaces is required. A hooped bike rack accommodating four bicycle parking spaces is shown on the site plan next to the convenience store.

- 2.9 Noise. Noise generation will be minimal for the proposed gas station and will likely include noise associated with conversation and vehicle traffic.
- 2.10 Lighting and Glare. All on-site lighting will be directed downward and contained on-site. The standard can be verified at the time of a building permit or prior to the issuance of an occupancy permit.
- 2.11 Odor. The site is to be utilized for fueling stations and a convenience store. The odors on the site would be minimal and likely result from vehicle exhaust from on-site vehicle circulation and the fueling stations. There are no odors that would be generated that would result in required mitigation.
- 2.12 Hours of Operation. The site is expected to be open Monday through Friday with hours of operation not extending into the hours between 11 p.m. and 5:30 a.m. on weekdays, and 11 p.m. and 6 a.m. on weekends.
- 2.13 Vibrations and Dust: There is no activity that will occur on site that would create vibration. Proposed driving and parking surfaces will be paved; therefore, dust is not expected.
- 2.14 Privacy and Safety. A combination of fencing and landscaping will screen and buffer the proposed development from abutting residential property. The property owner and store employees will be responsible for regular monitoring to ensure the site is maintained, safe, and free of items that are prohibited or potentially harmful.
- 2.15 Buffering and Screening. LDC 16.15.020 and 16.15.030 require landscape buffering and screening to reduce the impacts on adjacent uses which are of a different type. For commercial development, the site area excluding building footprint and impervious surfaces are required to be landscaped. In addition, buffering and screening are required for parking and screening of mechanical equipment.

A low screen landscaping is proposed along the south and east property lines abutting West Airport Road and Stoltz Hill Road. A fully sight obscuring fence and 5-foot landscape buffer is proposed along the north and west property lines abutting the residential zoning property. A final landscaping plan will be provided prior to the issuance of a building permit in accordance with applicable standards under LDC 16.15.020.C.2.f.

- 2.16 Landscaping required. LDC 16.15.020 stipulates that all setbacks, exclusive of accessways and other permitted intrusions, shall be landscaped in accordance with LDC 16.15.030. Landscape is proposed adjacent to the required parking area, the north and east property lines abutting West Airport Road and Stoltz Hill Road, as well as the south and west property lines abutting the residential zoning property. A final landscaping plan will be

provided prior to the issuance of a building permit in accordance with applicable standards under LDC 16.15.020.C.2.f.

- 2.17 Irrigation System. LDC 16.15.040 requires that all required landscape areas be provided with a piped underground irrigation system unless a licensed landscape architect or certified nurseryman submits written verification that the proposed plant materials do not require irrigation. An irrigation plan will be provided prior to the issuance of a building permit in accordance with applicable standards under LDC 16.15.040.
- 2.18 Screening of Refuse Containers. LDC 16.15.020.2.f.2 requires that refuse containers and disposal areas be screened from view by placement of a sight-obscuring fence, wall, or hedge at least six feet tall. Prior to the issuance of an occupancy permit, the refuse containers will be screened in accordance with LDC 16.15.020.2.f.2.

Criterion 3

C. The negative impacts of the proposed use on adjacent property and on the public can be mitigated through the application of other Code standards, or other reasonable conditions of approval.

- 3.1 There are no known negative impacts of the proposed use on adjacent property or the public.
- 3.2 Aesthetic and privacy concerns, if any, will be addressed through site and building design, landscaping and screening standards as discussed above, under Criterion B, and incorporated here by reference.
- 3.3 Any concerns regarding parking, access, and the availability and capacity of public facilities, will be address via compliance with the City of Lebanon Development Code, Public Improvement Standards, Adopted Standards and Specifications for Construction, and other relevant sections of the Lebanon Municipal Code.
- 3.4 The proposed structure and site improvements compliance with applicable Code standards referenced under this criterion will be assured prior to development on the subject property through subsequent building or other permit processes. For example, compliance with many requirements of the Oregon Structural Specialty Code, Oregon Fire Code, and City off-street Parking and Access Standard can only be evaluated in conjunction with a building permit application for construction on the site. However, where it is anticipated obstacles to compliance with any of the referenced regulations exist, it is appropriate to identify and resolve those issues, to the extent practicable, through this land use decision. Therefore, this criterion is met or can be met with reasonable conditions of approval.

Criterion 4

D. All required public facilities have the capacity to serve the proposal.

- 4.1 The development proposal is to construct one building, a 4,074 square foot convenience store with associated site and parking lot improvements. The subject property, 911 West Airport Road (Linn County Tax Assessor's Map No. 12S-02W-15BD Tax Lot 1000) is located northwest of the Stoltz Hill Road and West Airport Road intersection.
- 4.2 The site will maintain frontage on and access to/from West Airport Road and Stoltz Hill Road. West Airport Road and Stoltz Hill Road are classified as a minor arterial street and collector street, respectively. The accessway to West Airport Road is proposed to be 30 feet wide and right-in/right-out. The accessway to Stoltz Hill Road is proposed to be 36 feet wide with unrestricted access.
- 4.3 Airport Road is classified as a minor arterial and is mostly constructed to City standards. Improvements include curb, gutter, and sidewalk along the south side, curb and gutter along the north side, and a travel lane in each direction and center turn lane. Needed improvements include a sidewalk along the north side.
- 4.4 Stoltz Hill Road is classified as a collector street and is fully constructed to City standards. Improvements include a travel lane in each direction.
- 4.5 According to LDC 16.20.110(B), the City or other road authority may require a Traffic Impact Study as part of an application for development when an increase in site traffic volume generation by 300 Average Daily Trips. A TIA based on the proposed development was found to be warranted. Therefore, a TIA was completed by Kittelson and Associates is included as an attachment.
- 4.6 Lebanon's Transportation System Plan (TSP) does not identify any level of service or safety problems along either frontage of the site.

Sanitary Sewer

- 4.7 According to LDC 16.16.030 adequate sanitary sewer infrastructure and service must be made available to serve each new development, and such facilities and service must comply with the City's Sanitary Sewer Facility Plan, and applicable construction specifications.
- 4.8 City utility maps show an eight-inch public sanitary sewer main in Stoltz Hill Road along the property frontage. A connection to the public sanitary sewer in Stoltz Hill Road is proposed.

Water

- 4.9 According to LDC 16.16.030 adequate water infrastructure and service must be made available to serve each new development, and such facilities and service must comply with the City's Water System Facility Plan, and applicable construction specifications.
- 4.10 City utility maps show a 12-inch public water main in Airport Road. A connection to the public water system in Airport Road is proposed.

Storm Drainage

- 4.11 According to LDC 16.16.040, the City may grant land use approval and issue a development permit when adequate provisions for storm water runoff are or will be made available in compliance with the City's Storm Drainage Master Plan and all applicable local, state, and federal standards.
- 4.12 All new site development must maintain pre-development peak historic storm water discharge rates as per City standards. The application for a development proposal shall demonstrate through calculations acceptable to the City Engineer that this standard will be met by the proposed development.
- 4.13 City utility maps show a 15-inch public storm drainage in Airport Road. A connection to the public storm drainage system in Airport Road is proposed.

Police Protection

- 4.14 The proposed development is served by the City of Lebanon Police Department.
- 4.15 No comments or concerns were provided by the City of Lebanon Police Department during the pre-application meeting concerning this development.

Fire Safety

- 4.16 The proposed development is served by the Lebanon Fire Protection District.
- 4.17 No comments were provided by the Lebanon Fire Protection District during the pre-application. However, the provisions of the Oregon Fire Code will be verified at the time of a building permit and/or prior to the issuance of an occupancy permit.

Criterion 5

E. Existing conditions of approval required as part of a prior land use decision shall be met.

- 5.1 The subject property was annexed into the city of Lebanon and designed as Residential Mixed Density on the Lebanon Comprehensive Plan Map and Zone Map. There were no conditions of approval associated with this land use decision; therefore, this criterion is not applicable.

Criterion 6

F. The applicant shall be required to upgrade any existing development that does not comply with the applicable land use district standards, in conformance with Chapter 16.30, Non-Conforming Uses and Development.

- 6.1 The existing development is an existing single-family residence which conforms with the existing Residential Mixed Density Zone. Proposed development complies with applicable development code standards as outlined under the review criteria for each application and incorporated herein by reference; therefore, this criterion is met.

Criterion 7

G. In the case of a use existing prior to the effective date of this ordinance and classified in this Code as requiring a Conditional Use Hearing, a change in use, or in lot area or an alteration or enlargement of structure shall conform to the requirements for Conditional Use review.

7.1 The existing development is a single-family residence. The proposed fueling station use requires approval of a conditional use permit. Compliance with the Conditional Use review criteria is outlined in this section and incorporated herein by reference; therefore, this criterion is met.

Criterion 8

H. The application complies with all of the Community Development Standards in LDC Chapter 16.12 – 16.19.

8.1 Findings pertaining to Chapters 16.12 – 16.16 are provided under Section B and D above and incorporated herein by reference.

8.2 Chapters 16.17 and 16.18, regarding wireless communication facilities and signage, are not applicable as none are proposed with this application.

8.3 Chapter 16.19 regarding other community development and use standards, are not applicable as none are proposed with this application.

IV. Code Analysis – Chapter 16.20

With approval of the Comprehensive Plan Map and Zone Map amendments, the subject property will be zoned Mixed-Use. Per Lebanon Development Code (LDC) 16.06.060, Table 16.06-3, commercial stores are permitted in the MU zone via administrative review by the Planning Official or Planning Commission by referral from the Planning Official. In this case, all required development applications have been submitted for concurrent review and will be processed in accordance with the highest review procedure, which is quasi-judicial review.

The Administrative Review processes contained in LDC 16.20.040(A)-(C) identifies the application requirements and procedures. LDC 16.20.040(D)(1), requires a written decision addressing all relevant criteria, allowing the decision authority to approve, approve with conditions, or deny the application. LDC 16.20.040(D)(2) states Administrative Reviews involving the establishment of a new structure or facility, or the expansion of an existing structure or facility, shall be subject to the following specific criteria.

V. Administrative Review – Review Criteria

Criterion 1

1. LDC 16.20.040(D)(2)(a) - The proposal shall conform to use, height limits, setbacks, and similar development requirements of the underlying zone.

FINDING: The development proposal is to construct a 4,074 square foot convenience store with three-fuel pumps with associated site and parking lot improvements. The subject property,

911 West Airport Road (Linn County Tax Assessor's Map No. 12S-02W-15BD Tax Lot 1000) is located northwest of the Stoltz Hill Road and West Airport Road intersection.

The subject property is within the Residential Mixed-Use (Z-RM). A concurrent comprehensive plan map and zone map amendment to C-MU and Z-MU has been proposed with this application.

Use: According to LDC Chapter 16.06, Table 16.05-2, commercial uses of 15,000 square feet or less are permitted with Administrative Review in the MU zoning district and gas stations are permitted with Conditional Use approval.

The development standards for the MU zone are found in Chapter 16.06, Table 16.06-7 and are subject to neighborhood commercial zone standards as outlined below:

Lot Area and Dimensions: There is no minimum or maximum lot area or minimum lot size standards specified in Chapter 16.06, Table 16.06-7. However, LDC 16.06.110 includes special development standards for gas stations, which states a minimum of 12,000 square foot area for gas station development. Upon approval of the pending property consolidation application, the subject property will be ±1.02-acres (44,431 square feet).

Lot Coverage. Lot coverage is the area of the lot covered by a building or buildings expressed as a percentage of the total lot area. The preliminary site plan shows ±4,074 square foot convenience store and 2,408 square foot fuel pump canopy at build-out, or 15 percent lot coverage. Therefore, the lot coverage of the site layout is below the most restrictive 60 percent maximum of the NCM zone.

Setbacks: Required yards and other site development standards are as required in the Z-RM, Z-NMU, Z-NCM, Z-CCM, Z-HCM, Z-IND, or Z-PU zone. Based on site location and composition the neighborhood commercial standards (Z-NCM) would be most appropriate, which include a 10-foot front setback; 10-foot side yard setback; 10-foot street side yard setback; and 20-foot rear setback. A minimum 55-foot setback is also required from the center line of Airport Road per LDC Table 16.19.070-1.

As shown on the tentative site plan, the commercial structure, parking lot, and travel aisles will be setback at least 69 feet from the north; 6 feet from the east property line; 10-feet from the south property line; 10 feet from the west property line with proposed landscaping along all property boundaries.

In accordance with LDC 16.14.040(A)(3) and LDC 16.14.060(I), the proposed parking lot has a 5-foot landscape buffer and sight-obscuring fence. Therefore, the site meets the required setbacks of the MU zoning districts and the location and buffer standards for parking lots.

Buffering and Other Yard Requirements: LDC 16.09.110.5 establishes buffering, landscaping, and pedestrian/bicycle access. Buffering and pedestrian/bicycle access through required buffers are only required, when necessary, through Site Design Review, Conditional Use, and/or Planned Development Review, which is not applicable to this request. Therefore, only the landscaping requirements are applicable.

Landscaping: According to LDC 16.15.020(C)(2)(b)1, areas excluding building footprints and impervious surfaces shall be landscaped. A landscaping plan will be submitted prior to the issuance of a building permit in accordance with the standards of Chapter 16.15.

Site Layout and Design. The proposed development abuts a public right-of-way to the south and east, residential mixed-use zoning district to the north and west. Therefore, landscaping buffers along the building, or other visual or sound barriers are necessary to ensure compatibility.

Height. Building height is the vertical distance from grade to the average height of the height roof surface. As shown on the building elevation drawings the overall height of the convenience store and fuel canopy is 17-foot and 18 feet, respectively. Therefore, the building height will be less than the maximum 25 building heights in the NCM zoning district.

Criterion 2

2. LDC 16.20.040(D)(2)(b) - The proposal shall comply with applicable access and street improvement requirements in Chapters 16.12 and 16.13, respectively.

FINDINGS: According to LDC 16.12.020.A, LDC 16.12.030.F.3, LDC 16.12.030.L.4, commercial uses may be served by a minimum of 12-foot per travel lane.

The site will maintain frontage on and access to/from West Airport Road and Stoltz Hill Road. West Airport Road and Stoltz Hill Road are classified as a minor arterial street and collector street, respectively. The accessway to West Airport Road is proposed to be 30 feet wide and right-in/right-out. The accessway to Stoltz Hill Road is proposed to be 36 feet wide with unrestricted access.

The Lebanon Fire District reviewed the site plan and determined there is sufficient fire access to the site. The perimeter of the building can be served by a fire engine position on either Airport Road or Stoltz Hill Road.

Criterion 3

3. LDC 16.20.040(D)(2)(c) - The proposal shall comply with applicable parking requirements in Chapter 16.14.

FINDINGS: LDC 16.14.020.D states that when a structure is constructed, on-site vehicle and bicycle parking and loading spaces shall be provided in accordance with Section 16.14.070.

A parking standard for fueling stations is not found in Table 16.14.070-1: Off-Street Parking Requirements for Motor Vehicles and Bicycles by Types of Use. According to LDC 16.14.020.C, in instances where parking for a use is not specified, the Planning Official bases required parking upon the requirements of a similar use.

Parking demand for the proposed use is low due to the drive-up/drive-off nature of the fueling station business so parking is typically based on the net floor area of any related convenience store which will generate parking demand. Table Sections 16.14.070-1.6.e and 16.14.070-1.1.a

of the LDC requires retail stores to have four vehicle parking spaces per 1,000 square foot of net floor area.

The proposed convenience store is approximately 4,074 square feet; therefore, 16 parking spaces are required. A total of 17 spaces are shown in designated parking stalls and the remainder are provided adjacent to the fueling pumps.

Bicycle Parking. A parking standard for a fueling station is not found in Table 16.14.070-1: Off-Street Parking Requirements for Motor Vehicles and Bicycles by Types of Use. Five percent of required vehicle parking, or 4 spaces, whichever is greater is required for retail stores. A total of four bicycle parking spaces is required. A hooped bike rack accommodating four bicycle parking spaces is shown on the site plan next to the convenience store.

Criterion 4

4. LDC 16.20.040(D)(2)(d) - The proposal shall comply with applicable screening and landscaping provisions in Chapter 16.15.

FINDINGS: According to LDC 16.15.020(C)(2)(b)1, areas excluding building footprints and impervious surfaces shall be landscaped. A landscaping plan will be submitted prior to the issuance of a building permit in accordance with the standards of Chapter 16.15. Buffering and screening are also required for parking lots and service areas.

Buffering and Screening. LDC 16.15.020 and 16.15.030 require landscape buffering and screening to reduce the impacts on adjacent uses which are of a different type. For commercial development, the site area excluding building footprint and impervious surfaces are required to be landscaped. In addition, buffering and screening are required for parking and screening of mechanical equipment.

A low screen landscaping is proposed along the south and east property lines abutting West Airport Road and Stoltz Hill Road. A fully sight obscuring fence and 5-foot landscape buffer is proposed along the north and west property lines abutting the residential zoning property. A final landscaping plan will be provided prior to the issuance of a building permit in accordance with applicable standards under LDC 16.15.020.C.2.f.

Landscaping Required. LDC 16.15.020 stipulates that all setbacks, exclusive of accessways and other permitted intrusions, shall be landscaped in accordance with LDC 16.15.030. Landscape is proposed adjacent to the required parking area, the north and east property lines abutting West Airport Road and Stoltz Hill Road, as well as the south and west property lines abutting the residential zoning property. A final landscaping plan will be provided prior to the issuance of a building permit in accordance with applicable standards under LDC 16.15.020.C.2.f.

Irrigation System. LDC 16.15.040 requires that all required landscape areas be provided with a piped underground irrigation system unless a licensed landscape architect or certified nurseryman submits written verification that the proposed plant materials do not require irrigation. An irrigation plan or certification from a nurseryman will be provided prior to the

issuance of a building permit. An irrigation plan will be provided prior to the issuance of a building permit in accordance with applicable standards under LDC 16.15.040.

Screening of Refuse Containers. LDC 16.15.020.2.f.2 requires that refuse containers and disposal areas be screened from view by placement of a sight-obscuring fence, wall, or hedge at least six feet tall. Prior to the issuance of an occupancy permit, the refuse containers will be screened in accordance with LDC 16.15.020.2.f.2.

Criterion 5

5. **LDC 16.20.040(D)(2)(e) - Any required public facility improvements shall comply with provisions in Chapter 16.16.**

FINDINGS: Facilities are currently in place within adjacent rights-of-way. If required, all improvements and connections will conform to City Public Works and/or private service provider requirements.

Criterion 6

6. **LDC 16.20.040(D)(2)(f) - Where applicable, the proposal shall comply with development requirements within identified hazard areas and/or overlay zones.**

FINDINGS: The subject property is located outside of the steep slopes, riparian protection, and special transportation Area overlays.

Floodplain provisions are also not applicable as the site is in Zone X, outside of identified special flood hazard areas as shown on FEMA's Flood Insurance Rate Map #41043C0568G, dated September 29, 2010.

According to the National Wetland Inventory and shown on the site plan, there are no mapped wetlands on-site. Therefore, there will be no direct impacts to the wetland or wetland permits required for the proposed development.

According to Figures 16.11.020-1 to 16.11.020-3 the property is in the 494' horizontal surface, direct impact area of the Lebanon Airport's Airport Safety Zone (AS-OZ). Therefore, notice must be provided to the Oregon Department of Aviation.

Criterion 7

7. **LDC 16.20.040(D)(2)(g) - The proposal shall comply with the supplementary zone regulations contained in Chapter 16.19 or elsewhere in the Development Code.**

FINDINGS: This Chapter includes standards for a variety of activities and uses such as accessory uses, home occupations, exceptions to height limitations and so forth. None of these provisions are proposed with this application.

LDC 16.20.040(E)-(G), establish decision notice requirements and appeal provisions. For the record, these are administrative requirements applicable to the city, the applicant and those receiving a copy of the notice of decision.

III. OVERALL CONCLUSION

Based on the above analysis, the submitted applications meet all the applicable review criteria as outlined above.

IV. ATTACHMENTS

A. Architectural Plan Set

1. Cover Sheet, Sheet G0.01
2. Perspective Views, G3.01
3. Site Plan, A1.01
4. Level 01-Floor Plan, Sheet A1.21
5. Level 01-Pump Pad Plan, Sheet A1.23
6. Level 2 – Pump Pad Plan, Sheet A1.24
7. Elevations, Sheet A2.01
8. Elevations, Sheet A2.02
9. Elevations, Sheet A2.03

B. Civil Plan Set

1. Cover Sheet, Sheet C100
2. Existing Conditions, Sheet C101
3. Preliminary Civil Site Plan, Sheet CC200
4. Preliminary Grading and Drainage Plan, Sheet C300
5. Preliminary Utility Plan, Sheet C400

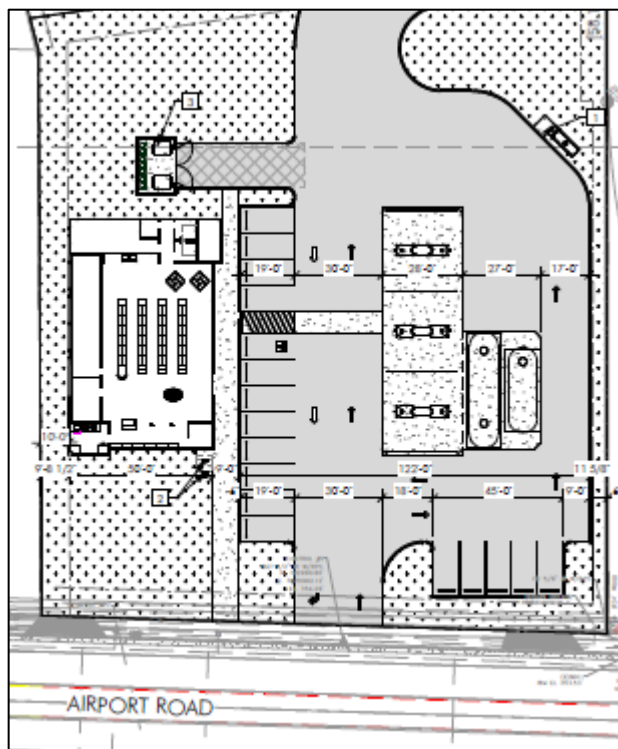
CITIZENS ARE INVITED TO PARTICIPATE in the public hearing and give written or oral testimony as described above that address applicable decision criteria during that part of the hearing process designated for testimony in favor of, or opposition to, the proposal. If additional documents or evidence are provided in support of the application subsequent to notice being sent, a party may, prior to the close of the hearing, request that the record remain open for at least seven days so such material may be reviewed.

Action of the Planning Commission and Appeals: The role of the Commission is to review the proposal and make a recommendation to the Lebanon City Council. A public hearing before the Council will be subsequently scheduled and notice provided. The Council decision is the final decision unless appealed to the Land Use Board of Appeals (LUBA). Failure to raise an issue in the hearing, orally or in writing, or failure to provide sufficient specificity to afford the decision makers an opportunity to respond to the issue precludes appeal to LUBA based on that issue.

Obtain Information: A copy of the application, all documents and evidence relied upon by the applicant, and applicable criteria are available for inspection at no cost and will be provided at the cost of 25 cents per single-sided page. If you have questions or would like additional information, please contact City of Lebanon Community Development Department, 925 Main Street; phone 541-258-4906; email cdc@lebanonoregon.gov

The meeting location is accessible to persons with disabilities. A request for an interpreter for the hearing impaired or for other accommodations for persons with disabilities should be made at least 48 hours before the meeting to 541-258-4906.

PROPOSED SITE PLAN





LAND USE APPLICATION

PROPERTY INFORMATION	
Site Address(es): 911 W AIRPORT RD	
Assessor's Map & Tax Lot No.(s): 12S02W15BD01000	
Comprehensive Plan Designation / Zoning Designation: C-RM / Z-RM	
Current Property Use: Signal Family Residential	
Project Description:	
Comprehensive Plan/Zone Map Amendments to change the designation of the subject property from C-RM to C-MU and Z-RM to Z-MU	
Conditional Use (Class III) approval of a three-pump gas station; Administrative Review (Class II) of a 4,074 square foot convenience store.	
APPLICANT / PRIMARY CONTACT INFORMATION	
Applicant: Laura LaRoque, Udell Engineering & Land Surveying, LLC	Phone: (541) 990-8661
Address: 63 E. Ash Street	Email: Laura@udelleng.com
City/State/Zip: Lebanon, OR 97355	
<i>I hereby certify that the statements, attachments, exhibits, plot plan and other information submitted as a part of this application are true; that the proposed land use activity does not violate State and/or Federal Law, or any covenants, conditions and restrictions associated with the subject property; and, any approval granted based on this information may be revoked if it is found that such statements are false.</i>	
APPLICANT SIGNATURE <i>Laura LaRoque</i>	Date: 8/21/23
PROPERTY OWNER INFORMATION (IF DIFFERENT THAN ABOVE)	
Owner: SRANNA JASWANT & ATHWAL SONA	Phone:
Address: 2342 KOKANEE WAY	Email:
City/State/Zip: LEBANON OR 97355	
OWNER SIGNATURE <i>[Signature]</i> <small>Soma athwal (Aug 21, 2023 13:37 PDT)</small>	Date: Aug 21, 2023
ADDITIONAL CONTACT INFORMATION	
Engineer / Surveyor: Brian Vandetta, Udell Engineering & Land Surveying, LLC	Phone: (541) 451-5125
Address: 63 E. Ash Street	Email: brianLaura@udelleng.com
City/State/Zip: Lebanon, OR 97355	
Architect: Leonard Lodder, Studio 3 Architecture, Inc.	Phone: (971) 239-0207
Address: 275 Court St. NE	Email: leonard@studio3architecture.com
City/State/Zip: Salem, OR 97301-3442	
Other:	Phone:
Address:	Email:
City/State/Zip	

THE CITY THAT FRIENDLINESS BUILT

REQUIRED SUBMITTALS

- Application and Filing Fee
- Narrative Describing the Proposed Development and addressing the Decision Criteria
 - LDC Article Two Land Uses and Land Use Zones
 - LDC Article Three Development Standards
 - LDC Article Four Review & Decision Requirements
 - LDC Article Five Exceptions to Standards (eg Variance, Non-Conforming Uses)
- Site Plan(s) drawn to scale with dimensions, Include other drawings if applicable
- Copy of current Property Deed showing Ownership, Easements, Property Restrictions

FOR OFFICE USE

***If more than one review process is required, applicant pays highest priced fee, then subsequent applications charged at half-price.**

Land Use Review Process		Fee	Land Use Review Process		Fee
X	Administrative Review	\$750		Planned Development – Preliminary	\$2,500
	Administrative Review (Planning Commission)	\$1,500		Planned Development – Final (Ministerial)	\$250
	Annexation	\$2,500		Planned Development – Final (Administrative)	\$750
	Code Interpretation	\$250		Planned Development – Final (Quasi-Judicial)	\$1,500
X	Comprehensive Plan Map/Text Amendment	\$2,500		Subdivision Tentative	\$1,500
X	Conditional Use	\$1,500		Subdivision Final	\$750
X	Fire District Plan Review	\$100		Tree Felling Permit (Steep Slopes only)	\$150 + \$5/tree
	Historic Preservation Review or Register	Varies		UGB Amendment	Actual Cost
	Land Partition	\$750		Variance (Class 1 – Minor Adjustment)	\$250
	Ministerial Review	\$250		Variance (Class 2 – Adjustment)	\$750
	Non-Conforming Use/Development	\$750		Variance (Class 3)	\$1,500
	Property (Lot) Line Adjustment	\$250	X	Zoning Map Amendment	\$2,500

APPLICATION RECEIPT & PAYMENT

Date Received:	Date Complete:	Receipt No.:
Received By:	Total Fee:	File No.:

THE CITY THAT FRIENDLINESS BUILT

Technical Memorandum

Project# 24995.12

September 14, 2023

To: Shana Olson, Project Manager
City of Lebanon
925 Main Street
Lebanon, OR 97355

From: Matt Hughart, Poppy Yang, and Chris Brehmer, PE

CC: Ron Whitlatch, Lebanon Engineering Services Director

RE: Airport Road and Stoltz Hill Road – Transportation Assessment

SUMMARY

This letter presents a Traffic Impact Analysis addressing the transportation impacts associated with a proposed comprehensive plan and zoning map amendment that would change an existing 1.02 acre parcel from Residential Mixed Density (Z-RM) to Z-MU Mixed Use.

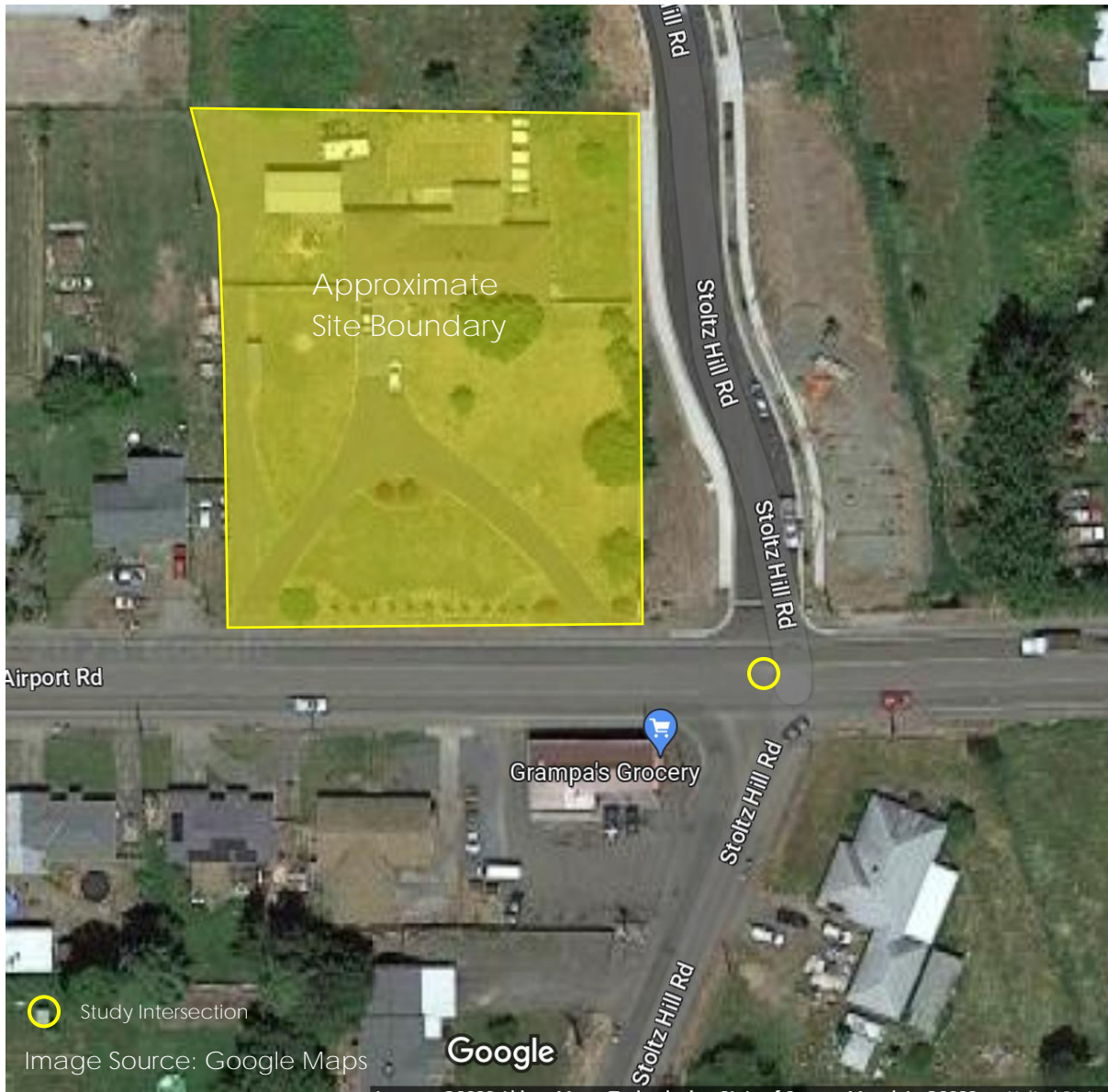
Based on the results of the transportation analysis outlined in this report, the proposed Mixed Use zone and the subsequent development of a 4,000 square foot convenience store/six pump fueling facility is not anticipated to result in a significant effect on the surrounding transportation network or require offsite transportation improvements. Additional details of our analyses are summarized herein.

INTRODUCTION

The 1.02-acre property is located in the northwest corner of the Airport Road/Stoltz Hill Road intersection (see Figure 1) and is currently zoned Residential Mixed Density. In order to support a proposed convenience store and fueling station, the owner is proposing to rezone the site to Mixed Use.

Modifications to existing zoning designations must be shown to meet the applicable criteria in Oregon Administrative Rule 660-012-0060, also known as the Transportation Planning Rule (TPR). Per the TPR, an analysis of whether the zoning overlay has the potential to create a significant effect to a transportation facility must be reviewed. The following report addresses the TPR requirements and the specific transportation-related impacts of a convenience store and fueling station.

Figure 1 – Site Vicinity Map and Study Intersection



STUDY SCOPE & ANALYSIS METHODOLOGY

The proposed land use action is a unique case in that the existing use of the property (residential use) already represents a reasonable maximum development scenario under the existing Residential Mixed Density zoning and the use of the property is already captured in the traffic counts. As such, the focus of this analysis is on the incremental impacts of the potential allowed uses under the proposed Mixed Use zone. In this case, the uses are assumed to include a convenience store and gas station.

STUDY SCOPE

This analysis identifies the transportation-related impacts associated with the application of the Mixed Use zone. The study was prepared in accordance with scoping direction from City staff. The study scope and overall

study area for this project were selected based on an analysis of current and future traffic volumes at study intersections and discussions with City staff. The analysis addresses the following:

- Existing land use and transportation system conditions within the site vicinity;
- Review of regional traffic growth, seasonal traffic patterns and planned transportation improvements;
- Site trip generation and distribution estimates for reasonable worst-case development scenario for the proposed Mixed Use zone;
- Planning horizon year 2039 traffic operations under existing Residential Mixed Density zoning and proposed Mixed Use zone scenarios;
- Transportation system adequacy to accommodate the proposed reasonable worst case development scenarios for the proposed Mixed Use zone;
- Assessment of zone change compliance with the TPR (OAR Section 660-12-060); and,
- Conclusions and recommendations.

STUDY INTERSECTION

The Airport Road/Stoltz Hill Road study intersection was identified as the sole study intersection in collaboration with City staff. Figure 1 illustrates the location of the study intersection. In addition to this intersection, the analysis includes operations at the proposed site access points. These will be discussed in greater detail later in this report.

TRAFFIC ANALYSIS TIME PERIODS

Study intersection operations were analyzed during the weekday morning (intersection peak hour between 7:00-9:00 AM) and evening peak hour (intersection peak hour between 4:00-6:00 PM).

ANALYSIS METHODOLOGY

All operational analyses described in this report were performed in accordance with the procedures stated in the *Highway Capacity Manual* (HCM). The 6th and 7th Editions of the HCM were used to assess study intersection operations during the peak 15 minutes of the peak hour. The peak hour factor (PHF) was derived from the existing manual turning movement counts and applied uniformly over each scenario. The operations analysis presented in this report was completed using Synchro 11 analysis software.

APPLICABLE MOBILITY STANDARDS

CITY OF LEBANON OPERATING STANDARDS

The City of Lebanon adopted the following mobility targets for all city-owned/maintained intersections.

- Signalized, All-way Stop, or Roundabout Controlled Intersections: The intersection as a whole must operate with a Level of Service (LOS) "E" or better and a v/c ratio not higher than 1.00 during the highest one-hour period on an average weekday (typically, but not always the evening peak period between 4 PM and 6 PM during the spring or fall).
- Two-way Stop and Yield Controlled Intersections: All intersection approaches during the highest one-hour period on an average weekday (typically, but not always the evening peak period between 4 PM and 6 PM during the spring or fall) shall operate with a volume-to-capacity (v/c) ratio not greater than 0.90.

EXISTING CONDITIONS

The existing conditions analysis identifies the site conditions and current physical and operational characteristics of roadways within the study area.

Site Conditions and Adjacent Land Uses

The site of the proposed rezone and development is currently occupied by a single-family home and bounded by existing residential to the west and north, Airport Road to the south, and Stoltz Hill Road (and a vacant parcel) to the east.

Transportation Facilities

Table 1 summarizes the characteristics of roadways within the site vicinity.

Table 1: Existing Transportation Facilities

Roadway	Functional Classification ¹	Number of Lanes	Posted Speed (mph)	Sidewalks	Bicycle Lanes	On-Street Parking
Stoltz Hill Road	Collector	2	25-35	Partial	None	No
Airport Road	Minor Arterial	3	25	None	Yes ²	No

¹ Per the City of Lebanon Transportation System Plan (TSP, 2019).

² An undesignated shoulder area is striped eastbound and westbound that could be used as a bike lane

Intersection Crash History

Intersection crash history was obtained and reviewed to identify potential safety issues in the site vicinity. Oregon Department of Transportation (ODOT) provided crash records for the Airport Road/Stoltz Hill Road intersection for the five-year period from January 1, 2016 through December 31, 2020. *Appendix A provides the ODOT crash report which provides more details on the reported crashes.* Table 2 summarizes the ODOT crash data.

Table 2: Reported Crash History (January 1, 2016 – December 31, 2020)

Study Intersection	Crash Type								Severity			Total
	Angle	Turn	Rear-End	Side Swipe	Fixed Object	Ped/Bike	Head-On	Other	PDO ¹	Injury	Fatal	
Airport Road/ Stoltz Hill Road	4	-	1	-	-	2	-	-	3	4	-	7

¹PDO = Property damage only

Intersection crash rates were calculated and compared to statewide crash rate performance thresholds following guidance in the ODOT Analysis Procedures Manual. For this analysis, the critical crash rate was calculated and compared to the 90th percentile crash rates for urban intersections by traffic control and configuration as shown in Table 3.

Table 3: Intersection Critical Crash Rate Assessment

Study Intersection Location	Critical Crash Rate by Intersection Type	Critical Crash Rate by Volume	Observed Crash Rate	Observed Crash Rate > Mean Crash Rate by Intersection	Observed Crash Rate > 90 th Percentile Rate
Airport Road/Stoltz Hill Road	0.50	0.38	0.30	No	No

¹Crash Rate reported as crashes per million entering vehicles (crashes/MEV).

Existing Traffic Conditions

Quality Counts collected turning movement counts at the study intersection in mid-September while local schools were in session. The counts were conducted on a typical mid-weekday during the morning peak period (7:00 – 9:00 AM) and evening peak period (4:00 – 6:00 PM). *Appendix B contains the traffic count data.*

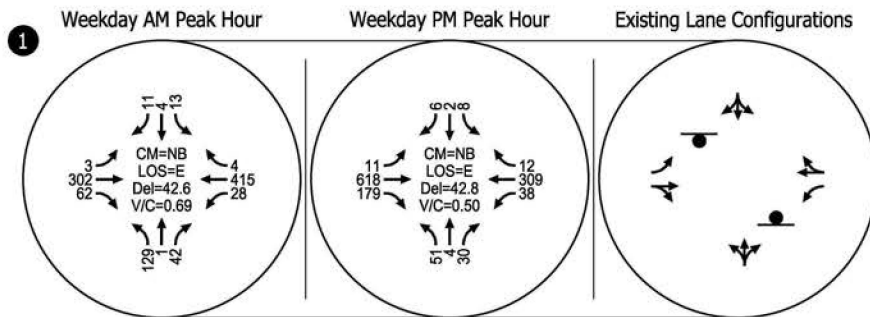
At the time the traffic counts were collected, the Lebanon Veterans Housing development located on Stoltz Hill Road north of Airport Road was only partially complete/occupied. As such, the measured volumes to/from the north leg of the intersection were replaced with the site-generated trip estimates from the *Lebanon Veterans Housing Transportation Impact Study* dated February 2020 to provide traffic volumes reflective of full buildout of the development.

Figure 2 illustrates existing lane configurations and traffic control devices, the existing intersection traffic volumes and the intersection operational performance during the weekday AM and PM peak hours. As shown, the study intersection currently operates in compliance with applicable City operational thresholds. *Appendix C contains the existing operations analysis worksheets.*

YEAR 2039 TRAFFIC CONDITIONS

This section of the report contains a detailed assessment of the long-term traffic impacts associated with and without the proposed zone change. More specifically, it evaluates the impacts of a convenience store/fueling station which would be allowed under the Mixed Use zone. The analysis of long-term traffic conditions is required by the State’s Transportation Planning Rule (TPR, OAR Section 660-12-0060), given that the proposed rezone would require an amendment to an acknowledged land use regulation and may have the potential to significantly affect a transportation facility.

To test for a significant effect and development-related impacts, an analysis of traffic conditions was conducted under the existing Residential Mixed Density designation (assuming continued use of the site as a single-family residence) and the proposed Mixed Use zone (assuming the development of a convenience store/fueling facility).



CM = INTERSECTION MOVEMENT
 LOS = INTERSECTION MOVEMENT LEVEL OF SERVICE
 Del = INTERSECTION MOVEMENT CONTROL DELAY
 V/C = INTERSECTION VOLUME-TO-CAPACITY RATIO

Existing Lane Configurations, Traffic Control Devices, & Volumes and Operations Weekday AM and PM Peak Hour, Lebanon, OR

Figure 2

Based on the required analysis, the impacts of traffic generated by the Mixed Use zone (using the proposed convenience store/fueling facility as a reasonable worst-case proxy) were examined in the following manner:

- Anticipated future traffic growth patterns were identified for the weekday AM and PM peak hour under the 2039 planning horizon year. This horizon year assumes no rezone and is indicative of future conditions with no land use modifications beyond those allowed under the Residential Mixed Density designation.
- A reasonable worst-case land development scenario (convenience store/fueling facility) was developed under the proposed Mixed Use zone. Estimates of average daily, weekday AM, and weekday PM peak hour site trips were prepared for the potential Mixed Use zone using the proposed convenience store/fueling facility.
- A site trip distribution pattern was derived through a review of existing traffic volumes and the site's proximity to the local and regional transportation network.
- Weekday AM and PM peak hour site-generated trips from the proposed convenience store/fueling facility were assigned to the surrounding streets and study intersection.
- Planning horizon year 2039 traffic volumes and operations were analyzed for the weekday AM and PM peak hour under existing background conditions and for the proposed Mixed Use zone designation.

ASSUMED INFRASTRUCTURE IMPROVEMENTS

The City of Lebanon is currently preparing design plans to construct a traffic signal at the Airport Road/Stoltz Hill Road intersection. Based on the likely design plans, it has been assumed that the signalization project will include a separate left-turn lane with at least 50 feet of storage and a shared through/right-turn lane on the northbound Stoltz Hill Road approach to Airport Road. For the purposes of the 2039 existing zoning and proposed zoning scenarios, these improvements have been assumed to be in place.

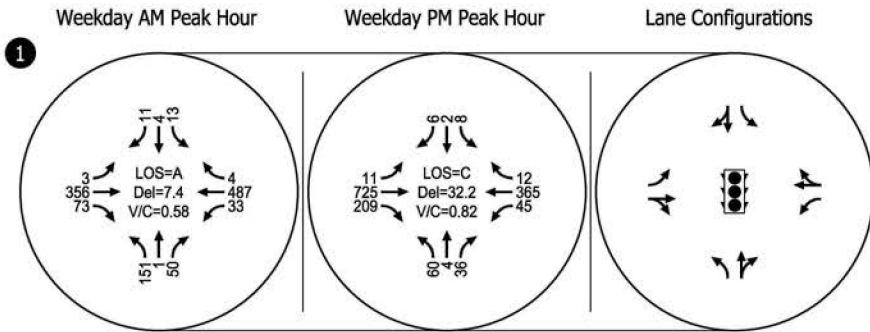
YEAR 2039 EXISTING ZONING SCENARIO TRAFFIC FORECAST

To achieve a reasonable estimate of existing zoning scenario traffic levels during the 2039 planning horizon year, a two percent per year growth rate was applied to the study intersection traffic volumes. This growth rate was derived through a review of Lebanon Transportation System Plan traffic volume forecasts.

The resulting year 2039 existing zoning scenario traffic volumes forecast for the weekday AM and PM peak hour are illustrated in Figure 3. The volumes shown reflect background traffic levels without any changes to the underlying zoning on the subject site.

YEAR 2039 EXISTING ZONING INTERSECTION OPERATIONS

Study intersection operations under the 2039 Existing Zoning Scenario were assessed to understand the base future year operations assuming no changes are made to the site zoning and the land continues to be used for residential purposes. Figure 3 summarizes the operational analyses for the weekday AM and PM peak hours reflective of anticipated regional and local traffic volume growth. As shown, the study intersection is forecast to continue to operate acceptably during both the weekday AM and PM peak hours. *Appendix D includes the 2039 existing zoning intersection operations analysis worksheets.*



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2039 Existing Zoning Traffic Conditions,
 Weekday AM and PM Peak Hours
 Lebanon, OR

Figure
 3

PROPOSED MIXED USE ZONE

Under the proposed Mixed Use zone, a 4,000 square-foot convenience store/6 pump fueling facility has been assumed for the site. Coincidentally, this use also represents a reasonable worst-case development scenario for the site considering the mix of allowed uses in the zone.

TRIP GENERATION

A trip generation estimate was prepared for the proposed convenience store/fueling facility based on information provided in the standard reference, *Trip Generation Manual, 11th Edition*, published by the Institute of Transportation Engineers (ITE). ITE land use code 945 (Convenience Store/Gas Station Vehicle – GFA (2-4k)) using six fueling positions as a basis for the estimate. The pass-by rates from the ITE 945 land use were subsequently applied to generate the net new trips from the site. Table 4 summarizes the estimates for the daily, weekday AM and weekday PM peak hours.

Table 4 – Trip Generation Estimate

Land Use	ITE Code	Size	Daily Trips	Weekday AM Peak Hour			Weekday PM Peak Hour		
				Total	In	Out	Total	In	Out
Convenience Store/Gas Station-GFA (2-4k)	945	6 fueling positions	1,590	96	48	48	112	56	56
Pass-By (62% (AM), 56% (PM))			-	58	29	29	62	31	31
<i>Total Net New Trips</i>			-	38	19	19	50	25	25

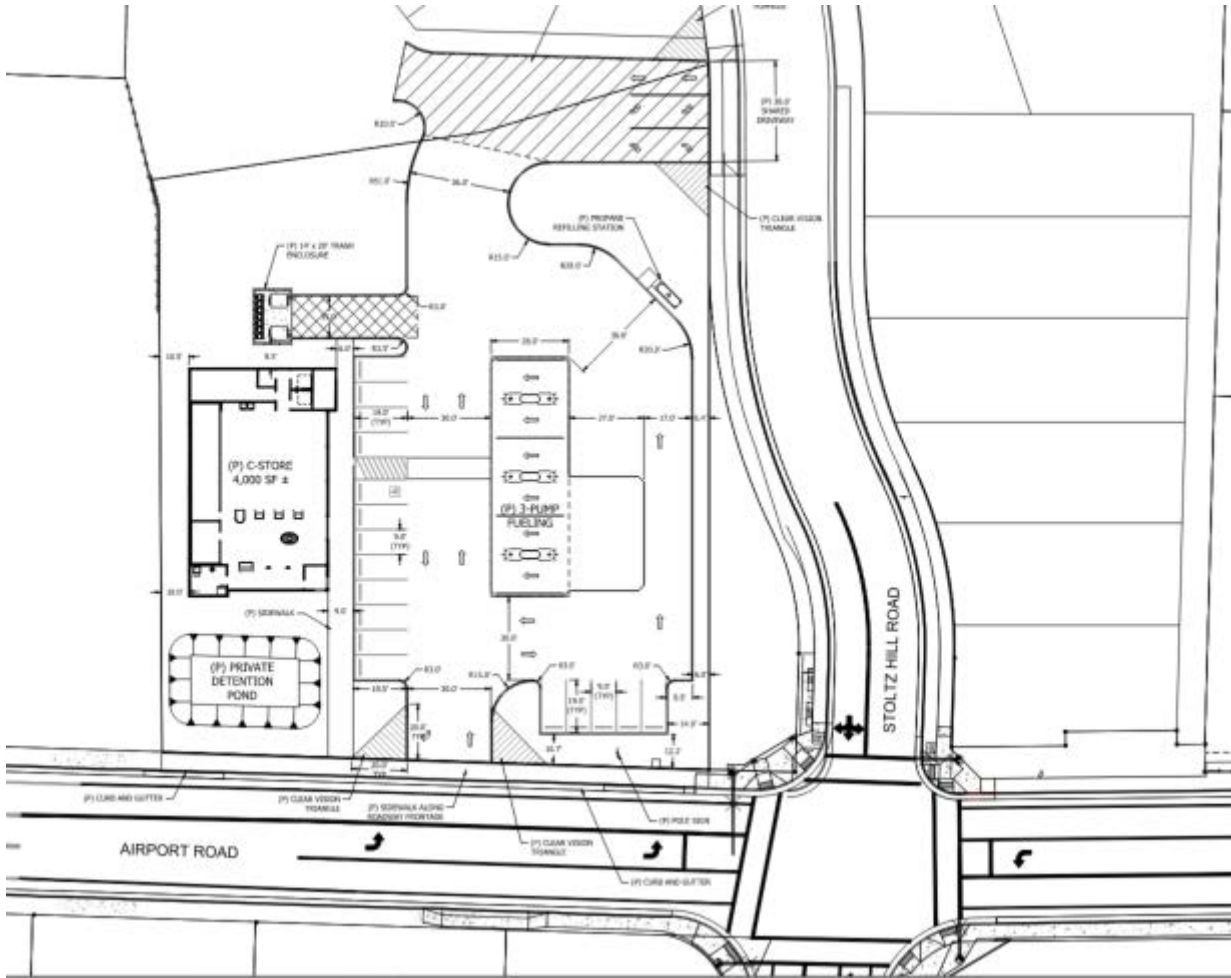
SITE TRIP DISTRIBUTION AND ASSIGNMENT

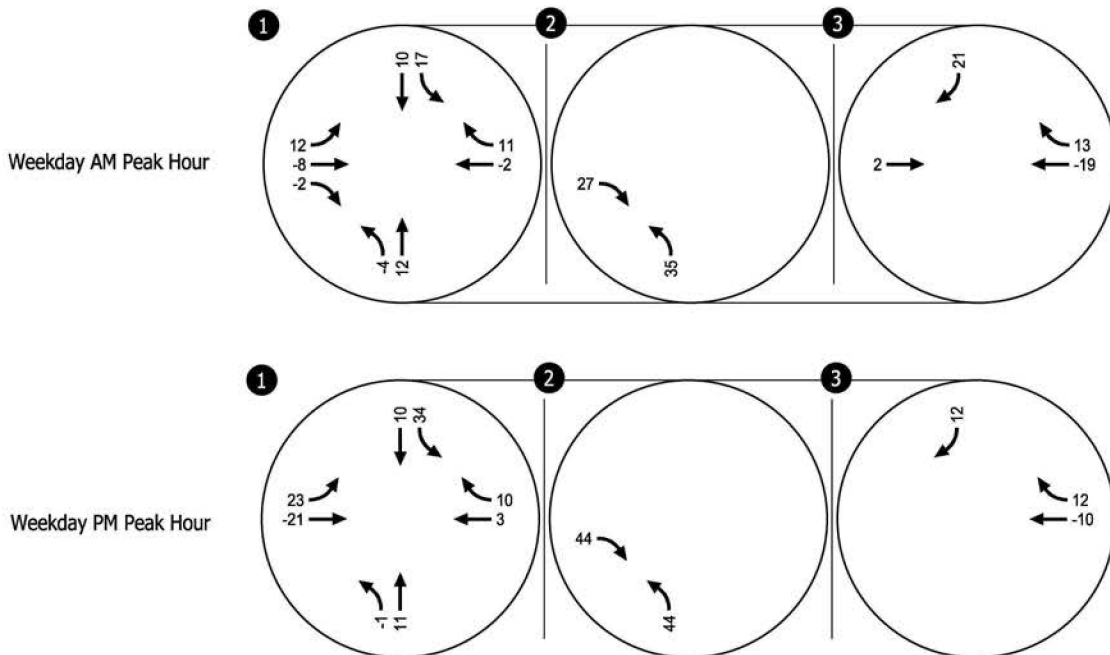
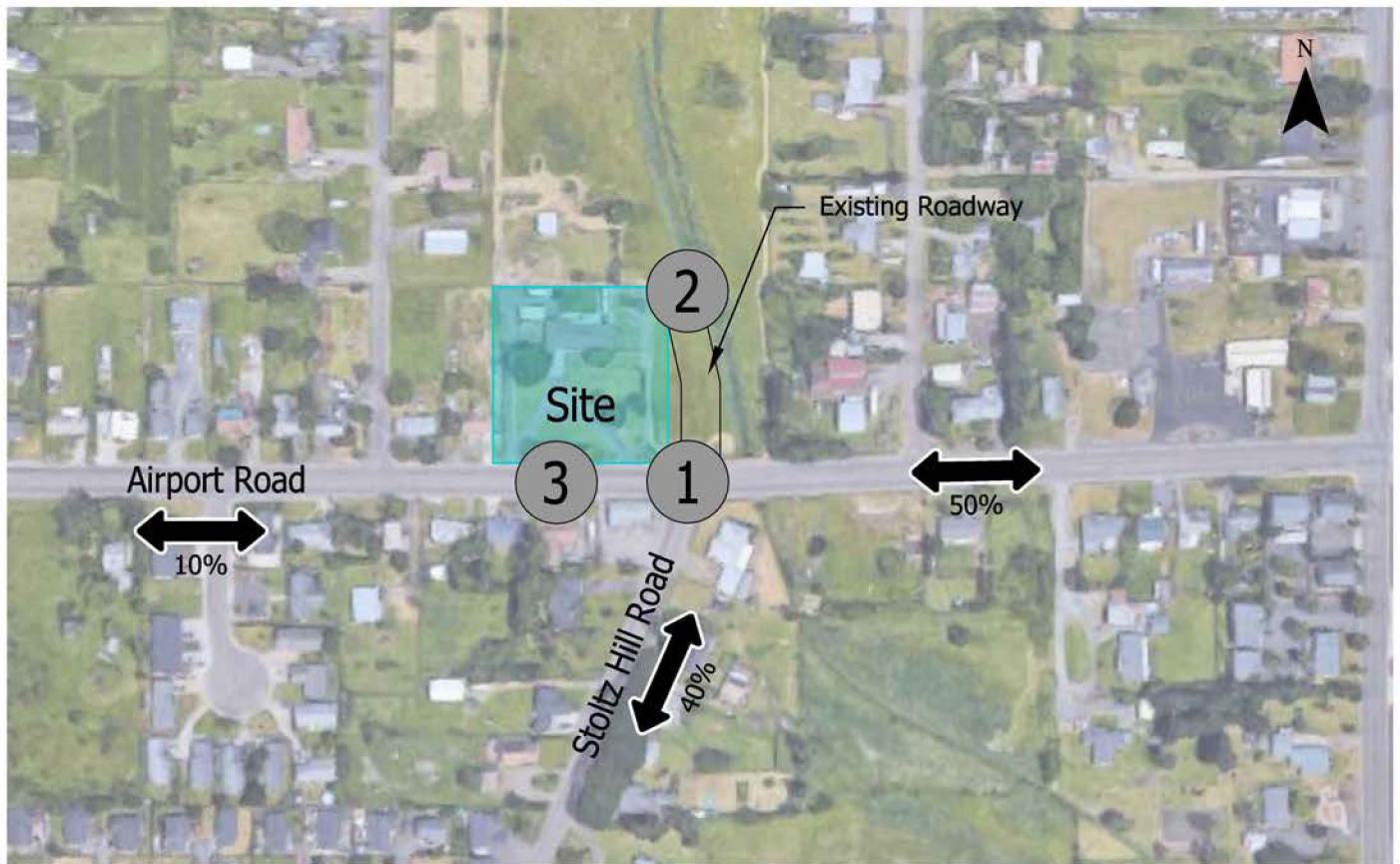
The site-generated trips from the proposed convenience store/fueling facility were distributed onto the study area roadway system assuming a proposed access scenario that includes a future right-in/right-out driveway connection to Airport Road and a full access driveway along the northern end of the site's Stoltz Hill Road frontage. The site layout and access scenario is shown in Exhibit 1. The local and regional distribution of site generated trips was determined via a combination of existing traffic patterns within the site vicinity. Figure 4 illustrates the resulting trip distribution pattern and site-generated trip assignment at the study intersection and assumed site access driveways.

YEAR 2039 MIXED USE ZONE INTERSECTION OPERATIONS

To reflect conditions anticipated under the proposed Mixed Use zone, the weekday AM and PM peak hour site generated traffic volumes shown in Figure 4 were added to the existing zoning traffic volumes shown in Figure 3 to arrive at the cumulative 2039 traffic volumes shown in Figure 5. Operations of the study intersections under 2039 conditions (with the site converted to a convenience store/fueling facility) are summarized in Figure 4 for the weekday AM and PM peak hours. As shown, the Airport Road/Stoltz Hill Road intersection and site driveways are forecast to continue to operate acceptably during both the weekday AM and PM peak hours. Appendix E includes the 2039 total traffic conditions intersection operations analysis worksheets.

Exhibit 1 – Site Layout and Access Scenario (Source: Udell Engineering and Land Surveying, LLC, August 2023)

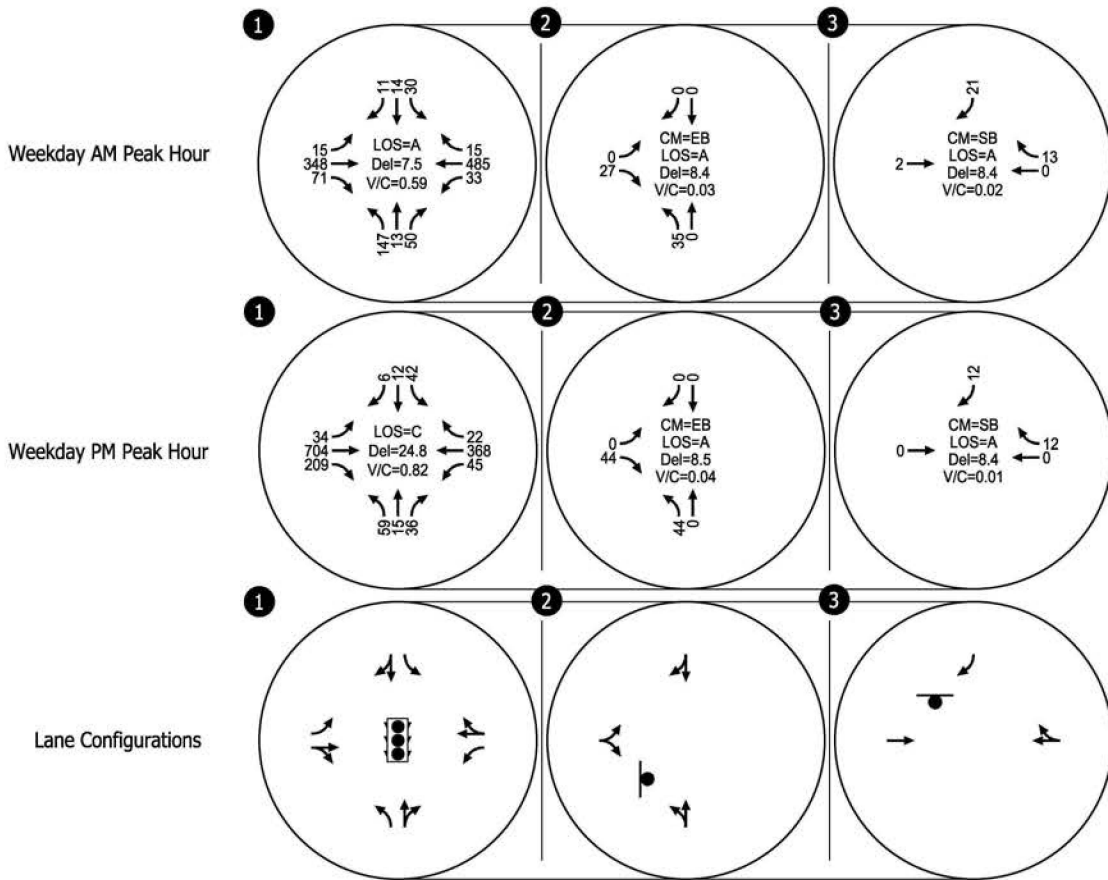




Estimated Trip Distribution Pattern and Site-Generated Trip Assignment
 Weekday AM and PM Peak Hour
 Lebanon, OR

Figure 4

H:\24\24995 - Lebanon TIA Review\012 - Airport Rd and Stoltz Hill Developments\report\figs\gas station\24995.012 Figures_gas station.dwg Sep 27, 2023 - 4:32pm - xyang Layout Tab: FIG4- Site Trips



CM = INTERSECTION MOVEMENT
 LOS = INTERSECTION MOVEMENT LEVEL OF SERVICE
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2039 Proposed Mixed Use Zone Traffic Conditions, Weekday AM and PM Peak Hours Lebanon, OR

Figure 5

TRANSPORTATION PLANNING RULE COMPLIANCE

This section addresses the Oregon Administrative Rule Section 660-12-0060 of the Oregon Transportation Planning Rule (TPR) requirements for the proposed zone change.

OAR Section 660-12-0060 Plan and Land Use Regulation Amendments of the TPR sets forth the criteria for evaluating plan and land use regulation amendments. The criteria establish the determination of significant effect on a transportation system resulting from a land use action; where a significant effect is identified, the criteria establish the means for achieving compliance. The relevant portion of this section of the TPR is reproduced below in italics followed by the response for this project in standard text.

660-12-0060 Plan and Land Use Regulation Amendments

(1) If an amendment to a functional plan, an acknowledged comprehensive plan, or a land use regulation (including a zoning map) would significantly affect an existing or planned transportation facility, then the local government must put in place measures as provided in section (2) of this rule, unless the amendment is allowed under section (3), (9) or (10) of this rule. A plan or land use regulation amendment significantly affects a transportation facility if it would:

(a) Change the functional classification of an existing or planned transportation facility (exclusive of correction of map errors in an adopted plan);

Response: The proposed Mixed Use zone will not require or result in any changes to the functional classification of any transportation facility in the vicinity of the site.

(b) Change standards implementing a functional classification system; or

Response: The proposed Mixed Use zone will not require changes to the standards that implement the functional classification system.

(c) Result in any of the effects listed in paragraphs (A) through (C) of this subsection. If a local government is evaluating a performance standard based on projected levels of motor vehicle traffic, then the results must be based on projected conditions measured at the end of the planning period identified in the adopted TSP. As part of evaluating projected conditions, the amount of traffic projected to be generated within the area of the amendment may be reduced if the amendment includes an enforceable, ongoing requirement that would demonstrably limit traffic generation, including, but not limited to, transportation demand management. This reduction may diminish or completely eliminate the significant effect of the amendment.

(A) Types or levels of travel or access that are inconsistent with the functional classification of an existing or planned transportation facility;

Response: The proposed Mixed Use zone would result in future traffic volumes that remain consistent with the functional classifications of the roadways in the study area.

(B) Degrade the performance of an existing or planned transportation facility such that it would not meet the performance standards identified in the TSP or comprehensive plan; or

Response: The proposed Mixed Use zone would not degrade operations of the study intersections below adopted performance targets.

NEAR-TERM SITE DEVELOPMENT ASSESSMENT

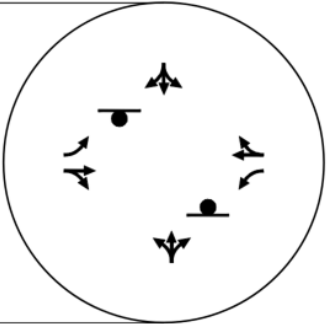
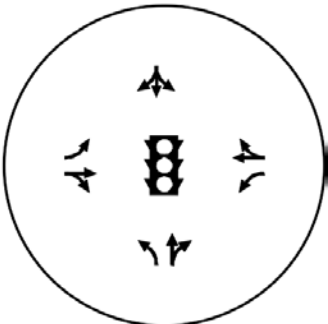
Based on the results of the proposed zoning scenario, the study intersections and site access driveways can operate acceptably assuming the development of 4,000 sq. ft. convenience store and fueling facility. To support a specific land use application for this operation, the following section includes a more detailed assessment of the development under an assumed 2024 buildout year.

Proposed Development Plan

As previously noted, the proposed development plan includes a 4,000 square foot convenience store and six fueling position gas station. A limited access right-in/right-out driveway is proposed off Airport Road and a full access driveway is proposed off the northern end of the site's Stoltz Hill Road frontage. Assuming the same trip generation outlined in Table 4 and the same site trip assignment shown in Figure 4, Table 5 summarizes the forecast operations of the Airport Road/Stoltz Hill Road intersection under the following two scenarios:

- The upcoming signalization and widening project is not fully complete by the time the proposed convenience store/fueling facility is assumed to be operational in 2024.
- The upcoming signalization project and northbound left-turn lane is fully complete and in place prior to opening of the proposed convenience store/fueling facility in 2024.

Table 5 - Airport Road/Stoltz Hill Road Intersection Mitigation Summary, 2024 Total Traffic Volumes

		Weekday AM Peak Hour				Weekday PM Peak Hour			
With Existing Unsignalized Airport Road/Stoltz Hill Road Lane Configuration									
	Operation	Approach	Lane	95 th Queue (ft)	Operation	Approach	Lane	95 th Queue (ft)	
	Critical NB Approach: LOS = F Del = 64.1 V/C = 0.83	NB	LT/TH/RT	175	Critical NB Approach LOS = F Del = 63.6 V/C = 0.67	NB	LT/TH/RT	100	
		SB	LT/TH/RT	50		SB	LT/TH/RT	75	
		EB	LT	25		EB	LT	25	
		WB	LT	25		WB	LT	25	
With Signalization and Separate Northbound Stoltz Hill Road Left-Turn Lane									
	Operation	Approach	Lane	95 th Queue (ft)	Operation	Approach	Lane	95 th Queue (ft)	
	LOS = A Del = 6.9 V/C = 0.53	NB	LT	25	LOS = B Del = 11.7 V/C = 0.72	NB	LT	25	
			TH/RT	25			TH/RT	25	
		SB	LT/TH/RT	25		SB	LT/TH/RT	25	
			EB	LT			25	EB	LT
		TH/RT		50		TH/RT	175		
		WB	LT	25		WB	LT	25	
			TH/RT	50			TH/RT	50	

Where: LT = Left-turn lane, LT/TH/RT = Shared left/through/right lane, TH/RT = Shared through/right lane

As shown in Table 5, assuming no signalization/widening, the critical northbound and southbound approaches are forecast to continue to meet City v/c standards during both the weekday AM and PM peak hours. However, **while not an official component of the City's adopted operating standards**, the critical northbound and southbound approaches are forecast to operate with higher levels of delay and long vehicle queues in the unsignalized scenario during both the weekday AM and PM peak hours than would otherwise exist under previous proposed development scenarios. Considering this increase in side-street delay, the anticipated change in traffic patterns through the Airport Road/Stoltz Hill Road intersection, and the regional nature of traffic demand along the Airport Road corridor, it is recommended that the planned signalization project be completed prior to the proposed convenience store/fueling facility being issued a certificate of occupancy.

CONVENIENCE STORE/FUELING FACILITY SITE ACCESS

As shown in the Table 5 queuing analysis, 95th percentile eastbound vehicle queues are projected to reach and extend past the proposed Airport Road site driveway upon signalization of the intersection. Eastbound queues will not only block the driveway from time to time, but the access will be located within the traditional influence area of a signalized intersection. Based on these considerations, turn movements at the Airport Road site access must be limited to right-turns only for safety reasons.

Given the **proposed driveway's proximity to the future signalized Airport Road/Stoltz Hill Road intersection and the need to limit movements at the access to right-turns only**, the following scenarios were examined:

- Scenario #1 – Install a non-traversable median barrier on Airport Road from the future signalized Stoltz Hill Road intersection to approximately 50 feet west of the convenience store/fueling facility site access. Key considerations associated with this treatment include:
 - The barrier would fully restrict the access driveway to right-in/right-out movements only and eliminate the need for a porkchop splitter island in the driveway throat (see Scenario #2 below).
 - The current center turn lane on Airport Road is approximately 11 feet wide. Construction of a non-traversable median would reduce the overall travel lane width to approximately 9-10 feet depending upon the median design and buffer space.
- Scenario #2 – Install a raised “porkchop” splitter island in the driveway throat of the convenience store/gas station site access driveway.
 - The porkchop splitter island would require a wider driveway, potentially impacting the proposed site layout.
 - Porkchop splitter islands are generally less effective at restricting left-in and left-out driveway movements.

While both options would help to better manage future site demand to the Airport Road corridor and reduce safety issues created by vehicle queue spillback, installation of a raised median is recommended due to its comparatively more effective access control ability.

PRELIMINARY SIGHT DISTANCE REVIEW

Intersection sight distance was preliminarily assessed at the proposed site driveway connection to Airport Road and Stoltz Hill Road. At the proposed Airport Road site access road intersection, Airport Road is flat and straight. As such, adequate sight distance is possible. To ensure adequate sight distance, it is recommended that the developer place and maintain all vegetation and other above ground signage in a manner that provides adequate sight distance per City standards.

At the proposed site access road intersection, Stoltz Hill Road is flat, but horizontal curvature is present. As such, adequate intersection sight distance is possible, but may be impacted by the final design of the site. To ensure

adequate sight distance, it is recommended that the developer place and maintain all vegetation and other above ground signage in a manner that provides adequate sight distance per City standards.

Preparation of a final sight distance evaluation is recommended at both the Airport Road and Stoltz Hill Road site driveways post construction to confirm adequate sight lines and setback distances are achieved in accordance with City standards.

SITE ACCESS TRAFFIC CONTROL

To accommodate future traffic movements on the site access driveways, a STOP (R1-1) sign should be installed on the southbound site driveway approach to Airport Road and the eastbound site driveway approach to Stoltz Hill Road in accordance with City standards and the *Manual on Uniform Traffic Control Devices* (MUTCD).

FINDINGS AND RECOMMENDATIONS

Based on the results of the transportation analysis outlined in this report, the proposed Mixed Use zone is not anticipated to result in a significant effect on the surrounding transportation network or require additional unplanned offsite mitigation. To support the land use application for a 4,000 sq. ft. convenience store and fueling facility, the following is recommended:

- In conjunction with the convenience store/fueling facility development, install a new STOP (R1-1) sign on the southbound site driveway approach to Airport Road in accordance with City standards and the *Manual on Uniform Traffic Control Devices* (MUTCD).
- In conjunction with the convenience store/fueling facility development, install a new STOP (R1-1) sign on the eastbound site driveway approach to Stoltz Hill Road in accordance with City standards and the *Manual on Uniform Traffic Control Devices* (MUTCD).
- In conjunction with the convenience store/fueling facility development, construct a non-traversable raised median barrier along north edge of the eastbound Airport Road left-turn lane from the Stoltz Hill Road intersection to approximately 50 feet west of the proposed site driveway.
- In conjunction with the planned near-term signalization of the Airport Road/Stoltz Hill Road intersection to be delivered by the City of Lebanon, it is recommend that the signalization project include the following design elements pending availability of adequate right-of-way along the west side of Stoltz Hill Road:
 - Incorporate a separate left-turn lane with at least 50 feet of storage and a shared through/right-turn lane on the northbound Stoltz Hill Road approach to Airport Road.
- To address the forecast high levels of delay associated with the northbound and southbound approaches and considering the anticipated change in traffic patterns through the Airport Road/Stoltz Hill Road intersection, it is recommended that the planned signalization project be completed prior to the proposed convenience store/fueling facility being issued a certificate of occupancy.

We trust this memorandum adequately addresses the traffic and circulation impacts. Please let us know if you have any questions regarding our analyses or need additional information.

APPENDIX

- A. ODOT Crash Data
- B. Traffic Count Data
- C. Existing Conditions Worksheets
- D. 2039 Existing Zoning Traffic Conditions Worksheets
- E. 2039 Proposed Zoning Traffic Conditions Worksheets
- F. Operations Analysis under 2024 Total Traffic Conditions Worksheets

Appendix A
ODOT Crash Data

OREGON DEPARTMENT OF TRANSPORTATION - POLICY, DATA AND ANALYSIS DIVISION
 TRANSPORTATION DATA SECTION - CRASH ANALYSIS AND REPORTING UNIT
 CRASH SUMMARIES BY YEAR BY COLLISION TYPE

Intersectional Crashes at Airport Rd & Stoltz Hill Rd in Linn County, OR.
 January 1, 2016 through December 31, 2020

COLLISION TYPE	FATAL CRASHES	NON- FATAL CRASHES	PROPERTY DAMAGE ONLY	TOTAL CRASHES	PEOPLE KILLED	PEOPLE INJURED	TRUCKS	DRY SURF	WET SURF	DAY	DARK	INTER- SECTION	INTER- SECTION RELATED	OFF- ROAD
YEAR: 2020														
TURNING MOVEMENTS	0	2	2	4	0	2	0	4	0	4	0	4	0	0
2020 TOTAL	0	2	2	4	0	2	0	4	0	4	0	4	0	0
YEAR: 2018														
REAR-END	0	1	0	1	0	1	0	1	0	1	0	1	0	0
TURNING MOVEMENTS	0	1	0	1	0	1	0	1	0	1	0	1	0	0
2018 TOTAL	0	2	0	2	0	2	0	2	0	2	0	2	0	0
YEAR: 2017														
TURNING MOVEMENTS	0	0	1	1	0	0	0	0	1	1	0	1	0	0
2017 TOTAL	0	0	1	1	0	0	0	0	1	1	0	1	0	0
FINAL TOTAL	0	4	3	7	0	4	0	6	1	7	0	7	0	0

Disclaimers: Effective 2016, collection of "Property Damage Only" (PDO) crash data elements was reduced for vehicles and participants. Age, Gender, License, Error and other elements are no longer available for PDO crash reporting. Please keep this in mind when comparing 2016 PDO crash data to prior years.

A higher number of crashes may be reported as of 2011 compared to prior years. This does not necessarily reflect an increase in annual crashes. The higher numbers may result from a change to an internal departmental process that allows the Crash Analysis and Reporting Unit to add previously unavailable, non-fatal crash reports to the annual data file. Please be aware of this change when comparing pre-2011 crash statistics. For all disclaimers, see https://www.oregon.gov/ODOT/Data/documents/Crash_Data_Disclaimers.pdf.

ACTION CODE TRANSLATION LIST

ACTION CODE	SHORT DESCRIPTION	LONG DESCRIPTION
000	NONE	NO ACTION OR NON-WARRANTED
001	SKIDDED	SKIDDED
002	ON/OFF V	GETTING ON OR OFF STOPPED OR PARKED VEHICLE
003	LOAD OVR	OVERHANGING LOAD STRUCK ANOTHER VEHICLE, ETC.
006	SLOW DN	SLOWED DOWN
007	AVOIDING	AVOIDING MANEUVER
008	PAR PARK	PARALLEL PARKING
009	ANG PARK	ANGLE PARKING
010	INTERFERE	PASSENGER INTERFERING WITH DRIVER
011	STOPPED	STOPPED IN TRAFFIC NOT WAITING TO MAKE A LEFT TURN
012	STP/L TRN	STOPPED BECAUSE OF LEFT TURN SIGNAL OR WAITING, ETC.
013	STP TURN	STOPPED WHILE EXECUTING A TURN
014	EMR V PKD	EMERGENCY VEHICLE LEGALLY PARKED IN THE ROADWAY
015	GO A/STOP	PROCEED AFTER STOPPING FOR A STOP SIGN/FLASHING RED.
016	TRN A/RED	TURNED ON RED AFTER STOPPING
017	LOSTCTRL	LOST CONTROL OF VEHICLE
018	EXIT DWY	ENTERING STREET OR HIGHWAY FROM ALLEY OR DRIVEWAY
019	ENTR DWY	ENTERING ALLEY OR DRIVEWAY FROM STREET OR HIGHWAY
020	STR ENTR	BEFORE ENTERING ROADWAY, STRUCK PEDESTRIAN, ETC. ON SIDEWALK OR SHOULDER
021	NO DRVR	CAR RAN AWAY - NO DRIVER
022	PREV COL	STRUCK, OR WAS STRUCK BY, VEHICLE OR PEDESTRIAN IN PRIOR COLLISION BEFORE ACC. STABILIZED
023	STALLED	VEHICLE STALLED OR DISABLED
024	DRVR DEAD	DEAD BY UNASSOCIATED CAUSE
025	FATIGUE	FATIGUED, SLEEPY, ASLEEP
026	SUN	DRIVER BLINDED BY SUN
027	HDLGHTS	DRIVER BLINDED BY HEADLIGHTS
028	ILLNESS	PHYSICALLY ILL
029	THRU MED	VEHICLE CROSSED, PLUNGED OVER, OR THROUGH MEDIAN BARRIER
030	PURSUIT	PURSUING OR ATTEMPTING TO STOP A VEHICLE
031	PASSING	PASSING SITUATION
032	PRKOFFRD	VEHICLE PARKED BEYOND CURB OR SHOULDER
033	CROS MED	VEHICLE CROSSED EARTH OR GRASS MEDIAN
034	X N/SGNL	CROSSING AT INTERSECTION - NO TRAFFIC SIGNAL PRESENT
035	X W/ SGNL	CROSSING AT INTERSECTION - TRAFFIC SIGNAL PRESENT
036	DIAGONAL	CROSSING AT INTERSECTION - DIAGONALLY
037	BTWN INT	CROSSING BETWEEN INTERSECTIONS
038	DISTRACT	DRIVER'S ATTENTION DISTRACTED
039	W/TRAF-S	WALKING, RUNNING, RIDING, ETC., ON SHOULDER WITH TRAFFIC
040	A/TRAF-S	WALKING, RUNNING, RIDING, ETC., ON SHOULDER FACING TRAFFIC
041	W/TRAF-P	WALKING, RUNNING, RIDING, ETC., ON PAVEMENT WITH TRAFFIC
042	A/TRAF-P	WALKING, RUNNING, RIDING, ETC., ON PAVEMENT FACING TRAFFIC
043	PLAYINRD	PLAYING IN STREET OR ROAD
044	PUSH MV	PUSHING OR WORKING ON VEHICLE IN ROAD OR ON SHOULDER
045	WORK ON	WORKING IN ROADWAY OR ALONG SHOULDER
046	W/ TRAFIC	NON-MOTORIST WALKING, RUNNING, RIDING, ETC. WITH TRAFFIC
047	A/ TRAFIC	NON-MOTORIST WALKING, RUNNING, RIDING, ETC. FACING TRAFFIC
050	LAY ON RD	STANDING OR LYING IN ROADWAY
051	ENT OFFRD	ENTERING / STARTING IN TRAFFIC LANE FROM OFF ROAD
052	MERGING	MERGING

ACTION CODE TRANSLATION LIST

ACTION CODE	SHORT DESCRIPTION	LONG DESCRIPTION
055	SPRAY	BLINDED BY WATER SPRAY
088	OTHER	OTHER ACTION
099	UNK	UNKNOWN ACTION

CAUSE CODE TRANSLATION LIST

CAUSE CODE	SHORT DESCRIPTION	LONG DESCRIPTION
00	NO CODE	NO CAUSE ASSOCIATED AT THIS LEVEL
01	TOO-FAST	TOO FAST FOR CONDITIONS (NOT EXCEED POSTED SPEED
02	NO-YIELD	DID NOT YIELD RIGHT-OF-WAY
03	PAS-STOP	PASSED STOP SIGN OR RED FLASHER
04	DIS SIG	DISREGARDED TRAFFIC SIGNAL
05	LEFT-CTR	DROVE LEFT OF CENTER ON TWO-WAY ROAD; STRADDLING
06	IMP-OVER	IMPROPER OVERTAKING
07	TOO-CLOS	FOLLOWED TOO CLOSELY
08	IMP-TURN	MADE IMPROPER TURN
09	DRINKING	ALCOHOL OR DRUG INVOLVED
10	OTHR-IMP	OTHER IMPROPER DRIVING
11	MECH-DEF	MECHANICAL DEFECT
12	OTHER	OTHER (NOT IMPROPER DRIVING)
13	IMP LN C	IMPROPER CHANGE OF TRAFFIC LANES
14	DIS TCD	DISREGARDED OTHER TRAFFIC CONTROL DEVICE
15	WRNG WAY	WRONG WAY ON ONE-WAY ROAD; WRONG SIDE DIVIDED RO
16	FATIGUE	DRIVER DROWSY/FATIGUED/SLEEPY
17	ILLNESS	PHYSICAL ILLNESS
18	IN RDWY	NON-MOTORIST ILLEGALLY IN ROADWAY
19	NT VISBL	NON-MOTORIST NOT VISIBLE; NON-REFLECTIVE CLOTHIN
20	IMP PKNG	VEHICLE IMPROPERLY PARKED
21	DEF STER	DEFECTIVE STEERING MECHANISM
22	DEF BRKE	INADEQUATE OR NO BRAKES
24	LOADSHFT	VEHICLE LOST LOAD OR LOAD SHIFTED
25	TIREFAIL	TIRE FAILURE
26	PHANTOM	PHANTOM / NON-CONTACT VEHICLE
27	INATTENT	INATTENTION
28	NM INATT	NON-MOTORIST INATTENTION
29	F AVOID	FAILED TO AVOID VEHICLE AHEAD
30	SPEED	DRIVING IN EXCESS OF POSTED SPEED
31	RACING	SPEED RACING (PER PAR)
32	CARELESS	CARELESS DRIVING (PER PAR)
33	RECKLESS	RECKLESS DRIVING (PER PAR)
34	AGGRESV	AGGRESSIVE DRIVING (PER PAR)
35	RD RAGE	ROAD RAGE (PER PAR)
40	VIEW OBS	VIEW OBSCURED
50	USED MDN	IMPROPER USE OF MEDIAN OR SHOULDER
51	FAIL LN	FAILED TO MAINTAIN LANE
52	OFF RD	RAN OFF ROAD

COLLISION TYPE CODE TRANSLATION LIST

COLL CODE	SHORT DESCRIPTION	LONG DESCRIPTION
&	OTH	MISCELLANEOUS
-	BACK	BACKING
0	PED	PEDESTRIAN
1	ANGL	ANGLE
2	HEAD	HEAD-ON
3	REAR	REAR-END
4	SS-M	SIDESWIPE - MEETING
5	SS-O	SIDESWIPE - OVERTAKING
6	TURN	TURNING MOVEMENT
7	PARK	PARKING MANEUVER
8	NCOL	NON-COLLISION
9	FIX	FIXED OBJECT OR OTHER OBJECT

CRASH TYPE CODE TRANSLATION LIST

CRASH TYPE	SHORT DESCRIPTION	LONG DESCRIPTION
&	OVERTURN	OVERTURNED
0	NON-COLL	OTHER NON-COLLISION
1	OTH RDWY	MOTOR VEHICLE ON OTHER ROADWAY
2	PRKD MV	PARKED MOTOR VEHICLE
3	PED	PEDESTRIAN
4	TRAIN	RAILWAY TRAIN
6	BIKE	PEDALCYCLIST
7	ANIMAL	ANIMAL
8	FIX OBJ	FIXED OBJECT
9	OTH OBJ	OTHER OBJECT
A	ANGL-STP	ENTERING AT ANGLE - ONE VEHICLE STOPPED
B	ANGL-OTH	ENTERING AT ANGLE - ALL OTHERS
C	S-STRGHT	FROM SAME DIRECTION - BOTH GOING STRAIGHT
D	S-1TURN	FROM SAME DIRECTION - ONE TURN, ONE STRAIGHT
E	S-1STOP	FROM SAME DIRECTION - ONE STOPPED
F	S-OTHER	FROM SAME DIRECTION-ALL OTHERS, INCLUDING PARKING
G	O-STRGHT	FROM OPPOSITE DIRECTION - BOTH GOING STRAIGHT
H	O-1 L-TURN	FROM OPPOSITE DIRECTION-ONE LEFT TURN, ONE STRAIGHT
I	O-1STOP	FROM OPPOSITE DIRECTION - ONE STOPPED
J	O-OTHER	FROM OPPOSITE DIRECTION-ALL OTHERS INCL. PARKING

DRIVER LICENSE CODE TRANSLATION LIST

LIC CODE	SHORT DESC	LONG DESCRIPTION
0	NONE	NOT LICENSED (HAD NEVER BEEN LICENSED)
1	OR-Y	VALID OREGON LICENSE
2	OTH-Y	VALID LICENSE, OTHER STATE OR COUNTRY
3	SUSP	SUSPENDED/REVOKED
4	EXP	EXPIRED
8	N-VAL	OTHER NON-VALID LICENSE
9	UNK	UNKNOWN IF DRIVER WAS LICENSED AT TIME OF CRASH

DRIVER RESIDENCE CODE TRANSLATION LIST

RES CODE	SHORT DESC	LONG DESCRIPTION
1	OR<25	OREGON RESIDENT WITHIN 25 MILE OF HOME
2	OR>25	OREGON RESIDENT 25 OR MORE MILES FROM HOME
3	OR-?	OREGON RESIDENT - UNKNOWN DISTANCE FROM HOME
4	N-RES	NON-RESIDENT
9	UNK	UNKNOWN IF OREGON RESIDENT

ERROR CODE TRANSLATION LIST

ERROR CODE	SHORT DESCRIPTION	FULL DESCRIPTION
000	NONE	NO ERROR
001	WIDE TRN	WIDE TURN
002	CUT CORN	CUT CORNER ON TURN
003	FAIL TRN	FAILED TO OBEY MANDATORY TRAFFIC TURN SIGNAL, SIGN OR LANE MARKINGS
004	L IN TRF	LEFT TURN IN FRONT OF ONCOMING TRAFFIC
005	L PROHIB	LEFT TURN WHERE PROHIBITED
006	FRM WRNG	TURNUED FROM WRONG LANE
007	TO WRONG	TURNUED INTO WRONG LANE
008	ILLEG U	U-TURNUED ILLEGALLY
009	IMP STOP	IMPROPERLY STOPPED IN TRAFFIC LANE
010	IMP SIG	IMPROPER SIGNAL OR FAILURE TO SIGNAL
011	IMP BACK	BACKING IMPROPERLY (NOT PARKING)
012	IMP PARK	IMPROPERLY PARKED
013	UNPARK	IMPROPER START LEAVING PARKED POSITION
014	IMP STRT	IMPROPER START FROM STOPPED POSITION
015	IMP LGHT	IMPROPER OR NO LIGHTS (VEHICLE IN TRAFFIC)
016	INATTENT	INATTENTION (FAILURE TO DIM LIGHTS PRIOR TO 4/1/97)
017	UNSF VEH	DRIVING UNSAFE VEHICLE (NO OTHER ERROR APPARENT)
018	OTH PARK	ENTERING/EXITING PARKED POSITION W/ INSUFFICIENT CLEARANCE; OTHER IMPROPER PARKING MANEUVER
019	DIS DRIV	DISREGARDED OTHER DRIVER'S SIGNAL
020	DIS SGNL	DISREGARDED TRAFFIC SIGNAL
021	RAN STOP	DISREGARDED STOP SIGN OR FLASHING RED
022	DIS SIGN	DISREGARDED WARNING SIGN, FLARES OR FLASHING AMBER
023	DIS OFCR	DISREGARDED POLICE OFFICER OR FLAGMAN
024	DIS EMER	DISREGARDED SIREN OR WARNING OF EMERGENCY VEHICLE
025	DIS RR	DISREGARDED RR SIGNAL, RR SIGN, OR RR FLAGMAN
026	REAR-END	FAILED TO AVOID STOPPED OR PARKED VEHICLE AHEAD OTHER THAN SCHOOL BUS
027	BIKE ROW	DID NOT HAVE RIGHT-OF-WAY OVER PEDALCYCLIST
028	NO ROW	DID NOT HAVE RIGHT-OF-WAY
029	PED ROW	FAILED TO YIELD RIGHT-OF-WAY TO PEDESTRIAN
030	PAS CURV	PASSING ON A CURVE
031	PAS WRNG	PASSING ON THE WRONG SIDE
032	PAS TANG	PASSING ON STRAIGHT ROAD UNDER UNSAFE CONDITIONS
033	PAS X-WK	PASSED VEHICLE STOPPED AT CROSSWALK FOR PEDESTRIAN
034	PAS INTR	PASSING AT INTERSECTION
035	PAS HILL	PASSING ON CREST OF HILL
036	N/PAS ZN	PASSING IN "NO PASSING" ZONE
037	PAS TRAF	PASSING IN FRONT OF ONCOMING TRAFFIC
038	CUT-IN	CUTTING IN (TWO LANES - TWO WAY ONLY)
039	WRNGSIDE	DRIVING ON WRONG SIDE OF THE ROAD (2-WAY UNDIVIDED ROADWAYS)

ERROR CODE TRANSLATION LIST

ERROR CODE	SHORT DESCRIPTION	FULL DESCRIPTION
040	THRU MED	DRIVING THROUGH SAFETY ZONE OR OVER ISLAND
041	F/ST BUS	FAILED TO STOP FOR SCHOOL BUS
042	F/SLO MV	FAILED TO DECREASE SPEED FOR SLOWER MOVING VEHICLE
043	TOO CLOSE	FOLLOWING TOO CLOSELY (MUST BE ON OFFICER'S REPORT)
044	STRDL LN	STRADDLING OR DRIVING ON WRONG LANES
045	IMP CHG	IMPROPER CHANGE OF TRAFFIC LANES
046	WRNG WAY	WRONG WAY ON ONE-WAY ROADWAY; WRONG SIDE DIVIDED ROAD
047	BASCRULE	DRIVING TOO FAST FOR CONDITIONS (NOT EXCEEDING POSTED SPEED)
048	OPN DOOR	OPENED DOOR INTO ADJACENT TRAFFIC LANE
049	IMPEDING	IMPEDING TRAFFIC
050	SPEED	DRIVING IN EXCESS OF POSTED SPEED
051	RECKLESS	RECKLESS DRIVING (PER PAR)
052	CARELESS	CARELESS DRIVING (PER PAR)
053	RACING	SPEED RACING (PER PAR)
054	X N/SGNL	CROSSING AT INTERSECTION, NO TRAFFIC SIGNAL PRESENT
055	X W/SGNL	CROSSING AT INTERSECTION, TRAFFIC SIGNAL PRESENT
056	DIAGONAL	CROSSING AT INTERSECTION - DIAGONALLY
057	BTWN INT	CROSSING BETWEEN INTERSECTIONS
059	W/TRAF-S	WALKING, RUNNING, RIDING, ETC., ON SHOULDER WITH TRAFFIC
060	A/TRAF-S	WALKING, RUNNING, RIDING, ETC., ON SHOULDER FACING TRAFFIC
061	W/TRAF-P	WALKING, RUNNING, RIDING, ETC., ON PAVEMENT WITH TRAFFIC
062	A/TRAF-P	WALKING, RUNNING, RIDING, ETC., ON PAVEMENT FACING TRAFFIC
063	PLAYINRD	PLAYING IN STREET OR ROAD
064	PUSH MV	PUSHING OR WORKING ON VEHICLE IN ROAD OR ON SHOULDER
065	WORK IN RD	WORKING IN ROADWAY OR ALONG SHOULDER
070	LAY ON RD	STANDING OR LYING IN ROADWAY
071	NM IMP USE	IMPROPER USE OF TRAFFIC LANE BY NON-MOTORIST
073	ELUDING	ELUDING / ATTEMPT TO ELUDE
079	F NEG CURV	FAILED TO NEGOTIATE A CURVE
080	FAIL LN	FAILED TO MAINTAIN LANE
081	OFF RD	RAN OFF ROAD
082	NO CLEAR	DRIVER MISJUDGED CLEARANCE
083	OVRSTEER	OVER-CORRECTING
084	NOT USED	CODE NOT IN USE
085	OVRLOAD	OVERLOADING OR IMPROPER LOADING OF VEHICLE WITH CARGO OR PASSENGERS
097	UNA DIS TC	UNABLE TO DETERMINE WHICH DRIVER DISREGARDED TRAFFIC CONTROL DEVICE

EVENT CODE TRANSLATION LIST

EVENT CODE	SHORT DESCRIPTION	LONG DESCRIPTION
001	FEL/JUMP	OCCUPANT FELL, JUMPED OR WAS EJECTED FROM MOVING VEHICLE
002	INTERFER	PASSENGER INTERFERED WITH DRIVER
003	BUG INTF	ANIMAL OR INSECT IN VEHICLE INTERFERED WITH DRIVER
004	INDRCT PED	PEDESTRIAN INDIRECTLY INVOLVED (NOT STRUCK)
005	SUB-PED	"SUB-PED": PEDESTRIAN INJURED SUBSEQUENT TO COLLISION, ETC.
006	INDRCT BIK	PEDALCYCLIST INDIRECTLY INVOLVED (NOT STRUCK)
007	HITCHIKR	HITCHHIKER (SOLICITING A RIDE)
008	PSNGR TOW	PASSENGER OR NON-MOTORIST BEING TOWED OR PUSHED ON CONVEYANCE
009	ON/OFF V	GETTING ON/OFF STOPPED/PARKED VEHICLE (OCCUPANTS ONLY; MUST HAVE PHYSICAL CONTACT W/ VEHICLE)
010	SUB OTRN	OVERTURNED AFTER FIRST HARMFUL EVENT
011	MV PUSHD	VEHICLE BEING PUSHED
012	MV TOWED	VEHICLE TOWED OR HAD BEEN TOWING ANOTHER VEHICLE
013	FORCED	VEHICLE FORCED BY IMPACT INTO ANOTHER VEHICLE, PEDALCYCLIST OR PEDESTRIAN
014	SET MOTN	VEHICLE SET IN MOTION BY NON-DRIVER (CHILD RELEASED BRAKES, ETC.)
015	RR ROW	AT OR ON RAILROAD RIGHT-OF-WAY (NOT LIGHT RAIL)
016	LT RL ROW	AT OR ON LIGHT-RAIL RIGHT-OF-WAY
017	RR HIT V	TRAIN STRUCK VEHICLE
018	V HIT RR	VEHICLE STRUCK TRAIN
019	HIT RR CAR	VEHICLE STRUCK RAILROAD CAR ON ROADWAY
020	JACKKNIFE	JACKKNIFE; TRAILER OR TOWED VEHICLE STRUCK TOWING VEHICLE
021	TRL OTRN	TRAILER OR TOWED VEHICLE OVERTURNED
022	CN BROKE	TRAILER CONNECTION BROKE
023	DETACH TRL	DETACHED TRAILING OBJECT STRUCK OTHER VEHICLE, NON-MOTORIST, OR OBJECT
024	V DOOR OPN	VEHICLE DOOR OPENED INTO ADJACENT TRAFFIC LANE
025	WHEELOFF	WHEEL CAME OFF
026	HOOD UP	HOOD FLEW UP
028	LOAD SHIFT	LOST LOAD, LOAD MOVED OR SHIFTED
029	TIREFAIL	TIRE FAILURE
030	PET	PET: CAT, DOG AND SIMILAR
031	LVSTOCK	STOCK: COW, CALF, BULL, STEER, SHEEP, ETC.
032	HORSE	HORSE, MULE, OR DONKEY
033	HRSE&RID	HORSE AND RIDER
034	GAME	WILD ANIMAL, GAME (INCLUDES BIRDS; NOT DEER OR ELK)
035	DEER ELK	DEER OR ELK, WAPITI
036	ANML VEH	ANIMAL-DRAWN VEHICLE
037	CULVERT	CULVERT, OPEN LOW OR HIGH MANHOLE
038	ATENUATN	IMPACT ATTENUATOR
039	PK METER	PARKING METER
040	CURB	CURB (ALSO NARROW SIDEWALKS ON BRIDGES)
041	JIGGLE	JIGGLE BAR OR TRAFFIC SNAKE FOR CHANNELIZATION
042	GDRL END	LEADING EDGE OF GUARDRAIL
043	GARDRAIL	GUARD RAIL (NOT METAL MEDIAN BARRIER)
044	BARRIER	MEDIAN BARRIER (RAISED OR METAL)
045	WALL	RETAINING WALL OR TUNNEL WALL
046	BR RAIL	BRIDGE RAILING OR PARAPET (ON BRIDGE OR APPROACH)
047	BR ABUTMNT	BRIDGE ABUTMENT (INCLUDED "APPROACH END" THRU 2013)
048	BR COLMN	BRIDGE PILLAR OR COLUMN
049	BR GIRDR	BRIDGE GIRDER (HORIZONTAL BRIDGE STRUCTURE OVERHEAD)
050	ISLAND	TRAFFIC RAISED ISLAND
051	GORE	GORE
052	POLE UNK	POLE - TYPE UNKNOWN
053	POLE UTL	POLE - POWER OR TELEPHONE
054	ST LIGHT	POLE - STREET LIGHT ONLY
055	TRF SGNL	POLE - TRAFFIC SIGNAL AND PED SIGNAL ONLY
056	SGN BRDG	POLE - SIGN BRIDGE
057	STOPSIGN	STOP OR YIELD SIGN

EVENT CODE TRANSLATION LIST

EVENT CODE	SHORT DESCRIPTION	LONG DESCRIPTION
058	OTH SIGN	OTHER SIGN, INCLUDING STREET SIGNS
059	HYDRANT	HYDRANT
060	MARKER	DELINEATOR OR MARKER (REFLECTOR POSTS)
061	MAILBOX	MAILBOX
062	TREE	TREE, STUMP OR SHRUBS
063	VEG OHED	TREE BRANCH OR OTHER VEGETATION OVERHEAD, ETC.
064	WIRE/CBL	WIRE OR CABLE ACROSS OR OVER THE ROAD
065	TEMP SGN	TEMPORARY SIGN OR BARRICADE IN ROAD, ETC.
066	PERM SGN	PERMANENT SIGN OR BARRICADE IN/OFF ROAD
067	SLIDE	SLIDES, FALLEN OR FALLING ROCKS
068	FRGN OBJ	FOREIGN OBSTRUCTION/DEBRIS IN ROAD (NOT GRAVEL)
069	EQP WORK	EQUIPMENT WORKING IN/OFF ROAD
070	OTH EQP	OTHER EQUIPMENT IN OR OFF ROAD (INCLUDES PARKED TRAILER, BOAT)
071	MAIN EQP	WRECKER, STREET SWEEPER, SNOW PLOW OR SANDING EQUIPMENT
072	OTHER WALL	ROCK, BRICK OR OTHER SOLID WALL
073	IRRGL PVMT	OTHER BUMP (NOT SPEED BUMP), POTHOLE OR PAVEMENT IRREGULARITY (PER PAR)
074	OVERHD OBJ	OTHER OVERHEAD OBJECT (HIGHWAY SIGN, SIGNAL HEAD, ETC.); NOT BRIDGE
075	CAVE IN	BRIDGE OR ROAD CAVE IN
076	HI WATER	HIGH WATER
077	SNO BANK	SNOW BANK
078	LO-HI EDGE	LOW OR HIGH SHOULDER AT PAVEMENT EDGE
079	DITCH	CUT SLOPE OR DITCH EMBANKMENT
080	OBJ FRM MV	STRUCK BY ROCK OR OTHER OBJECT SET IN MOTION BY OTHER VEHICLE (INCL. LOST LOADS)
081	FLY-OBJ	STRUCK BY ROCK OR OTHER MOVING OR FLYING OBJECT (NOT SET IN MOTION BY VEHICLE)
082	VEH HID	VEHICLE OBSCURED VIEW
083	VEG HID	VEGETATION OBSCURED VIEW
084	BLDG HID	VIEW OBSCURED BY FENCE, SIGN, PHONE BOOTH, ETC.
085	WIND GUST	WIND GUST
086	IMMERSED	VEHICLE IMMERSED IN BODY OF WATER
087	FIRE/EXP	FIRE OR EXPLOSION
088	FENC/BLD	FENCE OR BUILDING, ETC.
089	OTHR CRASH	CRASH RELATED TO ANOTHER SEPARATE CRASH
090	TO 1 SIDE	TWO-WAY TRAFFIC ON DIVIDED ROADWAY ALL ROUTED TO ONE SIDE
091	BUILDING	BUILDING OR OTHER STRUCTURE
092	PHANTOM	OTHER (PHANTOM) NON-CONTACT VEHICLE
093	CELL PHONE	CELL PHONE (ON PAR OR DRIVER IN USE)
094	VIOL GDL	TEENAGE DRIVER IN VIOLATION OF GRADUATED LICENSE PGM
095	GUY WIRE	GUY WIRE
096	BERM	BERM (EARTHEN OR GRAVEL MOUND)
097	GRAVEL	GRAVEL IN ROADWAY
098	ABR EDGE	ABRUPT EDGE
099	CELL WTNSD	CELL PHONE USE WITNESSED BY OTHER PARTICIPANT
100	UNK FIXD	FIXED OBJECT, UNKNOWN TYPE.
101	OTHER OBJ	NON-FIXED OBJECT, OTHER OR UNKNOWN TYPE
102	TEXTING	TEXTING
103	WZ WORKER	WORK ZONE WORKER
104	ON VEHICLE	PASSENGER RIDING ON VEHICLE EXTERIOR
105	PEDAL PSGR	PASSENGER RIDING ON PEDALCYCLE
106	MAN WHLCHR	PEDESTRIAN IN NON-MOTORIZED WHEELCHAIR
107	MTR WHLCHR	PEDESTRIAN IN MOTORIZED WHEELCHAIR
108	OFFICER	LAW ENFORCEMENT / POLICE OFFICER
109	SUB-BIKE	"SUB-BIKE": PEDALCYCLIST INJURED SUBSEQUENT TO COLLISION, ETC.
110	N-MTR	NON-MOTORIST STRUCK VEHICLE
111	S CAR VS V	STREET CAR/TROLLEY (ON RAILS OR OVERHEAD WIRE SYSTEM) STRUCK VEHICLE
112	V VS S CAR	VEHICLE STRUCK STREET CAR/TROLLEY (ON RAILS OR OVERHEAD WIRE SYSTEM)
113	S CAR ROW	AT OR ON STREET CAR OR TROLLEY RIGHT-OF-WAY

EVENT CODE TRANSLATION LIST

EVENT CODE	SHORT DESCRIPTION	LONG DESCRIPTION
114	RR EQUIP	VEHICLE STRUCK RAILROAD EQUIPMENT (NOT TRAIN) ON TRACKS
115	DSTRCT GPS	DISTRACTED BY NAVIGATION SYSTEM OR GPS DEVICE
116	DSTRCT OTH	DISTRACTED BY OTHER ELECTRONIC DEVICE
117	RR GATE	RAIL CROSSING DROP-ARM GATE
118	EXPNSN JNT	EXPANSION JOINT
119	JERSEY BAR	JERSEY BARRIER
120	WIRE BAR	WIRE OR CABLE MEDIAN BARRIER
121	FENCE	FENCE
123	OBJ IN VEH	LOOSE OBJECT IN VEHICLE STRUCK OCCUPANT
124	SLIPPERY	SLIDING OR SWERVING DUE TO WET, ICY, SLIPPERY OR LOOSE SURFACE (NOT GRAVEL)
125	SHLDR	SHOULDER GAVE WAY
126	BOULDER	ROCK(S), BOULDER (NOT GRAVEL; NOT ROCK SLIDE)
127	LAND SLIDE	ROCK SLIDE OR LAND SLIDE
128	CURVE INV	CURVE PRESENT AT CRASH LOCATION
129	HILL INV	VERTICAL GRADE / HILL PRESENT AT CRASH LOCATION
130	CURVE HID	VIEW OBSCURED BY CURVE
131	HILL HID	VIEW OBSCURED BY VERTICAL GRADE / HILL
132	WINDOW HID	VIEW OBSCURED BY VEHICLE WINDOW CONDITIONS
133	SPRAY HID	VIEW OBSCURED BY WATER SPRAY
134	TORRENTIAL	TORRENTIAL RAIN (EXCEPTIONALLY HEAVY RAIN)
135	RAIL OCC	INJURED OCCUPANT OF RAILWAY TRAIN, LIGHT RAIL, STREET CAR OR CABLE CAR

FUNCTIONAL CLASSIFICATION TRANSLATION LIST

FUNC CLASS	DESCRIPTION
01	RURAL PRINCIPAL ARTERIAL - INTERSTATE
02	RURAL PRINCIPAL ARTERIAL - OTHER
06	RURAL MINOR ARTERIAL
07	RURAL MAJOR COLLECTOR
08	RURAL MINOR COLLECTOR
09	RURAL LOCAL
11	URBAN PRINCIPAL ARTERIAL - INTERSTATE
12	URBAN PRINCIPAL ARTERIAL - OTHER FREEWAYS AND EXP
14	URBAN PRINCIPAL ARTERIAL - OTHER
16	URBAN MINOR ARTERIAL
17	URBAN MAJOR COLLECTOR
18	URBAN MINOR COLLECTOR
19	URBAN LOCAL
78	UNKNOWN RURAL SYSTEM
79	UNKNOWN RURAL NON-SYSTEM
98	UNKNOWN URBAN SYSTEM
99	UNKNOWN URBAN NON-SYSTEM

HIGHWAY COMPONENT TRANSLATION LIST

CODE	DESCRIPTION
0	MAINLINE STATE HIGHWAY
1	COUplet
3	FRONTAGE ROAD
6	CONNECTION
8	HIGHWAY - OTHER

INJURY SEVERITY CODE TRANSLATION LIST

CODE	SHORT DESC	LONG DESCRIPTION
1	KILL	FATAL INJURY (K)
2	INJA	SUSPECTED SERIOUS INJURY (A)
3	INJB	SUSPECTED MINOR INJURY (B)
4	INJC	POSSIBLE INJURY (C)
5	PRI	DIED PRIOR TO CRASH
7	NO<5	NO INJURY - 0 TO 4 YEARS OF AGE
9	NONE	NO APPARENT INJURY (O)

LIGHT CONDITION CODE TRANSLATION LIST

CODE	SHORT DESC	LONG DESCRIPTION
0	UNK	UNKNOWN
1	DAY	DAYLIGHT
2	DLIT	DARKNESS - WITH STREET LIGHTS
3	DARK	DARKNESS - NO STREET LIGHTS
4	DAWN	DAWN (TWILIGHT)
5	DUSK	DUSK (TWILIGHT)

MEDIAN TYPE CODE TRANSLATION LIST

CODE	SHORT DESC	LONG DESCRIPTION
0	NONE	NO MEDIAN
1	RSDMD	SOLID MEDIAN BARRIER
2	DIVMD	EARTH, GRASS OR PAVED MEDIAN

MILEAGE TYPE CODE TRANSLATION LIST

CODE	LONG DESCRIPTION
0	REGULAR MILEAGE
T	TEMPORARY
Y	SPUR
Z	OVERLAPPING

MOVEMENT TYPE CODE TRANSLATION LIST

CODE	SHORT DESC	LONG DESCRIPTION
0	UNK	UNKNOWN
1	STRGHT	STRAIGHT AHEAD
2	TURN-R	TURNING RIGHT
3	TURN-L	TURNING LEFT
4	U-TURN	MAKING A U-TURN
5	BACK	BACKING
6	STOP	STOPPED IN TRAFFIC
7	PRKD-P	PARKED - PROPERLY
8	PRKD-I	PARKED - IMPROPERLY
9	PARKNG	PARKING MANEUVER

NON-MOTORIST LOCATION CODE TRANSLATION LIST

CODE	LONG DESCRIPTION
00	AT INTERSECTION - NOT IN ROADWAY
01	AT INTERSECTION - INSIDE CROSSWALK
02	AT INTERSECTION - IN ROADWAY, OUTSIDE CROSSWALK
03	AT INTERSECTION - IN ROADWAY, XWALK AVAIL UNKNWN
04	NOT AT INTERSECTION - IN ROADWAY
05	NOT AT INTERSECTION - ON SHOULDER
06	NOT AT INTERSECTION - ON MEDIAN
07	NOT AT INTERSECTION - WITHIN TRAFFIC RIGHT-OF-WAY
08	NOT AT INTERSECTION - IN BIKE PATH OR PARKING LANE
09	NOT-AT INTERSECTION - ON SIDEWALK
10	OUTSIDE TRAFFICWAY BOUNDARIES
13	AT INTERSECTION - IN BIKE LANE
14	NOT AT INTERSECTION - IN BIKE LANE
15	NOT AT INTERSECTION - INSIDE MID-BLOCK CROSSWALK
16	NOT AT INTERSECTION - IN PARKING LANE
18	OTHER, NOT IN ROADWAY
99	UNKNOWN LOCATION

ROAD CHARACTER CODE TRANSLATION LIST

CODE	SHORT DESC	LONG DESCRIPTION
0	UNK	UNKNOWN
1	INTER	INTERSECTION
2	ALLEY	DRIVEWAY OR ALLEY
3	STRGHT	STRAIGHT ROADWAY
4	TRANS	TRANSITION
5	CURVE	CURVE (HORIZONTAL CURVE)
6	OPENAC	OPEN ACCESS OR TURNOUT
7	GRADE	GRADE (VERTICAL CURVE)
8	BRIDGE	BRIDGE STRUCTURE
9	TUNNEL	TUNNEL

PARTICIPANT TYPE CODE TRANSLATION LIST

CODE	SHORT DESC	LONG DESCRIPTION
0	OCC	UNKNOWN OCCUPANT TYPE
1	DRVR	DRIVER
2	PSNG	PASSENGER
3	PED	PEDESTRIAN
4	CONV	PEDESTRIAN USING A PEDESTRIAN CONVEYAL
5	PTOW	PEDESTRIAN TOWING OR TRAILERING AN OB
6	BIKE	PEDALCYCLIST
7	BTOW	PEDALCYCLIST TOWING OR TRAILERING AN (
8	PRKD	OCCUPANT OF A PARKED MOTOR VEHICLE
9	OTHR	OTHER TYPE OF NON-MOTORIST

TRAFFIC CONTROL DEVICE CODE TRANSLATION LIST

CODE	SHORT DESC	LONG DESCRIPTION
000	NONE	NO CONTROL
001	TRF SIGNAL	TRAFFIC SIGNALS
002	FLASHBCN-R	FLASHING BEACON - RED (STOP)
003	FLASHBCN-A	FLASHING BEACON - AMBER (SLOW)
004	STOP SIGN	STOP SIGN
005	SLOW SIGN	SLOW SIGN
006	REG-SIGN	REGULATORY SIGN
007	YIELD	YIELD SIGN
008	WARNING	WARNING SIGN
009	CURVE	CURVE SIGN
010	SCHL X-ING	SCHOOL CROSSING SIGN OR SPECIAL SIGNAL
011	OFGR/FLAG	POLICE OFFICER, FLAGMAN - SCHOOL PATROL
012	BRDG-GATE	BRIDGE GATE - BARRIER
013	TEMP-BARR	TEMPORARY BARRIER
014	NO-PASS-ZN	NO PASSING ZONE
015	ONE-WAY	ONE-WAY STREET
016	CHANNEL	CHANNELIZATION
017	MEDIAN BAR	MEDIAN BARRIER
018	PILOT CAR	PILOT CAR
019	SP PED SIG	SPECIAL PEDESTRIAN SIGNAL
020	X-BUCK	CROSSBUCK
021	THR-GN-SIG	THROUGH GREEN ARROW OR SIGNAL
022	L-GRN-SIG	LEFT TURN GREEN ARROW, LANE MARKINGS, OR SIGNAL
023	R-GRN-SIG	RIGHT TURN GREEN ARROW, LANE MARKINGS, OR SIGNAL
024	WIGWAG	WIGWAG OR FLASHING LIGHTS W/O DROP-ARM GATE
025	X-BUCK WRN	CROSSBUCK AND ADVANCE WARNING
026	WW W/ GATE	FLASHING LIGHTS WITH DROP-ARM GATES
027	OVRHD SGNL	SUPPLEMENTAL OVERHEAD SIGNAL (RR XING ONLY)
028	SP RR STOP	SPECIAL RR STOP SIGN
029	ILUM GRD X	ILLUMINATED GRADE CROSSING
037	RAMP METER	METERED RAMPS
038	RUMBLE STR	RUMBLE STRIP
040	AUTO. FLAG	AUTOMATED FLAGGER ASSISTANCE DEVICE
090	L-TURN REF	LEFT TURN REFUGE (WHEN REFUGE IS INVOLVED)
091	R-TURN ALL	RIGHT TURN AT ALL TIMES SIGN, ETC.
092	EMR SGN/FL	EMERGENCY SIGNS OR FLARES
093	ACCEL LANE	ACCELERATION OR DECELERATION LANES
094	R-TURN PRO	RIGHT TURN PROHIBITED ON RED AFTER STOPPING
095	BUS STPSGN	BUS STOP SIGN AND RED LIGHTS

VEHICLE TYPE CODE TRANSLATION LIST

CODE	SHORT DESC	LONG DESCRIPTION
00	PDO	NOT COLLECTED FOR PDO CRASHES
01	PSNGR CAR	PASSENGER CAR, PICKUP, LIGHT DELIVERY, ETC.
02	BOBTAIL	TRUCK TRACTOR WITH NO TRAILERS (BOBTAIL)
03	FARM TRCTR	FARM TRACTOR OR SELF-PROPELLED FARM EQUIPMENT
04	SEMI TOW	TRUCK TRACTOR WITH TRAILER/MOBILE HOME IN TOW
05	TRUCK	TRUCK WITH NON-DETACHABLE BED, PANEL, ETC.
06	MOPED	MOPED, MINIBIKE, SEATED MOTOR SCOOTER, MOTOR BIKE
07	SCHL BUS	SCHOOL BUS (INCLUDES VAN)
08	OTH BUS	OTHER BUS
09	MTRCYCLE	MOTORCYCLE, DIRT BIKE
10	OTHER	OTHER: FORKLIFT, BACKHOE, ETC.
11	MOTRHOME	MOTORHOME
12	TROLLEY	MOTORIZED STREET CAR/TROLLEY (NO RAILS/WIRES)
13	ATV	ATV
14	MTRSCTR	MOTORIZED SCOOTER (STANDING)
15	SNOWMOBILE	SNOWMOBILE
99	UNKNOWN	UNKNOWN VEHICLE TYPE

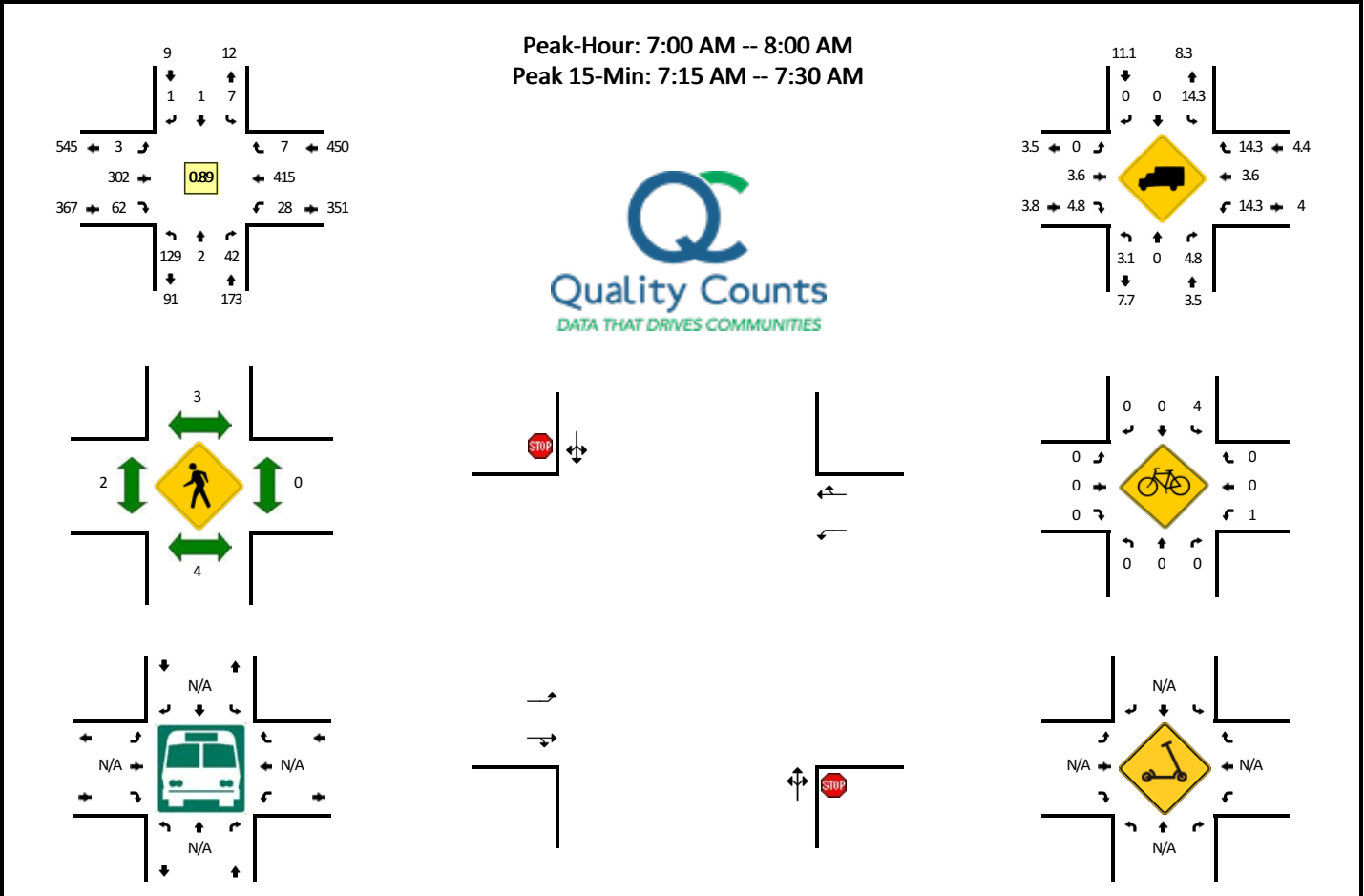
WEATHER CONDITION CODE TRANSLATION LIST

CODE	SHORT DESC	LONG DESCRIPTION
0	UNK	UNKNOWN
1	CLR	CLEAR
2	CLD	CLOUDY
3	RAIN	RAIN
4	SLT	SLEET
5	FOG	FOG
6	SNOW	SNOW
7	DUST	DUST
8	SMOK	SMOKE
9	ASH	ASH

Appendix B
Traffic Count Data

LOCATION: Stoltz Hill Rd -- Airport Rd
CITY/STATE: Linn, OR

QC JOB #: 15923201
DATE: Wed, Sep 14 2022

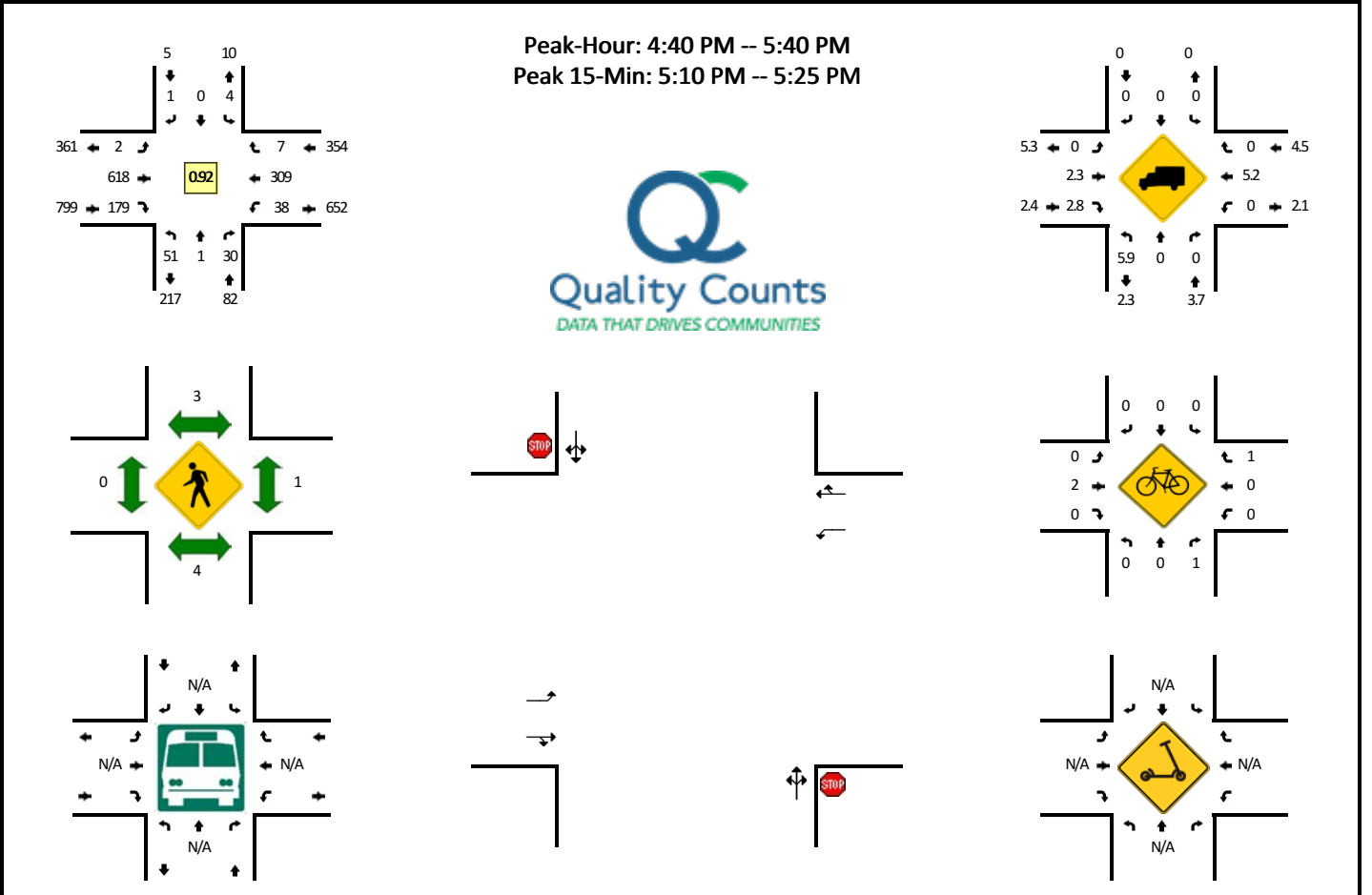


5-Min Count Period Beginning At	Stoltz Hill Rd (Northbound)				Stoltz Hill Rd (Southbound)				Airport Rd (Eastbound)				Airport Rd (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
7:00 AM	9	0	5	0	0	0	0	0	0	21	4	0	0	39	0	0	78	
7:05 AM	11	0	2	0	0	0	0	0	0	19	6	0	0	37	0	0	75	
7:10 AM	12	0	0	0	0	0	0	0	0	27	2	0	0	36	0	0	77	
7:15 AM	6	0	5	0	0	0	0	0	0	21	3	0	2	32	0	0	69	
7:20 AM	13	0	7	0	1	0	0	0	0	37	5	0	2	43	0	0	108	
7:25 AM	19	0	6	0	2	0	0	0	0	26	5	0	2	44	1	0	105	
7:30 AM	13	0	2	0	0	0	1	0	0	18	5	0	5	21	1	0	66	
7:35 AM	8	0	6	0	1	0	0	0	0	24	6	0	4	34	1	0	84	
7:40 AM	8	0	0	0	2	0	0	0	1	31	5	0	7	40	2	0	96	
7:45 AM	9	0	2	0	0	1	0	0	1	28	6	0	1	26	1	0	75	
7:50 AM	11	1	4	0	1	0	0	0	1	27	9	0	3	28	0	0	85	
7:55 AM	10	1	3	0	0	0	0	0	0	23	6	0	2	35	1	0	81	
8:00 AM	13	0	4	0	0	0	1	0	0	22	4	0	1	21	1	0	67	
8:05 AM	8	1	5	0	0	0	1	0	0	16	5	0	2	30	0	0	68	
8:10 AM	10	0	6	0	0	0	0	0	1	19	2	0	5	31	0	0	74	
8:15 AM	6	0	7	0	0	0	0	0	0	17	4	0	1	43	0	0	78	
8:20 AM	2	0	4	0	0	0	0	0	0	12	4	0	5	24	3	0	54	
8:25 AM	5	0	3	0	0	0	0	0	0	17	6	0	4	47	0	0	82	
8:30 AM	6	0	1	0	0	0	0	0	0	21	6	0	2	39	0	0	75	
8:35 AM	6	0	4	0	0	0	0	0	0	27	2	0	3	38	1	0	81	
8:40 AM	9	0	4	0	0	0	2	0	0	20	6	0	2	26	0	0	69	
8:45 AM	9	0	1	0	0	0	0	0	0	13	3	0	2	26	0	0	54	
8:50 AM	9	0	6	0	0	0	0	0	0	18	3	0	2	12	0	0	50	
8:55 AM	3	0	5	0	0	0	1	0	0	15	5	0	1	29	1	0	60	
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	152	0	72	0	12	0	0	0	0	336	52	0	24	476	4	0	1128	
Heavy Trucks	4	0	4		0	0	0		0	16	4		4	16	0		48	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		8	0	0			0	0		0	0	0		8	
Scoters																		

Comments:

LOCATION: Stoltz Hill Rd -- Airport Rd
CITY/STATE: Linn, OR

QC JOB #: 15923202
DATE: Wed, Sep 14 2022



5-Min Count Period Beginning At	Stoltz Hill Rd (Northbound)				Stoltz Hill Rd (Southbound)				Airport Rd (Eastbound)				Airport Rd (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
3:00 PM	2	0	3	0	1	0	0	0	1	37	10	0	6	15	0	0	75	
3:05 PM	4	0	1	0	0	0	0	0	0	19	6	0	1	22	2	0	55	
3:10 PM	4	0	5	0	0	0	0	0	0	31	8	0	3	39	1	0	91	
3:15 PM	12	0	3	0	0	0	0	0	0	33	11	0	3	36	1	0	99	
3:20 PM	6	0	2	0	1	0	1	0	0	33	9	0	2	27	0	0	81	
3:25 PM	4	0	4	0	0	0	0	0	0	27	6	0	5	32	0	0	78	
3:30 PM	7	0	3	0	0	0	0	0	0	49	15	0	3	29	0	0	106	
3:35 PM	8	0	2	0	1	0	2	0	0	49	17	0	5	22	0	0	106	
3:40 PM	5	0	5	0	0	0	0	0	1	43	15	0	5	31	0	0	105	
3:45 PM	2	0	0	0	1	0	0	0	0	50	12	0	2	35	1	0	103	
3:50 PM	4	0	4	0	0	0	0	0	0	45	14	0	2	31	0	0	100	
3:55 PM	4	0	3	0	2	0	0	0	0	59	19	0	2	24	0	0	113	
4:00 PM	6	0	5	0	2	0	0	0	0	43	12	0	2	23	0	0	93	
4:05 PM	1	0	1	0	0	0	1	0	0	48	19	0	4	33	1	0	108	
4:10 PM	1	0	4	0	0	1	0	0	0	40	17	0	7	36	1	0	107	
4:15 PM	4	0	5	0	0	0	0	0	1	48	13	0	6	33	0	0	110	
4:20 PM	2	0	1	0	0	0	1	0	0	38	16	0	3	24	0	0	85	
4:25 PM	1	0	2	0	0	1	0	0	0	47	18	0	0	19	2	0	90	
4:30 PM	3	0	2	0	0	0	0	0	0	40	15	0	3	36	0	0	99	
4:35 PM	6	0	1	0	0	0	0	0	0	32	16	0	4	20	0	0	79	
4:40 PM	3	0	1	0	2	0	0	0	0	53	14	0	6	26	0	0	105	
4:45 PM	4	0	3	0	0	0	0	0	0	52	18	0	4	26	0	0	107	
4:50 PM	5	0	1	0	0	0	0	0	1	60	13	0	1	27	1	0	109	
4:55 PM	7	0	4	0	0	0	1	0	0	52	17	0	2	18	0	0	101	
5:00 PM	1	0	1	0	0	0	0	0	0	45	11	0	4	29	0	0	91	
5:05 PM	3	0	3	0	0	0	0	0	0	31	18	0	2	35	2	0	94	
5:10 PM	6	0	4	0	0	0	0	0	0	72	12	0	0	21	2	0	117	
5:15 PM	1	1	5	0	0	0	0	0	0	62	15	0	4	27	0	0	115	
5:20 PM	7	0	4	0	1	0	0	0	0	55	17	0	3	19	0	0	106	
5:25 PM	6	0	1	0	1	0	0	0	1	51	18	0	5	30	0	0	113	
5:30 PM	4	0	2	0	0	0	0	0	0	47	15	0	2	21	0	0	91	
5:35 PM	4	0	1	0	0	0	0	0	0	38	11	0	5	30	2	0	91	
5:40 PM	2	0	1	0	2	0	0	0	0	57	8	0	4	20	0	0	94	
5:45 PM	4	0	8	0	0	1	0	0	0	39	13	0	3	29	0	0	97	
5:50 PM	3	0	6	0	0	0	0	0	0	48	15	0	1	20	1	0	94	
5:55 PM	8	0	3	0	0	0	1	0	0	55	15	0	4	12	0	0	98	

Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
All Vehicles	56	4	52	0	4	0	0	0	0	756	176	0	28	268	8	0	1352
Heavy Trucks	0	0	0		0	0	0		0	20	0		0	0	0		20
Buses																	
Pedestrians		0				0				0				4			4
Bicycles	0	0	4		0	0	0		0	0	0		0	0	0		4
Scoters																	

Comments:

Report generated on 9/22/2022 8:34 AM

SOURCE: Quality Counts, LLC (<http://www.qualitycounts.net>) 1-877-580-2212

Appendix C
Existing Conditions Worksheets

Intersection												
Int Delay, s/veh	8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗			↕			↕	
Traffic Vol, veh/h	3	302	62	28	415	4	129	1	42	13	4	11
Future Vol, veh/h	3	302	62	28	415	4	129	1	42	13	4	11
Conflicting Peds, #/hr	0	0	1	1	0	0	2	0	2	2	0	2
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	25	-	-	25	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	89	89	89	89	89	89	89	89	89	89	89	89
Heavy Vehicles, %	0	4	5	14	4	14	3	0	5	14	0	0
Mvmt Flow	3	339	70	31	466	4	145	1	47	15	4	12

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	470	0	0	410	0	0	921	913	377	936	946	470
Stage 1	-	-	-	-	-	-	381	381	-	530	530	-
Stage 2	-	-	-	-	-	-	540	532	-	406	416	-
Critical Hdwy	4.1	-	-	4.24	-	-	7.13	6.5	6.25	7.24	6.5	6.2
Critical Hdwy Stg 1	-	-	-	-	-	-	6.13	5.5	-	6.24	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.13	5.5	-	6.24	5.5	-
Follow-up Hdwy	2.2	-	-	2.326	-	-	3.527	4	3.345	3.626	4	3.3
Pot Cap-1 Maneuver	1102	-	-	1087	-	-	250	276	663	233	264	598
Stage 1	-	-	-	-	-	-	639	617	-	511	530	-
Stage 2	-	-	-	-	-	-	524	529	-	598	595	-
Platoon blocked, %		-	-	-	-	-						
Mov Cap-1 Maneuver	1102	-	-	1086	-	-	235	267	661	210	255	597
Mov Cap-2 Maneuver	-	-	-	-	-	-	235	267	-	210	255	-
Stage 1	-	-	-	-	-	-	636	615	-	509	515	-
Stage 2	-	-	-	-	-	-	493	514	-	552	593	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s	0.1			0.5			42.6			18.8		
HCM LOS							E			C		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	279	1102	-	-	1086	-	-	292
HCM Lane V/C Ratio	0.693	0.003	-	-	0.029	-	-	0.108
HCM Control Delay (s)	42.6	8.3	-	-	8.4	-	-	18.8
HCM Lane LOS	E	A	-	-	A	-	-	C
HCM 95th %tile Q(veh)	4.7	0	-	-	0.1	-	-	0.4

Intersection												
Int Delay, s/veh	3.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↶	↷		↶	↷			↕			↕	
Traffic Vol, veh/h	11	618	179	38	309	12	51	4	30	8	2	6
Future Vol, veh/h	11	618	179	38	309	12	51	4	30	8	2	6
Conflicting Peds, #/hr	1	0	0	0	0	1	8	0	3	3	0	8
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	25	-	-	25	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	2	3	0	5	0	6	0	0	0	0	0
Mvmt Flow	12	672	195	41	336	13	55	4	33	9	2	7

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	350	0	0	867	0	0	1231	1226	773	1241	1317	352
Stage 1	-	-	-	-	-	-	794	794	-	426	426	-
Stage 2	-	-	-	-	-	-	437	432	-	815	891	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.16	6.5	6.2	7.1	6.5	6.2
Critical Hdwy Stg 1	-	-	-	-	-	-	6.16	5.5	-	6.1	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.16	5.5	-	6.1	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.554	4	3.3	3.5	4	3.3
Pot Cap-1 Maneuver	1220	-	-	785	-	-	151	180	402	153	159	696
Stage 1	-	-	-	-	-	-	376	403	-	610	589	-
Stage 2	-	-	-	-	-	-	590	586	-	374	363	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1219	-	-	785	-	-	140	169	401	131	149	690
Mov Cap-2 Maneuver	-	-	-	-	-	-	140	169	-	131	149	-
Stage 1	-	-	-	-	-	-	372	399	-	603	558	-
Stage 2	-	-	-	-	-	-	548	555	-	336	359	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.1	1	42.8	25.6
HCM LOS			E	D

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	184	1219	-	-	785	-	-	192
HCM Lane V/C Ratio	0.502	0.01	-	-	0.053	-	-	0.091
HCM Control Delay (s)	42.8	8	-	-	9.8	-	-	25.6
HCM Lane LOS	E	A	-	-	A	-	-	D
HCM 95th %tile Q(veh)	2.5	0	-	-	0.2	-	-	0.3

Appendix D
2039 Existing Zoning Traffic Conditions
Worksheets

HCM Signalized Intersection Capacity Analysis Background 2039 AM Peak Hour - Mitigation
 101: Stoltz Hill Rd & Airport Rd 24995.012 Stoltz Hill Road



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↗		↖	↗	
Traffic Volume (vph)	3	356	73	33	487	4	151	1	50	13	4	11
Future Volume (vph)	3	356	73	33	487	4	151	1	50	13	4	11
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.5	4.5		4.5	4.5		4.5	4.5		4.5	4.5	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Frbp, ped/bikes	1.00	1.00		1.00	1.00		1.00	0.98		1.00	0.98	
Flpb, ped/bikes	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Frt	1.00	0.97		1.00	1.00		1.00	0.85		1.00	0.89	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00		0.95	1.00	
Satd. Flow (prot)	1805	1771		1583	1824		1749	1511		1581	1659	
Flt Permitted	0.34	1.00		0.41	1.00		0.75	1.00		0.72	1.00	
Satd. Flow (perm)	651	1771		686	1824		1376	1511		1198	1659	
Peak-hour factor, PHF	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Adj. Flow (vph)	3	400	82	37	547	4	170	1	56	15	4	12
RTOR Reduction (vph)	0	15	0	0	1	0	0	40	0	0	9	0
Lane Group Flow (vph)	3	467	0	37	550	0	170	17	0	15	7	0
Confl. Peds. (#/hr)			1	1			2		2	2		2
Heavy Vehicles (%)	0%	4%	5%	14%	4%	14%	3%	0%	5%	14%	0%	0%
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2			6		
Actuated Green, G (s)	14.7	14.7		14.7	14.7		9.6	9.6		9.6	9.6	
Effective Green, g (s)	14.7	14.7		14.7	14.7		9.6	9.6		9.6	9.6	
Actuated g/C Ratio	0.44	0.44		0.44	0.44		0.29	0.29		0.29	0.29	
Clearance Time (s)	4.5	4.5		4.5	4.5		4.5	4.5		4.5	4.5	
Vehicle Extension (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
Lane Grp Cap (vph)	287	781		302	805		396	435		345	478	
v/s Ratio Prot		0.26			c0.30			0.01			0.00	
v/s Ratio Perm	0.00			0.05			c0.12			0.01		
v/c Ratio	0.01	0.60		0.12	0.68		0.43	0.04		0.04	0.02	
Uniform Delay, d1	5.2	7.1		5.5	7.4		9.6	8.5		8.5	8.5	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Incremental Delay, d2	0.0	1.2		0.2	2.4		0.8	0.0		0.1	0.0	
Delay (s)	5.2	8.3		5.7	9.9		10.4	8.6		8.6	8.5	
Level of Service	A	A		A	A		B	A		A	A	
Approach Delay (s)		8.3			9.6			9.9			8.5	
Approach LOS		A			A			A			A	

Intersection Summary		
HCM 2000 Control Delay	9.1	HCM 2000 Level of Service
HCM 2000 Volume to Capacity ratio	0.58	A
Actuated Cycle Length (s)	33.3	Sum of lost time (s)
Intersection Capacity Utilization	50.0%	9.0
Analysis Period (min)	15	ICU Level of Service
		A

c Critical Lane Group

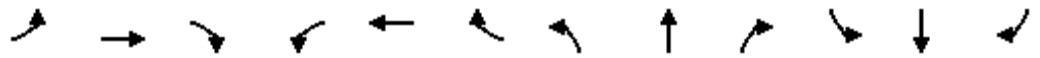
HCM 6th Signalized Intersection Summary
 101: Stoltz Hill Rd & Airport Rd

Background 2039 AM Peak Hour - Mitigation
 24995.012 Stoltz Hill Road



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↗		↖	↗	
Traffic Volume (veh/h)	3	356	73	33	487	4	151	1	50	13	4	11
Future Volume (veh/h)	3	356	73	33	487	4	151	1	50	13	4	11
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1841	1826	1693	1841	1693	1856	1900	1826	1693	1900	1900
Adj Flow Rate, veh/h	3	400	82	37	547	4	170	1	56	15	4	12
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Percent Heavy Veh, %	0	4	5	14	4	14	3	0	5	14	0	0
Cap, veh/h	437	636	130	456	783	6	577	6	356	515	94	282
Arrive On Green	0.43	0.43	0.43	0.43	0.43	0.43	0.23	0.23	0.23	0.23	0.23	0.23
Sat Flow, veh/h	870	1482	304	826	1825	13	1380	28	1579	1214	417	1251
Grp Volume(v), veh/h	3	0	482	37	0	551	170	0	57	15	0	16
Grp Sat Flow(s),veh/h/ln	870	0	1786	826	0	1838	1380	0	1607	1214	0	1668
Q Serve(g_s), s	0.1	0.0	5.5	1.0	0.0	6.4	2.9	0.0	0.7	0.3	0.0	0.2
Cycle Q Clear(g_c), s	6.4	0.0	5.5	6.5	0.0	6.4	3.1	0.0	0.7	1.0	0.0	0.2
Prop In Lane	1.00		0.17	1.00		0.01	1.00		0.98	1.00		0.75
Lane Grp Cap(c), veh/h	437	0	766	456	0	789	577	0	362	515	0	376
V/C Ratio(X)	0.01	0.00	0.63	0.08	0.00	0.70	0.29	0.00	0.16	0.03	0.00	0.04
Avail Cap(c_a), veh/h	665	0	1234	673	0	1270	1220	0	1111	1080	0	1153
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	8.7	0.0	5.8	8.3	0.0	6.1	9.1	0.0	8.1	8.5	0.0	7.9
Incr Delay (d2), s/veh	0.0	0.0	0.9	0.1	0.0	1.1	0.3	0.0	0.2	0.0	0.0	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.0	0.0	1.8	0.2	0.0	2.2	1.1	0.0	0.3	0.1	0.0	0.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	8.7	0.0	6.7	8.4	0.0	7.2	9.4	0.0	8.3	8.5	0.0	7.9
LnGrp LOS	A	A	A	A	A	A	A	A	A	A	A	A
Approach Vol, veh/h		485			588			227				31
Approach Delay, s/veh		6.7			7.3			9.1				8.2
Approach LOS		A			A			A				A
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		10.4		15.7		10.4		15.7				
Change Period (Y+Rc), s		4.5		4.5		4.5		4.5				
Max Green Setting (Gmax), s		18.0		18.0		18.0		18.0				
Max Q Clear Time (g_c+I1), s		5.1		8.4		3.0		8.5				
Green Ext Time (p_c), s		0.6		2.2		0.1		2.7				
Intersection Summary												
HCM 6th Ctrl Delay				7.4								
HCM 6th LOS				A								

HCM Signalized Intersection Capacity Analysis Background 2039 PM Peak Hour - Mitigation
 101: Stoltz Hill Rd & Airport Rd 24995.012 Stoltz Hill Road



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↗		↖	↗	
Traffic Volume (vph)	11	725	209	45	365	12	60	4	36	8	2	6
Future Volume (vph)	11	725	209	45	365	12	60	4	36	8	2	6
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.5	4.5		4.5	4.5		4.5	4.5		4.5	4.5	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Frbp, ped/bikes	1.00	1.00		1.00	1.00		1.00	0.98		1.00	0.98	
Flpb, ped/bikes	1.00	1.00		1.00	1.00		0.99	1.00		1.00	1.00	
Frt	1.00	0.97		1.00	1.00		1.00	0.86		1.00	0.88	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00		0.95	1.00	
Satd. Flow (prot)	1804	1788		1805	1802		1690	1608		1800	1643	
Flt Permitted	0.51	1.00		0.22	1.00		0.75	1.00		0.73	1.00	
Satd. Flow (perm)	977	1788		422	1802		1337	1608		1381	1643	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	12	788	227	49	397	13	65	4	39	9	2	7
RTOR Reduction (vph)	0	18	0	0	2	0	0	31	0	0	6	0
Lane Group Flow (vph)	12	997	0	49	408	0	65	12	0	9	3	0
Confl. Peds. (#/hr)	1					1	8		3	3		8
Confl. Bikes (#/hr)			2									
Heavy Vehicles (%)	0%	2%	3%	0%	5%	0%	6%	0%	0%	0%	0%	0%
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2			6		
Actuated Green, G (s)	18.0	18.0		18.0	18.0		7.1	7.1		7.1	7.1	
Effective Green, g (s)	18.0	18.0		18.0	18.0		7.1	7.1		7.1	7.1	
Actuated g/C Ratio	0.53	0.53		0.53	0.53		0.21	0.21		0.21	0.21	
Clearance Time (s)	4.5	4.5		4.5	4.5		4.5	4.5		4.5	4.5	
Vehicle Extension (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
Lane Grp Cap (vph)	515	943		222	951		278	334		287	342	
v/s Ratio Prot		c0.56			0.23			0.01			0.00	
v/s Ratio Perm	0.01			0.12			c0.05			0.01		
v/c Ratio	0.02	1.06		0.22	0.43		0.23	0.04		0.03	0.01	
Uniform Delay, d1	3.8	8.1		4.3	4.9		11.2	10.8		10.8	10.7	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Incremental Delay, d2	0.0	45.7		0.5	0.3		0.4	0.0		0.0	0.0	
Delay (s)	3.9	53.7		4.8	5.2		11.7	10.8		10.8	10.7	
Level of Service	A	D		A	A		B	B		B	B	
Approach Delay (s)		53.1			5.2			11.3			10.8	
Approach LOS		D			A			B			B	

Intersection Summary			
HCM 2000 Control Delay	36.2	HCM 2000 Level of Service	D
HCM 2000 Volume to Capacity ratio	0.82		
Actuated Cycle Length (s)	34.1	Sum of lost time (s)	9.0
Intersection Capacity Utilization	68.8%	ICU Level of Service	C
Analysis Period (min)	15		
c Critical Lane Group			

HCM 6th Signalized Intersection Summary
 101: Stoltz Hill Rd & Airport Rd

Background 2039 PM Peak Hour - Mitigation
 24995.012 Stoltz Hill Road























Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↗		↖	↗	
Traffic Volume (veh/h)	11	725	209	45	365	12	60	4	36	8	2	6
Future Volume (veh/h)	11	725	209	45	365	12	60	4	36	8	2	6
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		1.00	0.98		0.98	0.98		0.98
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1870	1856	1900	1826	1900	1811	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	12	788	227	49	397	13	65	4	39	9	2	7
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	0	2	3	0	5	0	6	0	0	0	0	0
Cap, veh/h	629	759	219	219	961	31	453	27	261	433	66	229
Arrive On Green	0.55	0.55	0.55	0.55	0.55	0.55	0.18	0.18	0.18	0.18	0.18	0.18
Sat Flow, veh/h	991	1388	400	564	1758	58	1332	149	1451	1358	364	1273
Grp Volume(v), veh/h	12	0	1015	49	0	410	65	0	43	9	0	9
Grp Sat Flow(s),veh/h/ln	991	0	1788	564	0	1815	1332	0	1600	1358	0	1637
Q Serve(g_s), s	0.2	0.0	18.0	0.0	0.0	4.4	1.4	0.0	0.7	0.2	0.0	0.1
Cycle Q Clear(g_c), s	4.6	0.0	18.0	18.0	0.0	4.4	1.5	0.0	0.7	0.9	0.0	0.1
Prop In Lane	1.00		0.22	1.00		0.03	1.00		0.91	1.00		0.78
Lane Grp Cap(c), veh/h	629	0	977	219	0	992	453	0	288	433	0	295
V/C Ratio(X)	0.02	0.00	1.04	0.22	0.00	0.41	0.14	0.00	0.15	0.02	0.00	0.03
Avail Cap(c_a), veh/h	629	0	977	219	0	992	941	0	874	930	0	895
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	5.7	0.0	7.5	16.5	0.0	4.4	11.8	0.0	11.4	11.8	0.0	11.1
Incr Delay (d2), s/veh	0.0	0.0	39.4	0.5	0.0	0.3	0.1	0.0	0.2	0.0	0.0	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.1	0.0	20.0	0.6	0.0	1.3	0.6	0.0	0.4	0.1	0.0	0.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	5.7	0.0	46.8	17.0	0.0	4.6	11.9	0.0	11.6	11.8	0.0	11.2
LnGrp LOS	A	A	F	B	A	A	B	A	B	B	A	B
Approach Vol, veh/h		1027			459			108			18	
Approach Delay, s/veh		46.4			6.0			11.8			11.5	
Approach LOS		D			A			B			B	
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		10.4		22.5		10.4		22.5				
Change Period (Y+Rc), s		4.5		4.5		4.5		4.5				
Max Green Setting (Gmax), s		18.0		18.0		18.0		18.0				
Max Q Clear Time (g_c+I1), s		3.5		20.0		2.9		20.0				
Green Ext Time (p_c), s		0.3		0.0		0.0		0.0				
Intersection Summary												
HCM 6th Ctrl Delay				32.2								
HCM 6th LOS				C								

Appendix E
2039 Proposed Zoning Traffic Conditions
Worksheets

HCM Signalized Intersection Capacity Analysis
 101: Stoltz Hill Rd & Airport Rd

Total 2039 PM Peak Hour - Mitigation
 24995.012 Stoltz Hill Road

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	34	704	209	45	368	22	59	15	36	42	12	6
Future Volume (vph)	34	704	209	45	368	22	59	15	36	42	12	6
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.5	4.5		4.5	4.5		4.5	4.5		4.5	4.5	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Frbp, ped/bikes	1.00	1.00		1.00	1.00		1.00	0.98		1.00	0.99	
Flpb, ped/bikes	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Frt	1.00	0.97		1.00	0.99		1.00	0.89		1.00	0.95	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00		0.95	1.00	
Satd. Flow (prot)	1805	1752		1583	1802		1749	1614		1581	1786	
Flt Permitted	0.50	1.00		0.22	1.00		0.74	1.00		0.72	1.00	
Satd. Flow (perm)	955	1752		370	1802		1371	1614		1200	1786	
Peak-hour factor, PHF	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Adj. Flow (vph)	37	765	227	49	400	24	64	16	39	46	13	7
RTOR Reduction (vph)	0	19	0	0	4	0	0	31	0	0	6	0
Lane Group Flow (vph)	37	973	0	49	420	0	64	24	0	46	14	0
Confl. Peds. (#/hr)			1	1			2		2	2		2
Heavy Vehicles (%)	0%	4%	5%	14%	4%	14%	3%	0%	5%	14%	0%	0%
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2			6		
Actuated Green, G (s)	18.0	18.0		18.0	18.0		7.0	7.0		7.0	7.0	
Effective Green, g (s)	18.0	18.0		18.0	18.0		7.0	7.0		7.0	7.0	
Actuated g/C Ratio	0.53	0.53		0.53	0.53		0.21	0.21		0.21	0.21	
Clearance Time (s)	4.5	4.5		4.5	4.5		4.5	4.5		4.5	4.5	
Vehicle Extension (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
Lane Grp Cap (vph)	505	927		195	954		282	332		247	367	
v/s Ratio Prot		c0.56			0.23			0.01			0.01	
v/s Ratio Perm	0.04			0.13			c0.05			0.04		
v/c Ratio	0.07	1.05		0.25	0.44		0.23	0.07		0.19	0.04	
Uniform Delay, d1	3.9	8.0		4.3	4.9		11.2	10.9		11.1	10.8	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Incremental Delay, d2	0.1	43.5		0.7	0.3		0.4	0.1		0.4	0.0	
Delay (s)	4.0	51.5		5.0	5.2		11.7	11.0		11.5	10.9	
Level of Service	A	D		A	A		B	B		B	B	
Approach Delay (s)		49.8			5.2			11.3			11.3	
Approach LOS		D			A			B			B	
Intersection Summary												
HCM 2000 Control Delay			33.1	HCM 2000 Level of Service				C				
HCM 2000 Volume to Capacity ratio			0.82									
Actuated Cycle Length (s)			34.0	Sum of lost time (s)				9.0				
Intersection Capacity Utilization			67.5%	ICU Level of Service				C				
Analysis Period (min)			15									

c Critical Lane Group

HCM 6th Signalized Intersection Summary
 101: Stoltz Hill Rd & Airport Rd

Total 2039 PM Peak Hour - Mitigation
 24995.012 Stoltz Hill Road



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	34	704	209	45	368	22	59	15	36	42	12	6
Future Volume (veh/h)	34	704	209	45	368	22	59	15	36	42	12	6
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	0.99		0.99	0.99		0.99
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1841	1826	1693	1841	1693	1856	1900	1826	1693	1900	1900
Adj Flow Rate, veh/h	37	765	227	49	400	24	64	16	39	46	13	7
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	0	4	5	14	4	14	3	0	5	14	0	0
Cap, veh/h	639	761	226	223	960	58	433	79	193	386	188	101
Arrive On Green	0.56	0.56	0.56	0.56	0.56	0.56	0.16	0.16	0.16	0.16	0.16	0.16
Sat Flow, veh/h	978	1363	404	514	1719	103	1373	488	1189	1214	1159	624
Grp Volume(v), veh/h	37	0	992	49	0	424	64	0	55	46	0	20
Grp Sat Flow(s),veh/h/ln	978	0	1767	514	0	1822	1373	0	1677	1214	0	1783
Q Serve(g_s), s	0.7	0.0	18.0	0.0	0.0	4.3	1.3	0.0	0.9	1.1	0.0	0.3
Cycle Q Clear(g_c), s	5.0	0.0	18.0	18.0	0.0	4.3	1.6	0.0	0.9	2.0	0.0	0.3
Prop In Lane	1.00		0.23	1.00		0.06	1.00		0.71	1.00		0.35
Lane Grp Cap(c), veh/h	639	0	987	223	0	1017	433	0	272	386	0	290
V/C Ratio(X)	0.06	0.00	1.01	0.22	0.00	0.42	0.15	0.00	0.20	0.12	0.00	0.07
Avail Cap(c_a), veh/h	639	0	987	223	0	1017	977	0	937	867	0	996
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	5.5	0.0	7.1	16.1	0.0	4.1	12.1	0.0	11.7	12.6	0.0	11.4
Incr Delay (d2), s/veh	0.0	0.0	29.9	0.5	0.0	0.3	0.2	0.0	0.4	0.1	0.0	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.2	0.0	16.0	0.6	0.0	1.2	0.6	0.0	0.5	0.4	0.0	0.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	5.6	0.0	37.0	16.6	0.0	4.4	12.3	0.0	12.1	12.7	0.0	11.5
LnGrp LOS	A	A	F	B	A	A	B	A	B	B	A	B
Approach Vol, veh/h		1029			473			119			66	
Approach Delay, s/veh		35.9			5.6			12.2			12.3	
Approach LOS		D			A			B			B	
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		9.7		22.5		9.7		22.5				
Change Period (Y+Rc), s		4.5		4.5		4.5		4.5				
Max Green Setting (Gmax), s		18.0		18.0		18.0		18.0				
Max Q Clear Time (g_c+I1), s		3.6		20.0		4.0		20.0				
Green Ext Time (p_c), s		0.3		0.0		0.1		0.0				
Intersection Summary												
HCM 6th Ctrl Delay			24.8									
HCM 6th LOS			C									

Intersection						
Int Delay, s/veh	4.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑	↑			↑
Traffic Vol, veh/h	0	0	0	12	0	12
Future Vol, veh/h	0	0	0	12	0	12
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	0	0	13	0	13

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	-	0	-
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	0	-	-
Stage 1	0	-	-
Stage 2	0	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	SB
HCM Control Delay, s	0	0	8.4
HCM LOS			A

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	1081
HCM Lane V/C Ratio	-	-	-	0.012
HCM Control Delay (s)	-	-	-	8.4
HCM Lane LOS	-	-	-	A
HCM 95th %tile Q(veh)	-	-	-	0

Intersection						
Int Delay, s/veh	7.8					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	0	44	44	0	0	0
Future Vol, veh/h	0	44	44	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	48	48	0	0	0

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	97	1	1	0	0
Stage 1	1	-	-	-	-
Stage 2	96	-	-	-	-
Critical Hdwy	6.4	6.2	4.1	-	-
Critical Hdwy Stg 1	5.4	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-
Pot Cap-1 Maneuver	907	1090	1635	-	-
Stage 1	1028	-	-	-	-
Stage 2	933	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	881	1090	1635	-	-
Mov Cap-2 Maneuver	881	-	-	-	-
Stage 1	998	-	-	-	-
Stage 2	933	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.5	7.3	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1635	-	1090	-	-
HCM Lane V/C Ratio	0.029	-	0.044	-	-
HCM Control Delay (s)	7.3	0	8.5	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0.1	-	0.1	-	-

HCM Signalized Intersection Capacity Analysis
 101: Stoltz Hill Rd & Airport Rd

Total 2023 AM Peak Hour - Mitigation
 24995.012 Stoltz Hill Road



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↗		↖	↗	
Traffic Volume (vph)	15	303	61	29	422	15	128	13	44	30	14	11
Future Volume (vph)	15	303	61	29	422	15	128	13	44	30	14	11
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	4.5	4.5		4.5	4.5		4.5	4.5		4.5	4.5	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Frbp, ped/bikes	1.00	1.00		1.00	1.00		1.00	0.98		1.00	0.99	
Flpb, ped/bikes	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Frt	1.00	0.97		1.00	0.99		1.00	0.89		1.00	0.94	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00		0.95	1.00	
Satd. Flow (prot)	1805	1772		1583	1811		1749	1593		1581	1761	
Flt Permitted	0.40	1.00		0.49	1.00		0.74	1.00		0.72	1.00	
Satd. Flow (perm)	769	1772		821	1811		1361	1593		1190	1761	
Peak-hour factor, PHF	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Adj. Flow (vph)	17	340	69	33	474	17	144	15	49	34	16	12
RTOR Reduction (vph)	0	15	0	0	3	0	0	35	0	0	9	0
Lane Group Flow (vph)	17	394	0	33	488	0	144	29	0	34	19	0
Confl. Peds. (#/hr)			1	1			2		2	2		2
Heavy Vehicles (%)	0%	4%	5%	14%	4%	14%	3%	0%	5%	14%	0%	0%
Turn Type	Perm	NA		Perm	NA		Perm	NA		Perm	NA	
Protected Phases		4			8			2			6	
Permitted Phases	4			8			2			6		
Actuated Green, G (s)	13.5	13.5		13.5	13.5		8.9	8.9		8.9	8.9	
Effective Green, g (s)	13.5	13.5		13.5	13.5		8.9	8.9		8.9	8.9	
Actuated g/C Ratio	0.43	0.43		0.43	0.43		0.28	0.28		0.28	0.28	
Clearance Time (s)	4.5	4.5		4.5	4.5		4.5	4.5		4.5	4.5	
Vehicle Extension (s)	3.0	3.0		3.0	3.0		3.0	3.0		3.0	3.0	
Lane Grp Cap (vph)	330	761		352	778		385	451		337	499	
v/s Ratio Prot		0.22			c0.27			0.02			0.01	
v/s Ratio Perm	0.02			0.04			c0.11			0.03		
v/c Ratio	0.05	0.52		0.09	0.63		0.37	0.06		0.10	0.04	
Uniform Delay, d1	5.2	6.6		5.3	7.0		9.0	8.2		8.3	8.2	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Incremental Delay, d2	0.1	0.6		0.1	1.6		0.6	0.1		0.1	0.0	
Delay (s)	5.3	7.2		5.4	8.6		9.6	8.3		8.4	8.2	
Level of Service	A	A		A	A		A	A		A	A	
Approach Delay (s)		7.1			8.4			9.2			8.3	
Approach LOS		A			A			A			A	

Intersection Summary		
HCM 2000 Control Delay	8.1	HCM 2000 Level of Service
HCM 2000 Volume to Capacity ratio	0.53	A
Actuated Cycle Length (s)	31.4	Sum of lost time (s)
Intersection Capacity Utilization	45.4%	9.0
Analysis Period (min)	15	ICU Level of Service
		A

c Critical Lane Group

HCM 6th Signalized Intersection Summary
 101: Stoltz Hill Rd & Airport Rd

Total 2023 AM Peak Hour - Mitigation
 24995.012 Stoltz Hill Road



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↗		↖	↗	
Traffic Volume (veh/h)	15	303	61	29	422	15	128	13	44	30	14	11
Future Volume (veh/h)	15	303	61	29	422	15	128	13	44	30	14	11
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1841	1826	1693	1841	1693	1856	1900	1826	1693	1900	1900
Adj Flow Rate, veh/h	17	340	69	33	474	17	144	15	49	34	16	12
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Percent Heavy Veh, %	0	4	5	14	4	14	3	0	5	14	0	0
Cap, veh/h	471	603	122	501	717	26	583	86	282	528	222	167
Arrive On Green	0.41	0.41	0.41	0.41	0.41	0.41	0.22	0.22	0.22	0.22	0.22	0.22
Sat Flow, veh/h	920	1485	301	883	1766	63	1365	390	1274	1206	1006	754
Grp Volume(v), veh/h	17	0	409	33	0	491	144	0	64	34	0	28
Grp Sat Flow(s),veh/h/ln	920	0	1786	883	0	1829	1365	0	1664	1206	0	1760
Q Serve(g_s), s	0.4	0.0	4.3	0.7	0.0	5.3	2.3	0.0	0.8	0.6	0.0	0.3
Cycle Q Clear(g_c), s	5.6	0.0	4.3	5.0	0.0	5.3	2.6	0.0	0.8	1.3	0.0	0.3
Prop In Lane	1.00		0.17	1.00		0.03	1.00		0.77	1.00		0.43
Lane Grp Cap(c), veh/h	471	0	725	501	0	742	583	0	368	528	0	389
V/C Ratio(X)	0.04	0.00	0.56	0.07	0.00	0.66	0.25	0.00	0.17	0.06	0.00	0.07
Avail Cap(c_a), veh/h	784	0	1333	802	0	1365	1300	0	1241	1161	0	1313
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	8.1	0.0	5.5	7.4	0.0	5.8	8.4	0.0	7.6	8.1	0.0	7.4
Incr Delay (d2), s/veh	0.0	0.0	0.7	0.1	0.0	1.0	0.2	0.0	0.2	0.1	0.0	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.1	0.0	1.3	0.2	0.0	1.7	0.8	0.0	0.3	0.2	0.0	0.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	8.1	0.0	6.2	7.5	0.0	6.8	8.7	0.0	7.8	8.2	0.0	7.5
LnGrp LOS	A	A	A	A	A	A	A	A	A	A	A	A
Approach Vol, veh/h		426			524			208				62
Approach Delay, s/veh		6.3			6.9			8.4				7.9
Approach LOS		A			A			A				A
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		9.8		14.3		9.8		14.3				
Change Period (Y+Rc), s		4.5		4.5		4.5		4.5				
Max Green Setting (Gmax), s		18.0		18.0		18.0		18.0				
Max Q Clear Time (g_c+I1), s		4.6		7.6		3.3		7.3				
Green Ext Time (p_c), s		0.6		1.9		0.1		2.5				
Intersection Summary												
HCM 6th Ctrl Delay				7.0								
HCM 6th LOS				A								

Intersection						
Int Delay, s/veh	4.9					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑	↑			↑
Traffic Vol, veh/h	0	2	0	13	0	21
Future Vol, veh/h	0	2	0	13	0	21
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	2	0	15	0	24

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	-	0	-
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	0	-	-
Stage 1	0	-	-
Stage 2	0	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	SB
HCM Control Delay, s	0	0	8.4
HCM LOS			A

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	1080
HCM Lane V/C Ratio	-	-	-	0.022
HCM Control Delay (s)	-	-	-	8.4
HCM Lane LOS	-	-	-	A
HCM 95th %tile Q(veh)	-	-	-	0.1

Intersection						
Int Delay, s/veh	7.7					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	0	27	35	0	0	0
Future Vol, veh/h	0	27	35	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	30	39	0	0	0

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	79	1	1	0	0
Stage 1	1	-	-	-	-
Stage 2	78	-	-	-	-
Critical Hdwy	6.4	6.2	4.1	-	-
Critical Hdwy Stg 1	5.4	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-
Pot Cap-1 Maneuver	929	1090	1635	-	-
Stage 1	1028	-	-	-	-
Stage 2	950	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	907	1090	1635	-	-
Mov Cap-2 Maneuver	907	-	-	-	-
Stage 1	1003	-	-	-	-
Stage 2	950	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.4	7.3	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1635	-	1090	-	-
HCM Lane V/C Ratio	0.024	-	0.028	-	-
HCM Control Delay (s)	7.3	0	8.4	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0.1	-	0.1	-	-

Appendix F
2024 Traffic Conditions Worksheets

Intersection												
Int Delay, s/veh	12.6											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗			↕			↕	
Traffic Vol, veh/h	15	303	61	29	422	15	128	13	44	30	14	11
Future Vol, veh/h	15	303	61	29	422	15	128	13	44	30	14	11
Conflicting Peds, #/hr	0	0	1	1	0	0	2	0	2	2	0	2
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	25	-	-	25	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	89	89	89	89	89	89	89	89	89	89	89	89
Heavy Vehicles, %	0	4	5	14	4	14	3	0	5	14	0	0
Mvmt Flow	17	340	69	33	474	17	144	15	49	34	16	12

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	491	0	0	410	0	0	975	967	378	992	993	485
Stage 1	-	-	-	-	-	-	410	410	-	549	549	-
Stage 2	-	-	-	-	-	-	565	557	-	443	444	-
Critical Hdwy	4.1	-	-	4.24	-	-	7.13	6.5	6.25	7.24	6.5	6.2
Critical Hdwy Stg 1	-	-	-	-	-	-	6.13	5.5	-	6.24	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.13	5.5	-	6.24	5.5	-
Follow-up Hdwy	2.2	-	-	2.326	-	-	3.527	4	3.345	3.626	4	3.3
Pot Cap-1 Maneuver	1083	-	-	1087	-	-	230	256	662	214	247	586
Stage 1	-	-	-	-	-	-	617	599	-	499	520	-
Stage 2	-	-	-	-	-	-	508	515	-	571	579	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1083	-	-	1086	-	-	206	244	660	182	236	585
Mov Cap-2 Maneuver	-	-	-	-	-	-	206	244	-	182	236	-
Stage 1	-	-	-	-	-	-	607	589	-	491	504	-
Stage 2	-	-	-	-	-	-	466	500	-	506	569	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s	0.3			0.5			64.1			26.8		
HCM LOS							F			D		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	250	1083	-	-	1086	-	-	226
HCM Lane V/C Ratio	0.831	0.016	-	-	0.03	-	-	0.273
HCM Control Delay (s)	64.1	8.4	-	-	8.4	-	-	26.8
HCM Lane LOS	F	A	-	-	A	-	-	D
HCM 95th %tile Q(veh)	6.6	0	-	-	0.1	-	-	1.1

Intersection						
Int Delay, s/veh	4.9					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑	↑			↑
Traffic Vol, veh/h	0	2	0	13	0	21
Future Vol, veh/h	0	2	0	13	0	21
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	2	0	15	0	24

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	-	0	- 0 -
Stage 1	-	-	- - -
Stage 2	-	-	- - -
Critical Hdwy	-	-	- - 6.2
Critical Hdwy Stg 1	-	-	- - -
Critical Hdwy Stg 2	-	-	- - -
Follow-up Hdwy	-	-	- - 3.3
Pot Cap-1 Maneuver	0	-	- 0 1080
Stage 1	0	-	- 0 -
Stage 2	0	-	- 0 -
Platoon blocked, %	-	-	- - -
Mov Cap-1 Maneuver	-	-	- - 1080
Mov Cap-2 Maneuver	-	-	- - -
Stage 1	-	-	- - -
Stage 2	-	-	- - -

Approach	EB	WB	SB
HCM Control Delay, s	0	0	8.4
HCM LOS			A

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	1080
HCM Lane V/C Ratio	-	-	-	0.022
HCM Control Delay (s)	-	-	-	8.4
HCM Lane LOS	-	-	-	A
HCM 95th %tile Q(veh)	-	-	-	0.1

Intersection						
Int Delay, s/veh	7.7					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	0	27	35	0	0	0
Future Vol, veh/h	0	27	35	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	89	89	89	89	89	89
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	30	39	0	0	0

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	79	1	1	0	0
Stage 1	1	-	-	-	-
Stage 2	78	-	-	-	-
Critical Hdwy	6.4	6.2	4.1	-	-
Critical Hdwy Stg 1	5.4	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-
Pot Cap-1 Maneuver	929	1090	1635	-	-
Stage 1	1028	-	-	-	-
Stage 2	950	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	907	1090	1635	-	-
Mov Cap-2 Maneuver	907	-	-	-	-
Stage 1	1003	-	-	-	-
Stage 2	950	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.4	7.3	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1635	-	1090	-	-
HCM Lane V/C Ratio	0.024	-	0.028	-	-
HCM Control Delay (s)	7.3	0	8.4	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0.1	-	0.1	-	-

Intersection												
Int Delay, s/veh	8.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗			↕			↕	
Traffic Vol, veh/h	34	611	183	40	321	22	51	15	32	42	12	6
Future Vol, veh/h	34	611	183	40	321	22	51	15	32	42	12	6
Conflicting Peds, #/hr	0	0	1	1	0	0	2	0	2	2	0	2
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	25	-	-	25	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	0	4	5	14	4	14	3	0	5	14	0	0
Mvmt Flow	37	664	199	43	349	24	55	16	35	46	13	7

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	373	0	0	864	0	0	1298	1298	767	1312	1385	363
Stage 1	-	-	-	-	-	-	839	839	-	447	447	-
Stage 2	-	-	-	-	-	-	459	459	-	865	938	-
Critical Hdwy	4.1	-	-	4.24	-	-	7.13	6.5	6.25	7.24	6.5	6.2
Critical Hdwy Stg 1	-	-	-	-	-	-	6.13	5.5	-	6.24	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.13	5.5	-	6.24	5.5	-
Follow-up Hdwy	2.2	-	-	2.326	-	-	3.527	4	3.345	3.626	4	3.3
Pot Cap-1 Maneuver	1197	-	-	730	-	-	138	163	397	128	145	686
Stage 1	-	-	-	-	-	-	359	384	-	568	577	-
Stage 2	-	-	-	-	-	-	580	570	-	332	346	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1197	-	-	729	-	-	118	148	396	100	132	685
Mov Cap-2 Maneuver	-	-	-	-	-	-	118	148	-	100	132	-
Stage 1	-	-	-	-	-	-	348	372	-	550	543	-
Stage 2	-	-	-	-	-	-	527	536	-	280	335	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s	0.3			1.1			63.6			71.1		
HCM LOS							F			F		

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	160	1197	-	-	729	-	-	115
HCM Lane V/C Ratio	0.666	0.031	-	-	0.06	-	-	0.567
HCM Control Delay (s)	63.6	8.1	-	-	10.3	-	-	71.1
HCM Lane LOS	F	A	-	-	B	-	-	F
HCM 95th %tile Q(veh)	3.8	0.1	-	-	0.2	-	-	2.7

Intersection						
Int Delay, s/veh	4.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑	↑			↑
Traffic Vol, veh/h	0	0	0	12	0	12
Future Vol, veh/h	0	0	0	12	0	12
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	0	0	13	0	13

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	-	0	- 0 - 7
Stage 1	-	-	- - -
Stage 2	-	-	- - -
Critical Hdwy	-	-	- - 6.2
Critical Hdwy Stg 1	-	-	- - -
Critical Hdwy Stg 2	-	-	- - -
Follow-up Hdwy	-	-	- - 3.3
Pot Cap-1 Maneuver	0	-	- 0 1081
Stage 1	0	-	- 0 -
Stage 2	0	-	- 0 -
Platoon blocked, %	-	-	- - -
Mov Cap-1 Maneuver	-	-	- - 1081
Mov Cap-2 Maneuver	-	-	- - -
Stage 1	-	-	- - -
Stage 2	-	-	- - -

Approach	EB	WB	SB
HCM Control Delay, s	0	0	8.4
HCM LOS			A

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	1081
HCM Lane V/C Ratio	-	-	-	0.012
HCM Control Delay (s)	-	-	-	8.4
HCM Lane LOS	-	-	-	A
HCM 95th %tile Q(veh)	-	-	-	0

Intersection						
Int Delay, s/veh	7.8					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	0	44	44	0	0	0
Future Vol, veh/h	0	44	44	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	0	48	48	0	0	0

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	97	1	1	0	0
Stage 1	1	-	-	-	-
Stage 2	96	-	-	-	-
Critical Hdwy	6.4	6.2	4.1	-	-
Critical Hdwy Stg 1	5.4	-	-	-	-
Critical Hdwy Stg 2	5.4	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-
Pot Cap-1 Maneuver	907	1090	1635	-	-
Stage 1	1028	-	-	-	-
Stage 2	933	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	881	1090	1635	-	-
Mov Cap-2 Maneuver	881	-	-	-	-
Stage 1	998	-	-	-	-
Stage 2	933	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	8.5	7.3	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1635	-	1090	-	-
HCM Lane V/C Ratio	0.029	-	0.044	-	-
HCM Control Delay (s)	7.3	0	8.5	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0.1	-	0.1	-	-

New Gas Station and C-Store

SITE DEVELOPMENT

911 Airport Road (at Stoltz Hill Road), Lebanon, OR 97355

**PRELIMINARY,
NOT FOR
CONSTRUCTION**

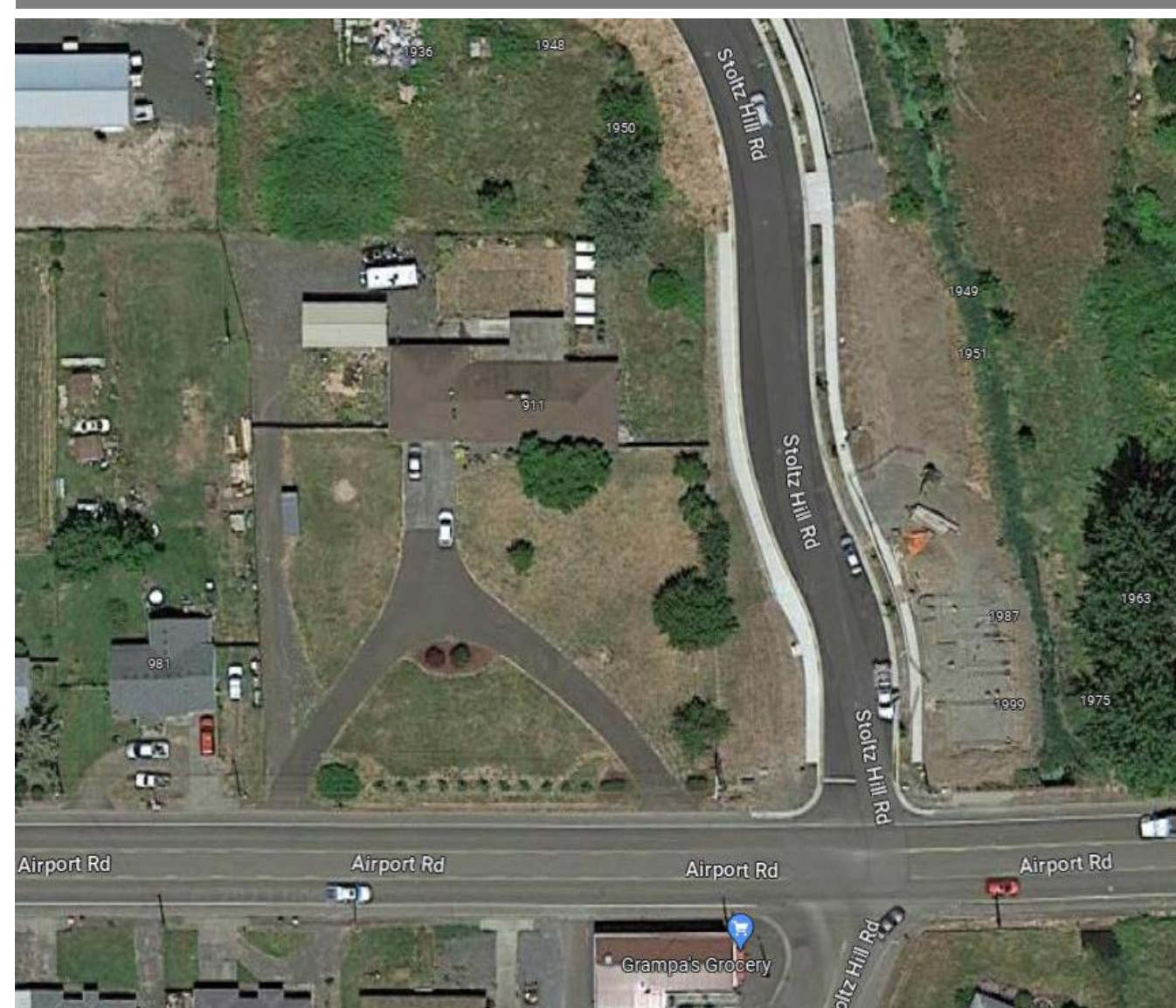
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PROJECT # 2023-087
DATE: 08/10/2023
REVISIONS

SITE VICINITY:



SITE:



DRAWINGS LIST:

Sheet Number	Sheet Name	Sheet Issue Date	Current Revision	Revision Description	Sheet Number	Sheet Name	Sheet Issue Date	Current Revision	Revision Description
GENERAL DRAWINGS									
G0.01	COVER SHEET	01/09/2020							
G3.01	PERSPECTIVE VIEWS	01/09/2020							
ARCHITECTURAL DRAWINGS									
A1.01	SITE PLAN	01/09/2020							
A1.21	LEVEL 01 - FLOOR PLAN	01/09/2020							
A1.23	LEVEL 01 - PUMP PAD PLAN	08/20/2020							
A1.24	LEVEL 02 - PUMP PAD PLAN	08/08/23							
A2.01	ELEVATIONS	01/09/2020							
A2.02	ELEVATIONS	01/09/2020							
A2.03	ELEVATIONS	08/20/2020							

PROJECT TEAM:

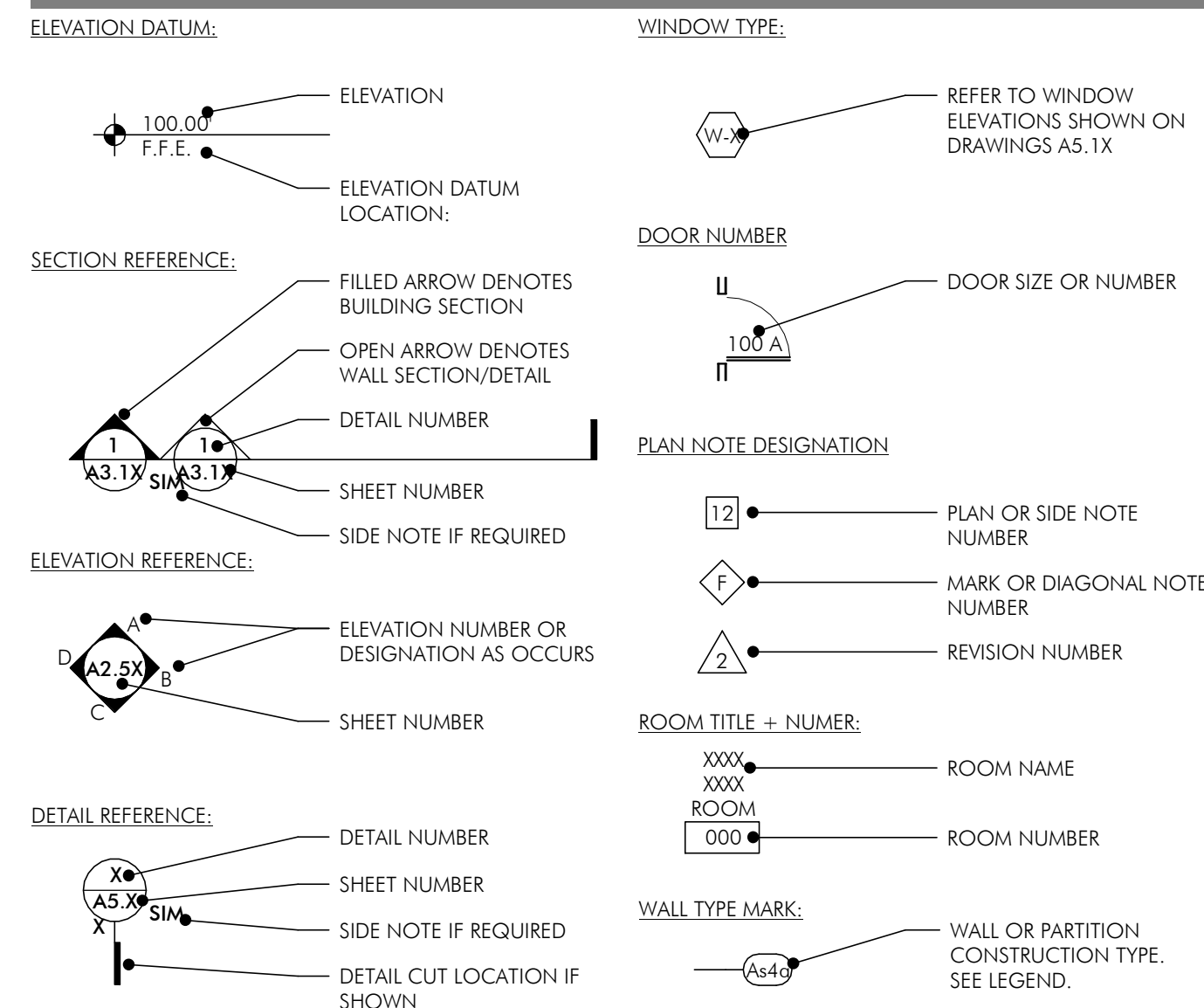
OWNER:
SONA ATHWAL
SWEET HOME, LLC
2515 Geary Street, SE Albany OR 97322
P: 503.682.2100 E: athwal1@yahoo.com

ARCHITECT:
STUDIO 3 ARCHITECTURE, Inc.
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P: 503.390.6500
Project Architect: Leonard Lodder, AIA, LEED AP
D: 971.239.0207
E: leonard@studio3architecture.com
W: www.studio3architecture.com

CIVIL ENGINEER:
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Brian Vandetta, PE, PLS,
P: 541.451.5125 E: brian@udelleng.com

CONTRACTOR:
AA APEX BUILDERS LLC
6732 Seven Mile Lane SE, Albany OR 97322
Randy (Sona) Athwal
C: 503.682.2100 E: athwal1@yahoo.com
CCB# 213143

SYMBOL LEGEND:



**New Gas Station and C-Store
Site Development**
911 Airport Road (at Stoltz Hill Road), Lebanon, OR 97355

SHEET:
G0.01

PRELIMINARY,
NOT FOR
CONSTRUCTION

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PROJECT # 2023-087
DATE: 08/10/2023
REVISIONS



1 3D View SITE VIEW FROM SOUTH EAST CORNER



2 3D View SITE ENTRY FROM STOLTZ HILL ROAD



3 3D View SITE ENTRY FROM AIRPORT ROAD

New Gas Station and C-Store Site Development

911 Airport Road (at Stoltz Hill Road), Lebanon, OR 97355

SHEET:

G3.01

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PROJECT # 2023-087 DATE: 08/10/2023 REVISIONS

New Gas Station and C-Store Site Development 911 Airport Road (at Stoltz Hill Road), Lebanon, OR 97355

SITE PLAN GENERAL NOTES:

- THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVES. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.
REFER TO CIVIL DRAWINGS FOR GRADING. SITE IS REQUIRED TO MEET THE LAWS OF FHA AND ADA. ACCESSIBLE ROUTES SHALL NOT EXCEED 5% (1 IN 20) OR CROSS SLOPES SHALL NOT EXCEED 2% (1 IN 50). ALL AT GRADE SIDEWALKS ARE ACCESSIBLE ROUTES.
JOINTS IN CONCRETE WALKS NOTED AS E.J. ARE TO BE CONSTRUCTED AS EXPANSION JOINTS. ALL OTHER JOINTS SHOWN, TO BE TOOLED CONTROL JOINTS, SEE CIVIL.
SEE LANDSCAPE DRAWINGS FOR LANDSCAPE AND IRRIGATION ELEMENTS.
SEE ELECTRICAL DRAWINGS FOR SITE LIGHTING.

SITE DEVELOPMENT CODE REVIEW:

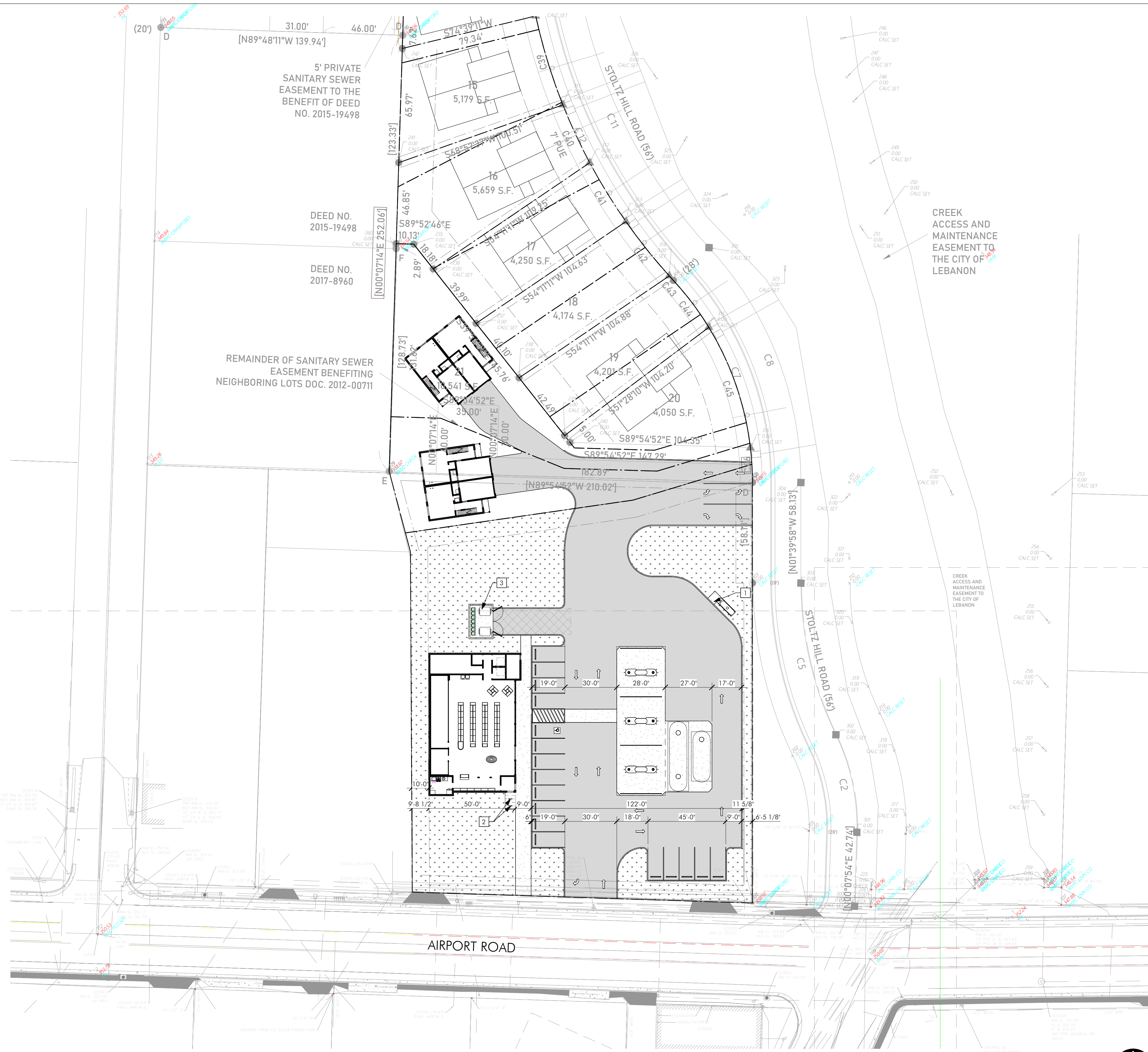
- SITE AREA: 44,816.20 sf = 1.3ac
ZONING: Z-2M Residential Mixed Density (Re-Zone) Z-NCM Neighborhood Commercial Zone
BUILDING AREAS: BLDG 1 LEVEL 01: 4,074 sf
PARKING: COMMERCIAL USES: 4/1000sf REQ'D = 16,296 Req'd. 17 Regular Spaces Provided 6 Spaces at Re-Fueling Pumps Total 23 Spaces Provided.
BIKE PARKING: 5% of Req'd Vehicle Spaces, or Min 4 Spaces. THEREFORE PROVIDE 4 BIKE PARKING SPACES.
LOADING SPACES: Not Required.

Table with 4 columns: DESCRIPTION, AREA sf, PERCENT, REMARKS. Rows include BUILDINGS (4,074.00, 9.09%), LANDSCAPING (15,381.73, 34.32%), ASPHALT PAVING (19,419.17, 43.33%), ACCESSORY STRUCTURES (280.00, 0.62%), CONCRETE SIDEWALKS (1,743.57, 3.89%), CONCRETE CURBS (369.65, 0.82%), CONCRETE PAVING AND MISC (3,548.00, 7.92%), CONCRETE PADS, MISCELLANEOUS (0.08, 0.00%), and a total of 44,816.20 sf at 100.00%.

Table with 4 columns: COVER DESCRIPTION, COVER AREA sf, PERCENT, COVER REMARKS. Rows include RE-FUELING CANOPY (2,523.00, 90.27%) and ENTRANCE CANOPIES (272.00, 9.73%), totaling 2,795.00 sf at 100.00%.

SITE PLAN NOTES:

- 1 PROPANE TANK RE-FILL STATION
2 PROVIDE HOOPS FOR 4 BICYCLE PARKING SPACES.
3 TRASH ENCLOSURE PER DETAILS



1 DEVELOPMENT SITE PLAN 1" = 30'-0"

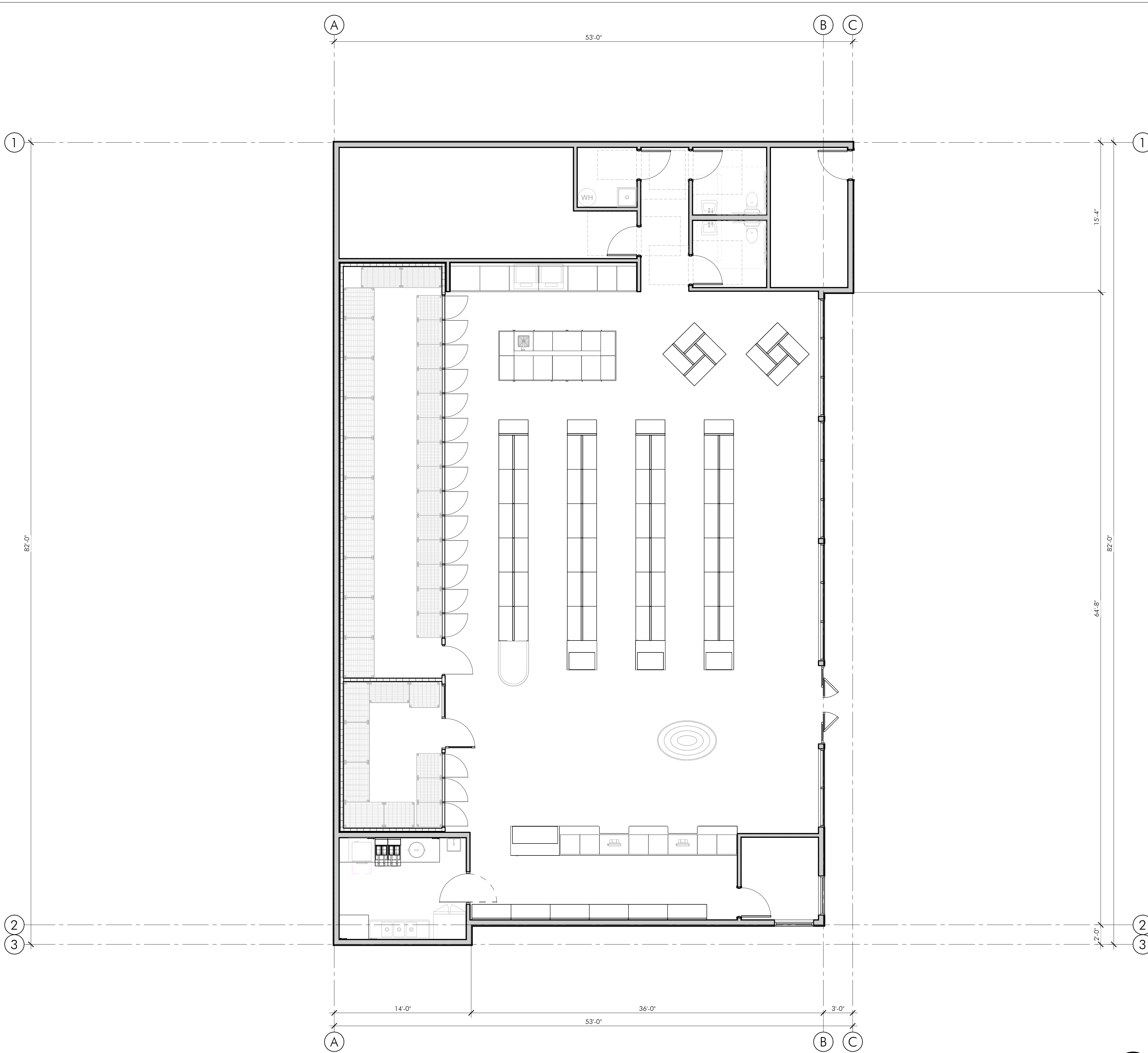
GENERAL PLAN NOTES:

- GENERAL NOTES APPLY TO ALL DRAWINGS.
- DRAWINGS ARE DIAGRAMMATIC ONLY AND SHOULD NOT BE SCALED. NOTIFY ARCHITECT OF ANY DISCREPANCIES IMMEDIATELY UPON DISCOVERY. OBTAIN CLARIFICATION OF DIMENSIONS OR DISCREPANCIES PRIOR TO PROCEEDING WITH AREA OF REQUIRED WORK.
- DIMENSIONS ARE TO FACE OF FRAMING. DIMENSIONS STATED AS CLEAR ARE TO FACE OF FINISH.
- SEE WALL SECTIONS FOR DESCRIPTION OF EXTERIOR WALL MATERIALS.
- ALL INTERIOR PARTITIONS TO RECEIVE GLASS FIBER INSULATION, FULL HEIGHT.
- COORDINATE LOCATION OF RECESSED OR SEMI-RECESSED ITEMS TO AVOID BACK TO BACK INSTALLATION AND TO REDUCE NOISE TRANSFER THROUGH PARTITIONS.
- INSTALL WALL BACKING FOR ALL WALL MOUNTED ITEMS, INCLUDING BUT NOT LIMITED TO THE FOLLOWING: DOOR STOPS, FIXTURES, WALL CABINETS, SHELVING, COUNTERS, TOILET ACCESSORIES, SECURITY EQUIPMENT, TACK BOARDS AND MARKER BOARDS, HAND RAILS AND WINDOW COVERING TRACKS.
- SEPARATE AREAS IN WHICH WORK IS BEING CONDUCTED FROM OTHER AREAS THAT ARE STILL OCCUPIED.
 - PROVIDE, ERECT, AND MAINTAIN TEMPORARY DUSTPROOF PARTITIONS OF SUITABLE CONSTRUCTION IN LOCATIONS INDICATED ON DRAWINGS OR AS DIRECTED.
- PROTECT EXISTING WORK TO REMAIN.
 - PREVENT MOVEMENT OF STRUCTURE; PROVIDE SHORING AND BRACING IF NECESSARY.
 - PERFORM CUTTING TO ACCOMPLISH REMOVALS NEATLY AND AS SPECIFIED FOR CUTTING NEW WORK.
 - REPAIR ADJACENT CONSTRUCTION AND FINISHES DAMAGED DURING REMOVAL WORK.
 - PATCH AS SPECIFIED FOR PATCHING NEW WORK.
- REMOVE DEBRIS, JUNK, AND TRASH FROM SITE.
- REMOVE FROM SITE ALL MATERIALS NOT TO BE REUSED ON SITE; DO NOT BURN OR BURY.
- LEAVE SITE IN CLEAN CONDITION, READY FOR SUBSEQUENT WORK.
- CLEAN UP SPILLAGE AND WIND-BLOWN DEBRIS FROM PUBLIC AND PRIVATE LANDS.
- WORK SHOWN ON THESE DRAWINGS IS TO BE SUPPLIED, FURNISHED, CONSTRUCTED, INSTALLED ALL AS PER THE GENERAL CONDITIONS AND THE SPECIFICATIONS: EXCEPTIONS AS DESCRIBED BY THE FOLLOWING ABBREVIATIONS:
 - CFCI CONTRACTOR FURNISHED - CONTRACTOR INSTALLED.
 - OFCI OWNER FURNISHED - CONTRACTOR INSTALLED.
 - OFOI OWNER FURNISHED - OWNER INSTALLED.
 - NIC OR N.I.C. NOT IN CONTRACT OR NOT A PART OF THIS CONTRACT.

PLAN LEGEND:

FLOOR PLAN NOTES:

- 1 SAMPLE PLAN NOTE



1 LEVEL 01 - GROUND FLOOR PLAN

0 2 4 6 8 10 16 3/16" = 1'-0"



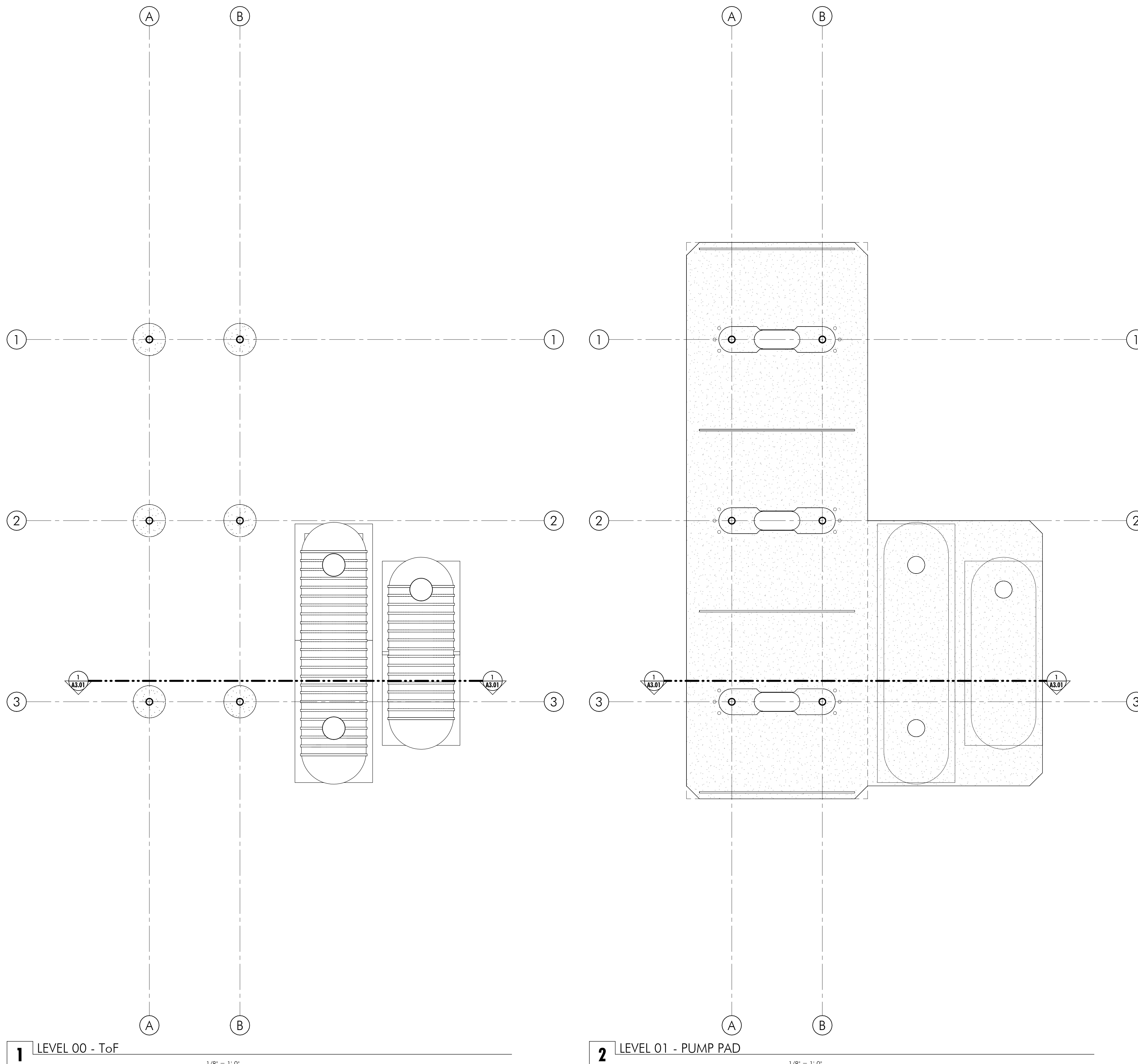
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4. SEE WALL SECTIONS FOR DESCRIPTION OF EXTERIOR WALL MATERIALS.
5. ALL INTERIOR PARTITIONS TO RECEIVE GLASS FIBER INSULATION, FULL HEIGHT.
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8. SEPARATE AREAS IN WHICH WORK IS BEING CONDUCTED FROM OTHER AREAS THAT ARE STILL OCCUPIED.
 - A. PROVIDE, ERECT, AND MAINTAIN TEMPORARY DUSTPROOF PARTITIONS OF SUITABLE CONSTRUCTION IN LOCATIONS INDICATED ON DRAWINGS OR AS DIRECTED.
9. PROTECT EXISTING WORK TO REMAIN.
 - A. PREVENT MOVEMENT OF STRUCTURE; PROVIDE SHORING AND BRACING IF NECESSARY.
 - B. PERFORM CUTTING TO ACCOMPLISH REMOVALS NEATLY AND AS SPECIFIED FOR CUTTING NEW WORK.
 - C. REPAIR ADJACENT CONSTRUCTION AND FINISHES DAMAGED DURING REMOVAL WORK.
 - D. PATCH AS SPECIFIED FOR PATCHING NEW WORK.
10. REMOVE DEBRIS, JUNK, AND TRASH FROM SITE.
11. REMOVE FROM SITE ALL MATERIALS NOT TO BE REUSED ON SITE; DO NOT BURN OR BURY.
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13. CLEAN UP SPILLAGE AND WIND-BLOWN DEBRIS FROM PUBLIC AND PRIVATE LANDS.
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PLAN LEGEND:

FLOOR PLAN NOTES:

1 SAMPLE PLAN NOTE



1 LEVEL 00 - ToF

1/8" = 1'-0"

2 LEVEL 01 - PUMP PAD

1/8" = 1'-0"

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PROJECT # 2023-087
DATE: 08/07/2023
REVISIONS

**Gas Station and C-Store
3-Pump Canopy**
911 Airport Road (at Stoltz Hill Road), Lebanon, OR 97355

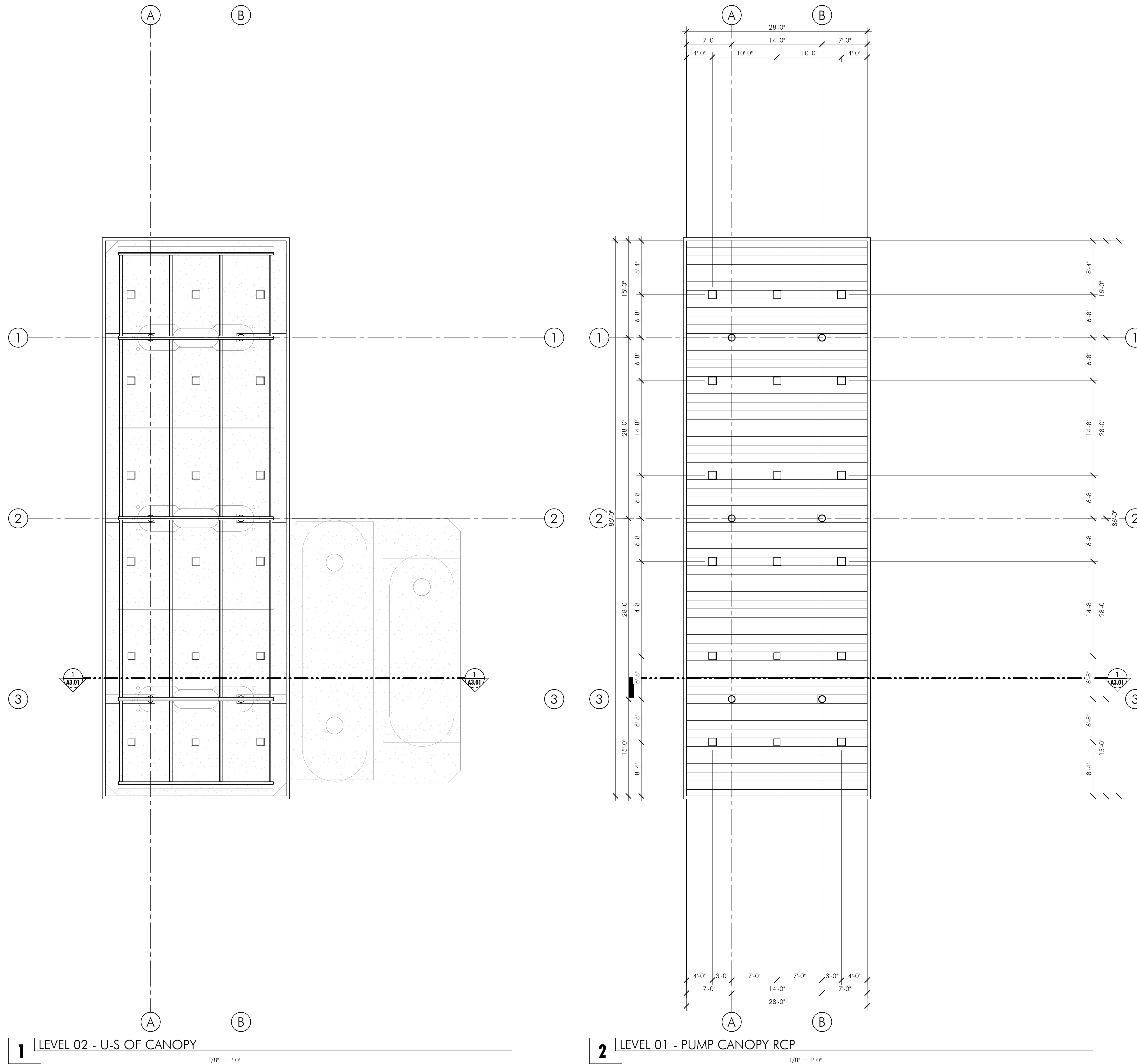
SHEET:
A1.24

GENERAL PLAN NOTES:

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FLOOR PLAN NOTES:

1 SAMPLE PLAN NOTE



1 LEVEL 02 - U-S OF CANOPY
1/8" = 1'-0"

2 LEVEL 01 - PUMP CANOPY RCP
1/8" = 1'-0"

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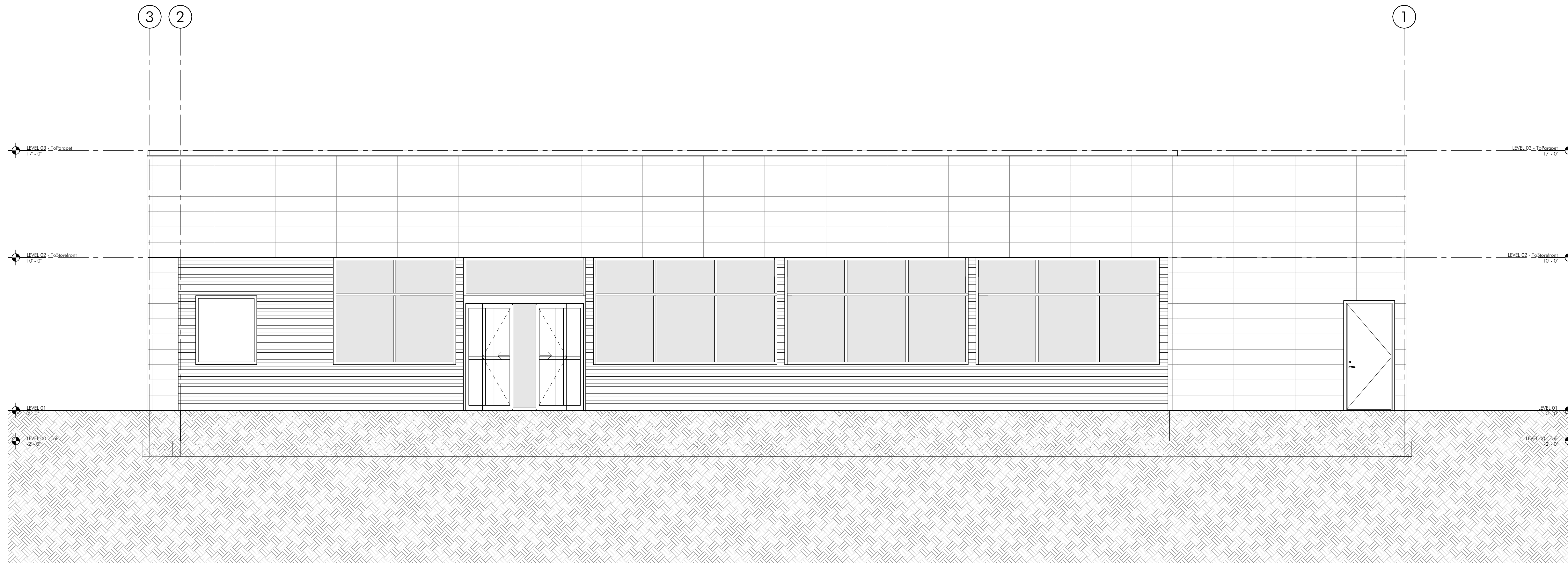
PROJECT # 2023-087
DATE: 08/07/2023
REVISIONS

**New Gas Station and C-Store
Convenience Store**
911 Airport Road (at Stoltz Hill Road), Lebanon, OR 97355

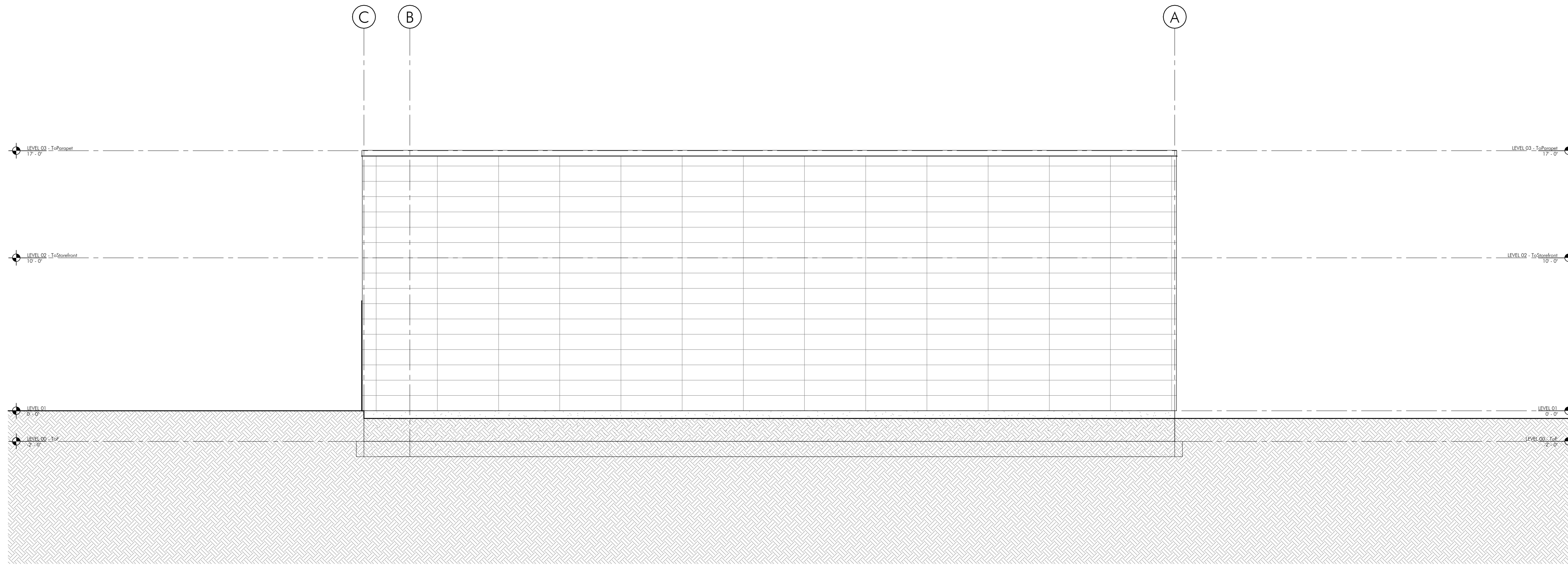
SHEET:
A2.01

ELEVATION NOTES:

1 SAMPLE ELEVATION NOTE



1 EAST ELEVATION
0' 1' 2' 4' 8' 12' 1/4" = 1'-0"



2 NORTH ELEVATION
0' 1' 2' 4' 8' 12' 1/4" = 1'-0"

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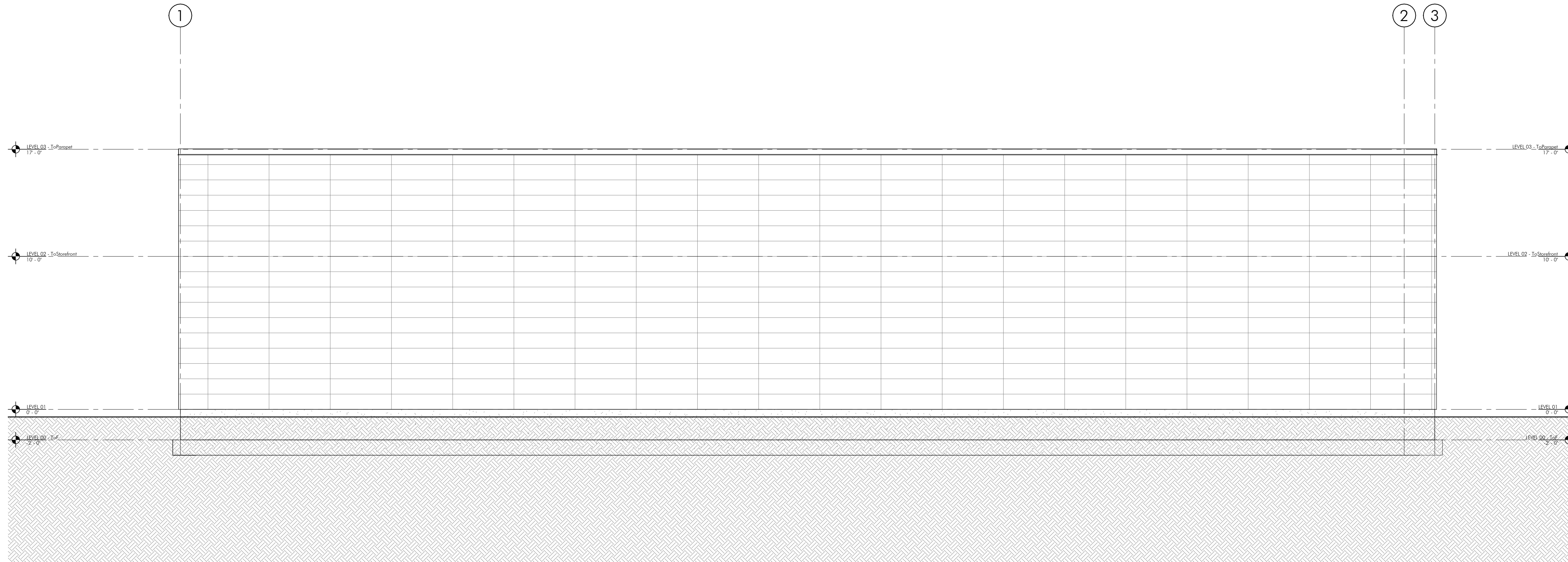
PROJECT # 2023-087
DATE: 08/07/2023
REVISIONS

**New Gas Station and C-Store
Convenience Store**
911 Airport Road (at Stoltz Hill Road), Lebanon, OR 97355

SHEET:
A2.02

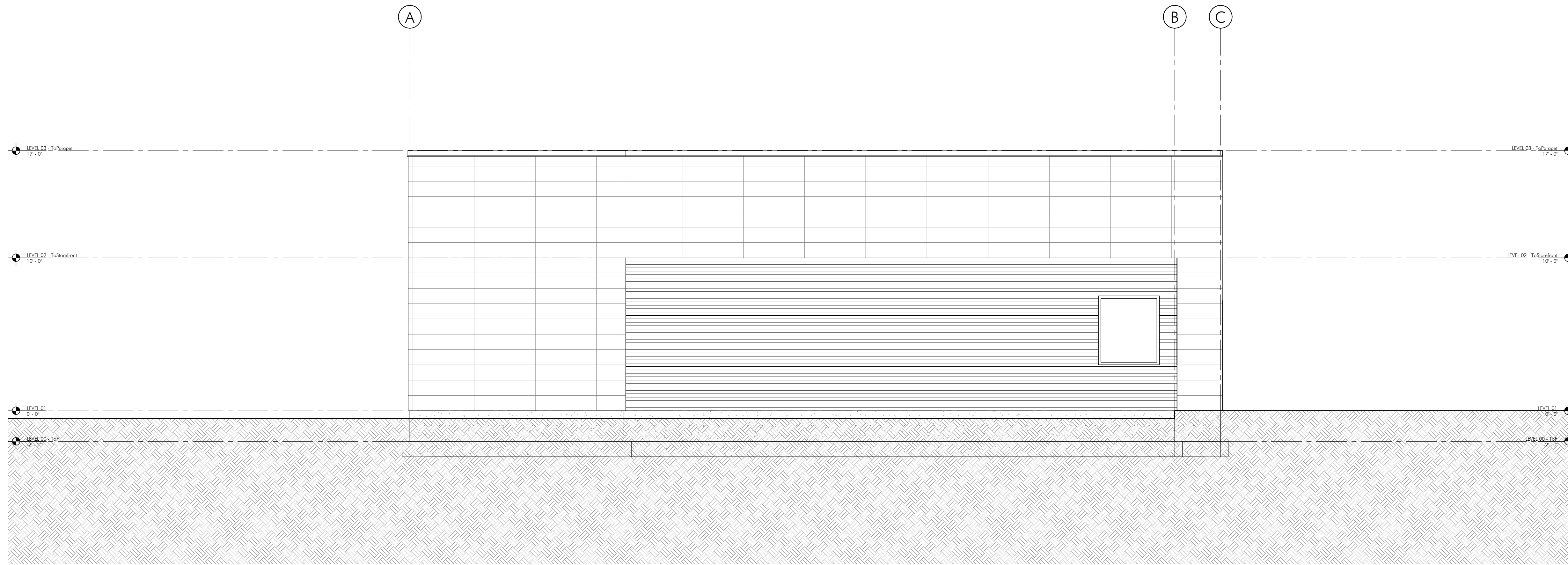
ELEVATION NOTES:

1 SAMPLE ELEVATION NOTE



1 WEST ELEVATION

1/4" = 1'-0"



2 SOUTH ELEVATION

1/4" = 1'-0"

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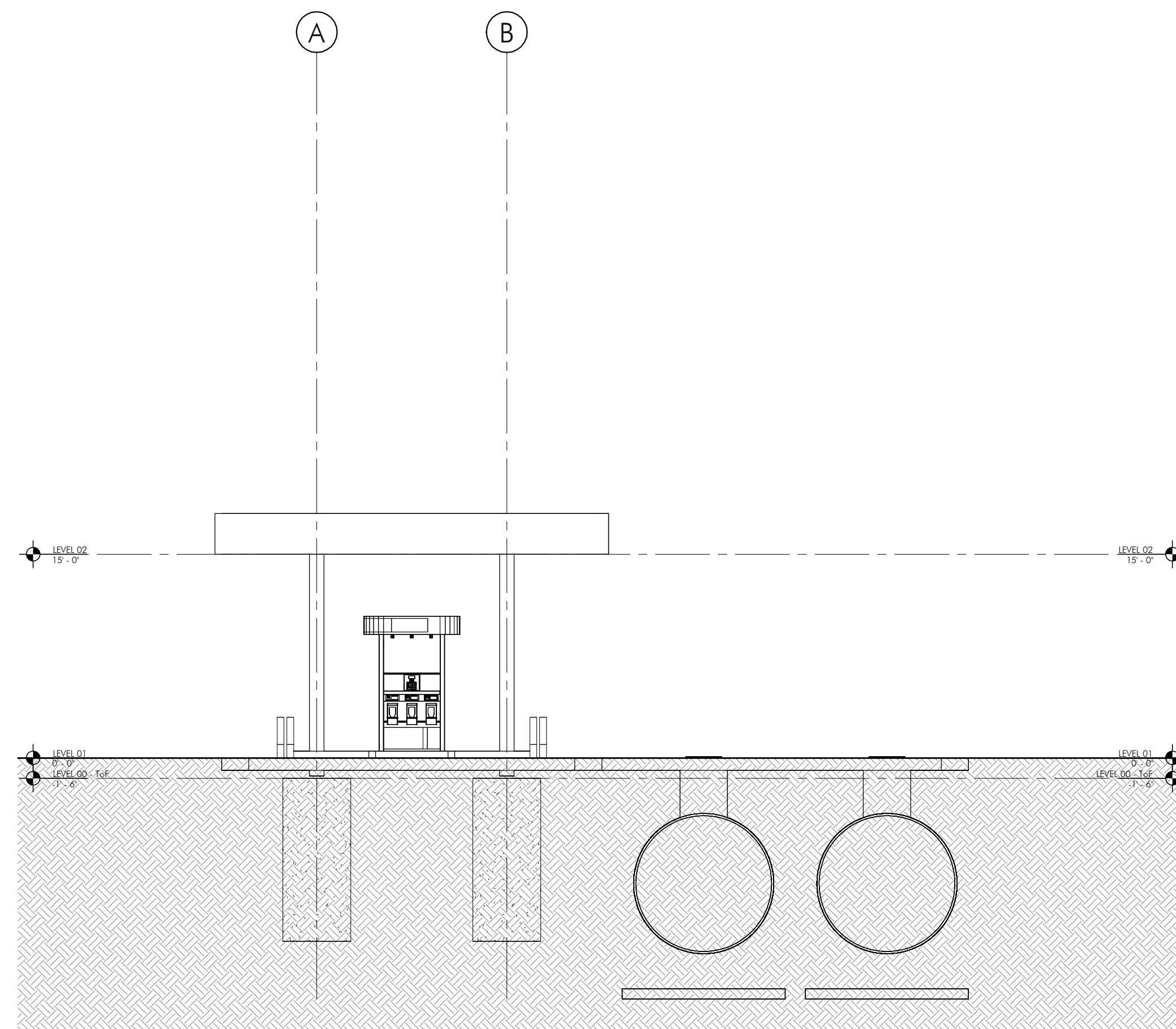
PROJECT # 2023-087
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REVISIONS

**Gas Station and C-Store
3-Pump Canopy**
911 Airport Road (at Stoltz Hill Road), Lebanon, OR 97355

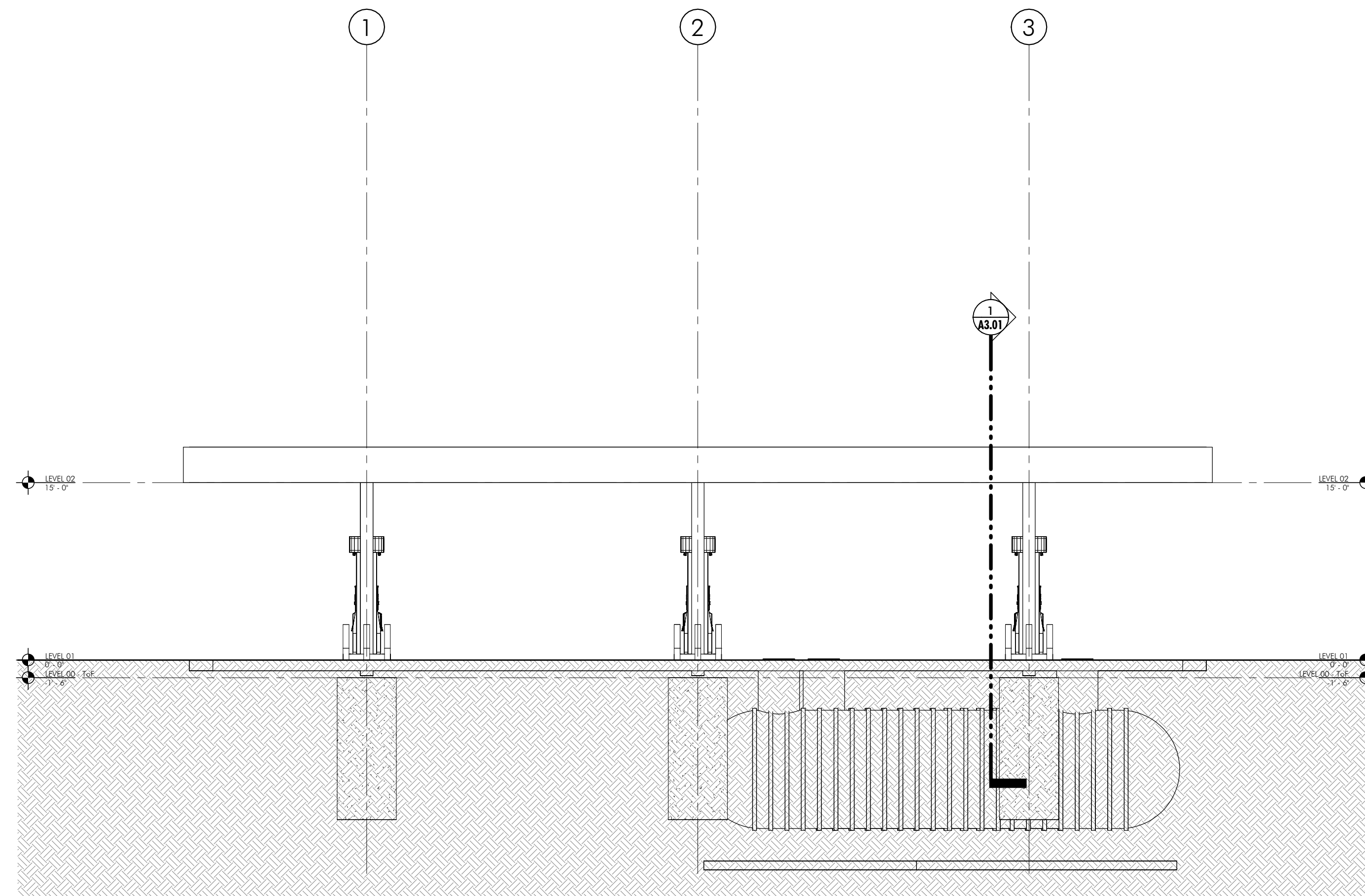
SHEET:
A2.03

ELEVATION NOTES:

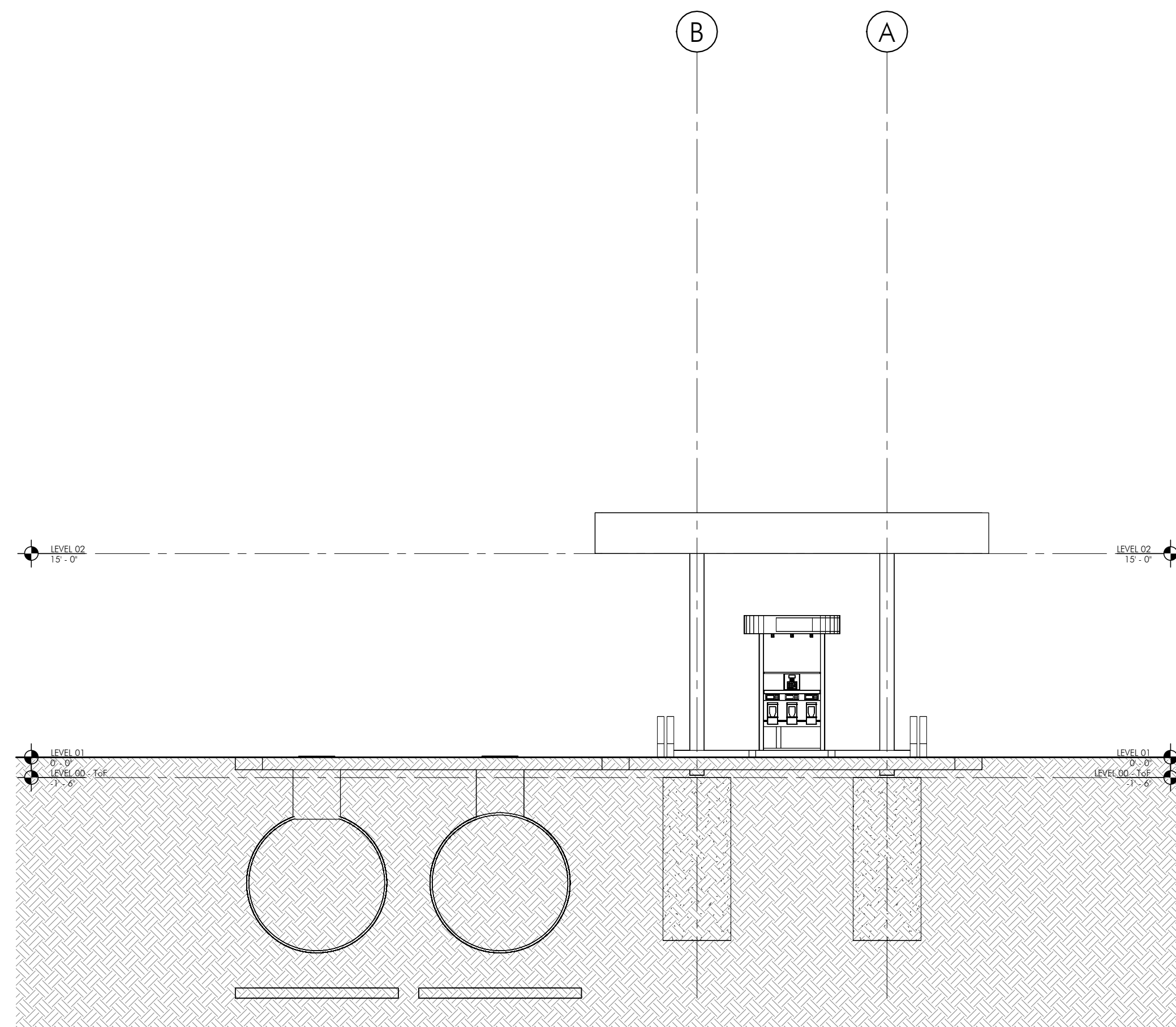
1 SAMPLE ELEVATION NOTE



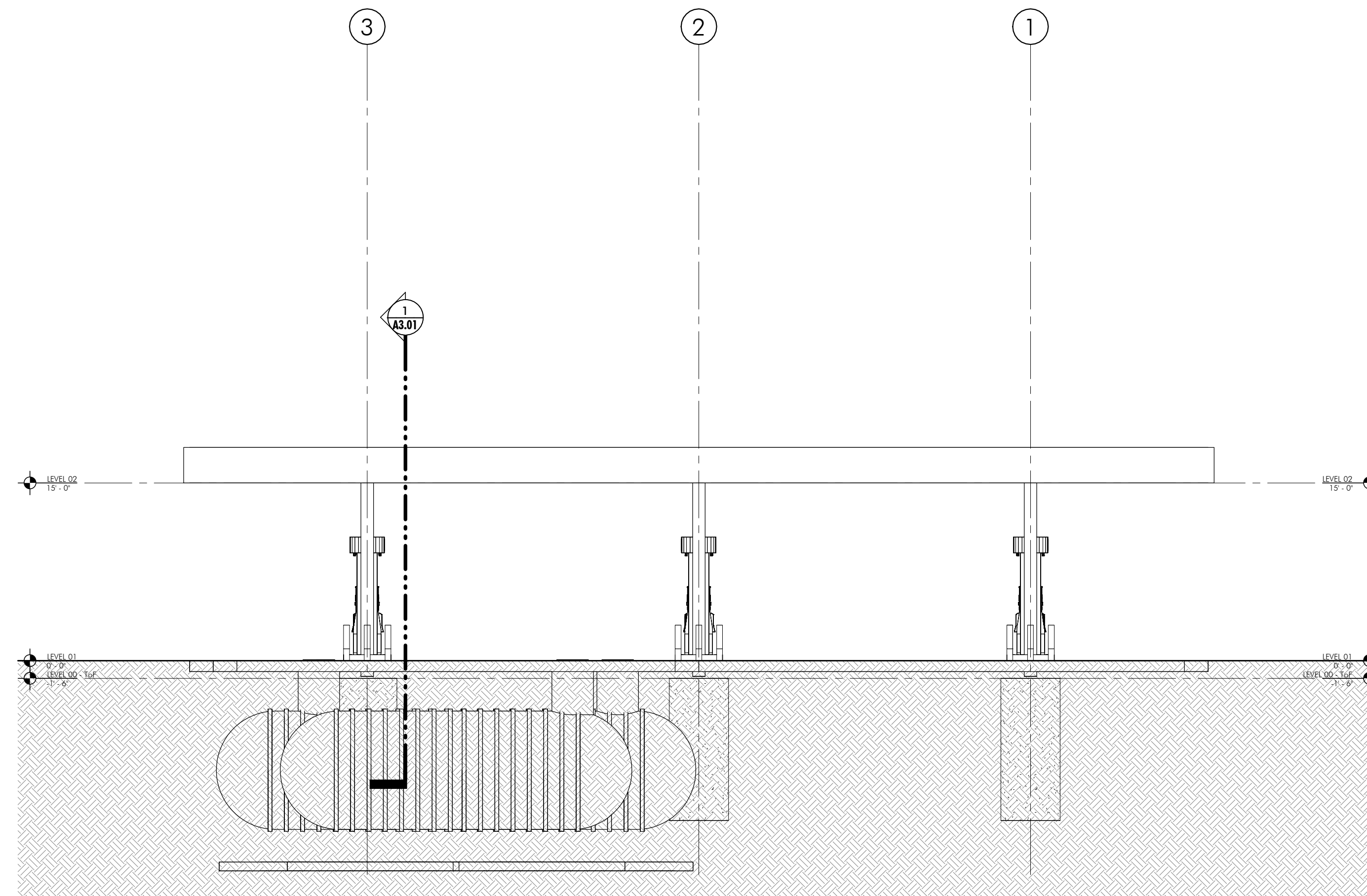
3 SOUTH ELEVATION
0' 2' 4' 8' 16' 24' 1/8" = 1'-0"



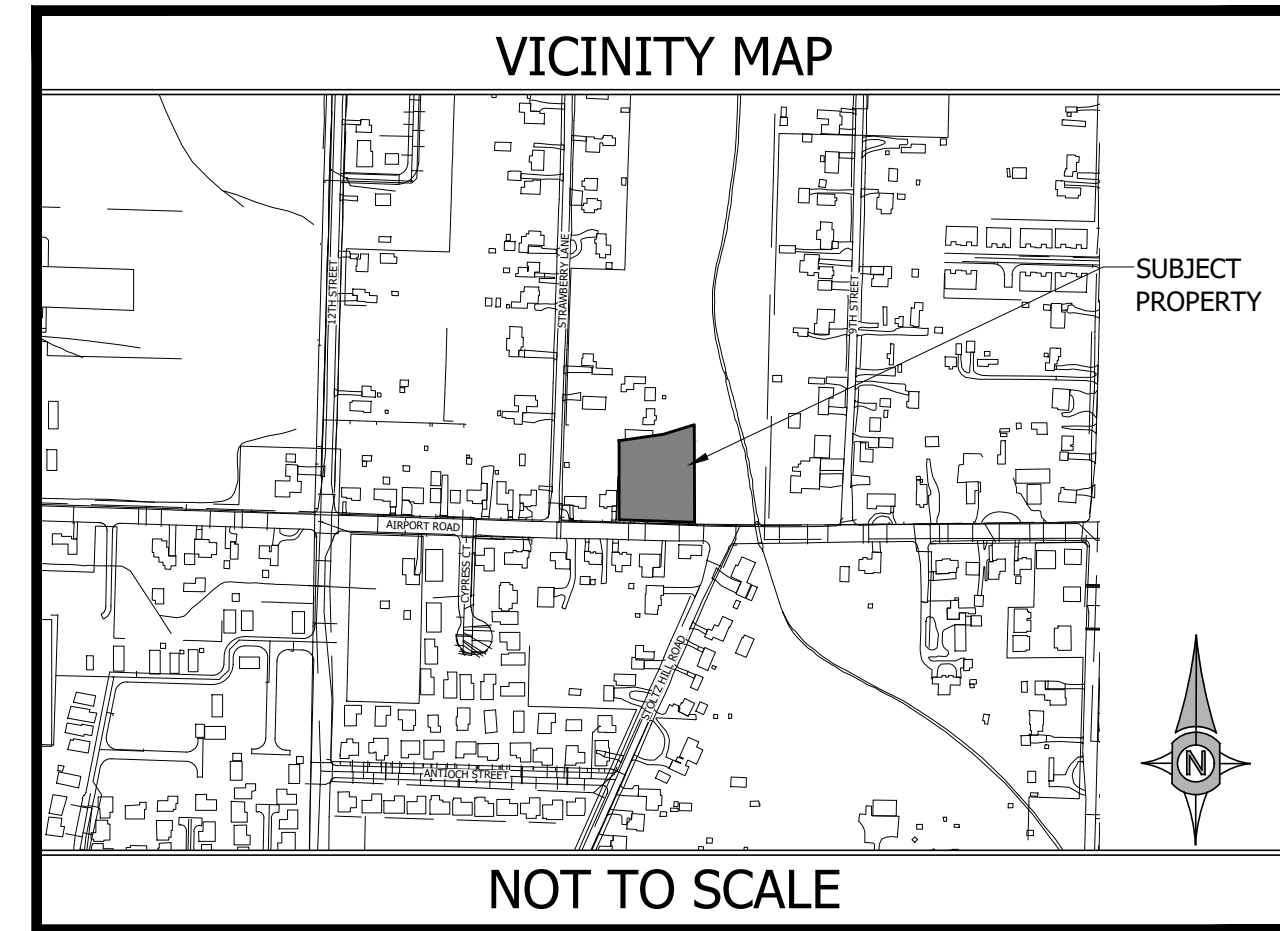
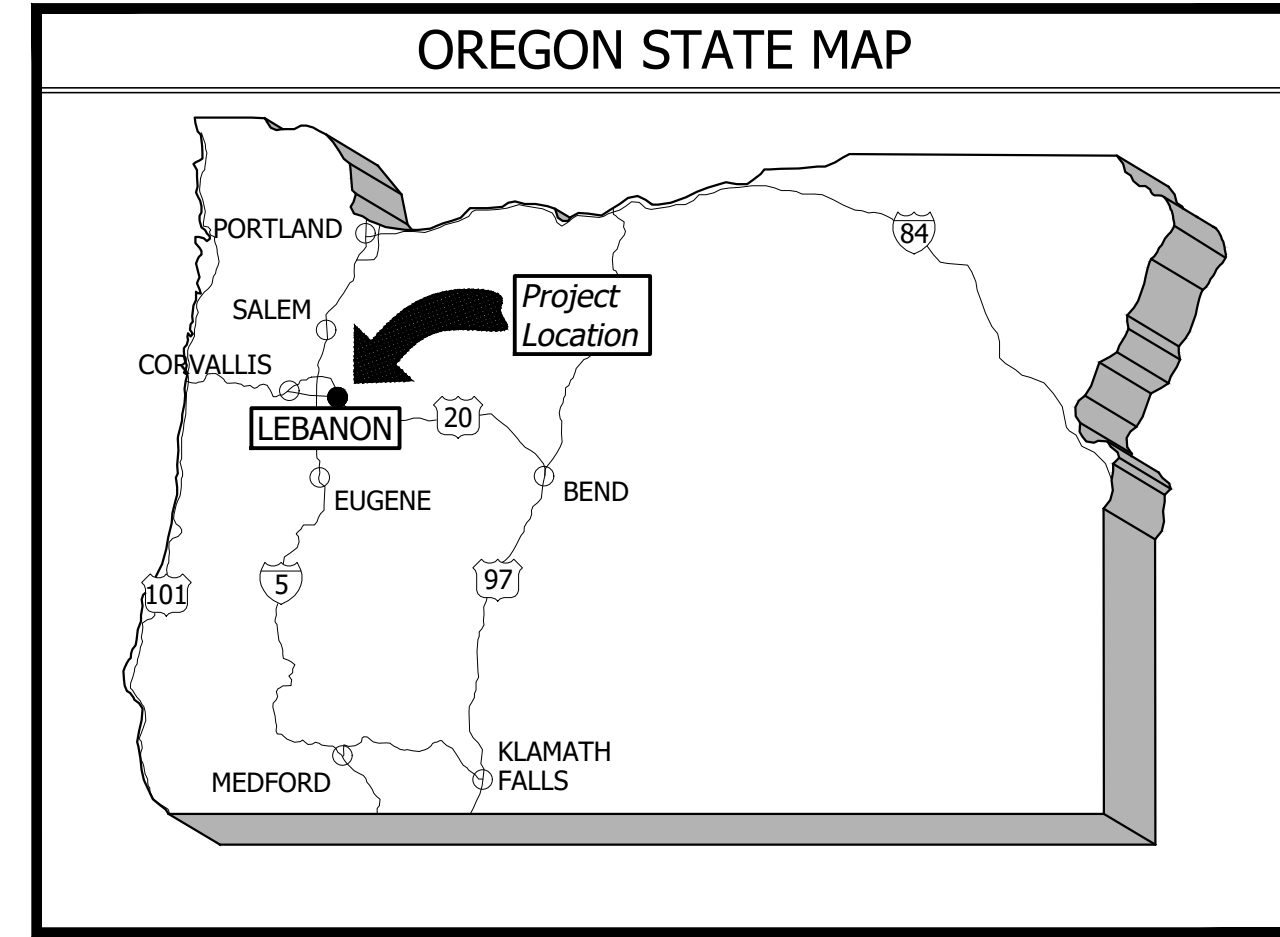
1 WEST ELEVATION
0' 2' 4' 8' 16' 24' 1/8" = 1'-0"



2 NORTH ELEVATION
0' 2' 4' 8' 16' 24' 1/8" = 1'-0"



4 EAST ELEVATION
0' 2' 4' 8' 16' 24' 1/8" = 1'-0"



SHEET INDEX

C100	- COVER SHEET
C101	- EXISTING CONDITIONS/DEMO PLAN
C200	- PRELIMINARY CIVIL SITE PLAN
C300	- PRELIMINARY GRADING AND DRAINAGE PLAN
C400	- PRELIMINARY UTILITY PLAN

SHEET REVISIONS

PROPERTY

TAX MAP: 12S-2W-15BD
TAX LOT: 1000

SITE ADDRESS: 911 S AIRPORT RD
LEBANON, OR 97355

DEVELOPER

SONA (RANDY) ATHWAL
6732 SEVEN MILE LANE SE
ALBANY, OR 97321
(541) 979-1794

OWNER

JASWANT SRANNA & SONA ATHWAL
2342 KOKANEE WAY
LEBANON, OR 97355
(541) 979-1794

DESIGN TEAM

CIVIL ENGINEER

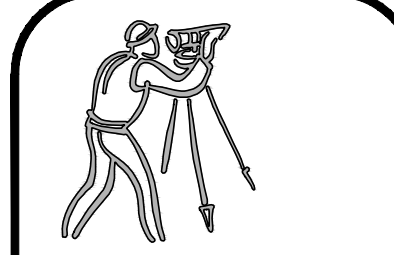
UDELL ENGINEERING AND LAND SURVEYING, LLC
63 E. ASH STREET
LEBANON, OREGON 97355
(541) 451-5125

SURVEYOR

UDELL ENGINEERING AND LAND SURVEYING, LLC
63 E. ASH STREET
LEBANON, OREGON 97355
(541) 451-5125

ARCHITECT

STUDIO 3 ARCHITECTURE, INC
275 COURT STREET NE
SALEM, OREGON 97301-3442
(503) 390-6500



CLIENT:
SONA (RANDY) ATHWAL
6732 SEVEN MILE LANE SE
ALBANY, OR 97321
(541) 979.1794

UDELL ENGINEERING AND LAND SURVEYING, LLC
63 EAST ASH ST.
LEBANON, OREGON 97355
(541) 451-5125 PH.
(541) 451-1366 FAX

COVER SHEET

AIRPORT ROAD C-STORE/FUELING
911 S AIRPORT ROAD
LEBANON, OREGON

DATE: AUGUST 18, 2023
PROJECT: AIRPORT ROAD C-STORE
DRAWN BY: BSV, MLM
CHECKED BY: BSV

PROJECT LEGENDS

EXISTING SYMBOL LEGEND	ABBREVIATIONS LEGEND	GRADING LEGEND
- EXISTING CONTROL POINT	SS - SANITARY SEWER	S = 0.00% - SURFACE DRAIN DIRECTION
- EXISTING GAS VALVE	SD - STORM DRAIN	(E)P 000.00 - EXISTING TOP OF PAVEMENT ELEVATION
- EXISTING WATER VALVE	MH - MANHOLE	(E)TC 000.00 - EXISTING TOP OF CURB ELEVATION
- EXISTING WATER METER	CB - CATCH BASIN	(E)G 000.00 - EXISTING GROUND ELEVATION
- EXISTING FIRE HYDRANT	CO - CLEAN OUT	P 000.00 - DESIGN TOP OF ASPHALT PAVEMENT ELEVATION
- EXISTING POWER POLE	PP - UTILITY POLE	C 000.00 - DESIGN TOP OF CONCRETE PAVEMENT ELEVATION
- EXISTING 6' CYCLONE FENCE	IE - INVERT ELEVATION (FLOW LINE)	W 000.00 - DESIGN TOP OF SIDEWALK ELEVATION
- EXISTING 6' WOOD FENCE	FF - FINISH FLOOR	TC 000.00 - DESIGN TOP OF CURB ELEVATION
- EXISTING SD LINE LOCATE	R.O.W. - RIGHT-OF-WAY	GR 000.00 - DESIGN TOP OF GRAVEL ELEVATION
- EXISTING SS LINE LOCATE	FD - FOUND	GND 000.00 - DESIGN TOP OF GROUND ELEVATION
- EXISTING WATER LINE LOCATE	CS - COUNTY SURVEY	TW 000.00 - DESIGN TOP OF WALL ELEVATION
- EXISTING PHONE LINE LOCATE	IR - IRON ROD	
- EXISTING UNDERGROUND POWER LOCATE	R - RADIUS	
- EXISTING OVERHEAD POWER	BO - BLOWOFF	
- EXISTING GAS LINE LOCATE	S.L. - STREET LIGHT	
- EXISTING COMMUNICATION LINE LOCATE	TC - TOP FACE OF CURB	
- EXISTING CONTOUR ELEVATION	BW - BACK OF WALK	
	CL - CENTERLINE	
	FH - FIRE HYDRANT	
	CI - CURB INLET	
	T.O.P. - TOP OF PIPE	
	FL - FLOW LINE	
	PUE - PUBLIC UTILITY EASEMENT	
	EX OR (E) - EXISTING	
	(P) - PROPOSED	
	PE - PLAIN END	
	FLGD OR FLG - FLANGED	
	MJ - MECHANICAL JOINT	
	D.I. - DUCTILE IRON	
	W/L - WATERLINE	
	TW - TOP OF WALL	
	SDAD - STORM DRAIN AREA DRAIN	

DESIGN SYMBOL LEGEND

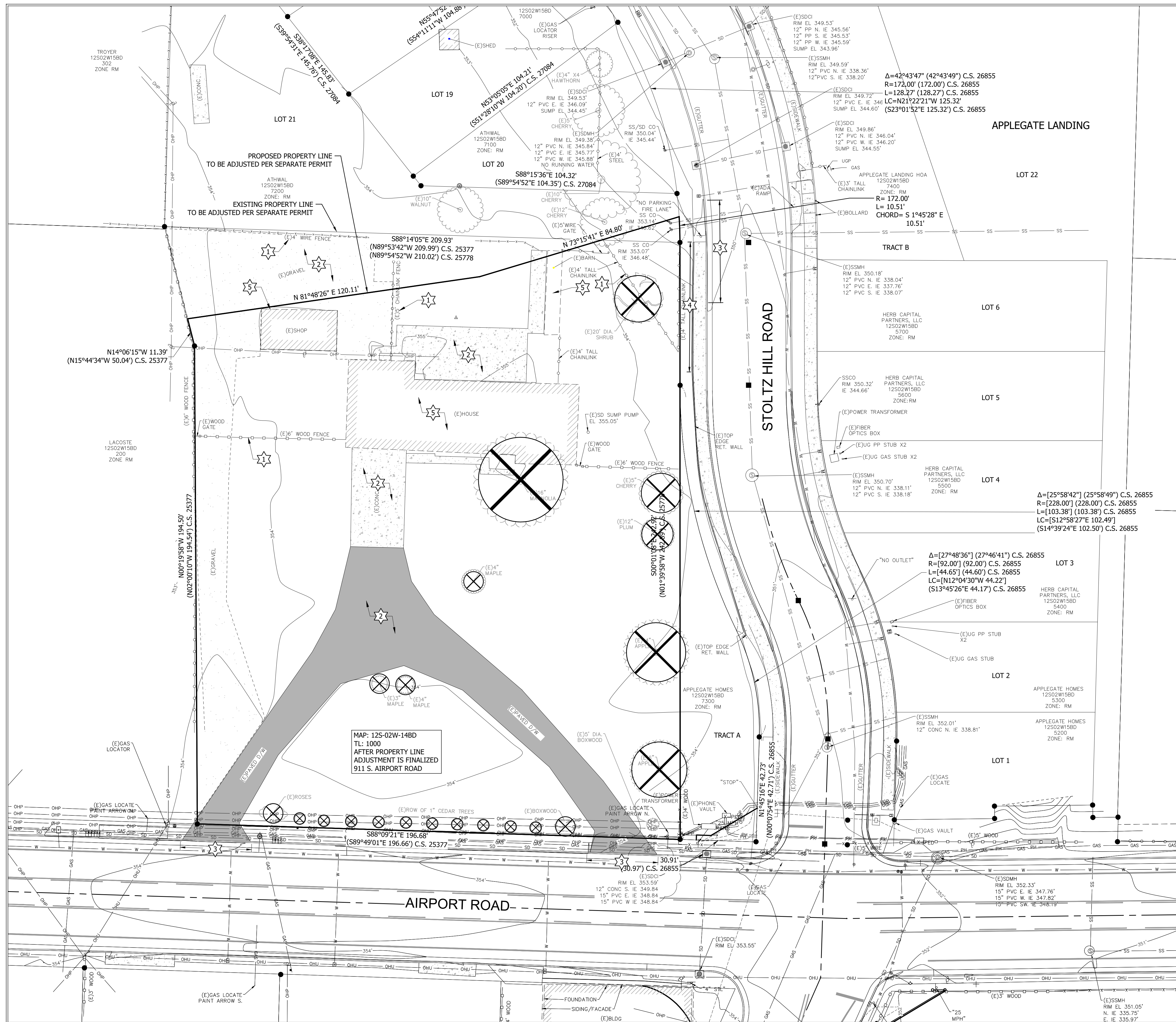
- DESIGN WATER METER
- DESIGN CURB INLET
- DESIGN CATCH BASIN
- DESIGN DITCH INLET
- DESIGN SS MANHOLE
- DESIGN SD MANHOLE
- DESIGN WATER VALVE
- DESIGN FIRE HYDRANT

THIS MAP WAS PREPARED FOR
PLANNING PURPOSES ONLY.
NOT TO BE USED FOR
CONSTRUCTION.

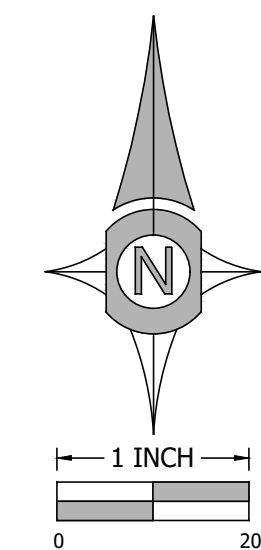
PLAN REVISIONS	DATE

Sheet **C100**




SCALE: SEE BARSCALE



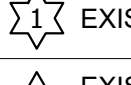
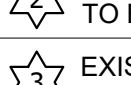
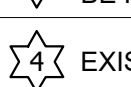


MAP: 125-02W-14BD
 TL: 1000
 AFTER PROPERTY LINE
 ADJUSTMENT IS FINALIZED
 911 S. AIRPORT ROAD

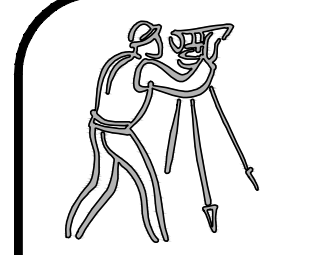


TREE LEGEND

-  EXISTING DECIDUOUS TREE TO REMAIN
-  EXISTING EVERGREEN TREE TO REMAIN
-  EXISTING TREE TO BE REMOVED

DEMOLITION NOTES

-  EXISTING FENCING TO BE REMOVED
-  EXISTING GRAVEL, CONCRETE, OR ASPHALT TO BE REMOVED.
-  EXISTING CURB, GUTTER, AND SIDEWALK TO BE REMOVED.
-  EXISTING RETAINING WALL TO BE REMOVED.
-  EXISTING STRUCTURE TO BE DEMOLISHED WITH PROPER PERMITS



CLIENT:

SOMA (RANDY) ATHWAL
 6732 SEVEN MILE LANE SE
 ALBANY, OR 97321
 (541) 979.1794

UDELL ENGINEERING
 AND
 LAND SURVEYING, LLC

63 EAST ASH ST.
 LEBANON, OREGON 97355
 (541) 451-5125 PH.
 (541) 451-1366 FAX

EXISTING CONDITIONS/DEMO PLAN

AIRPORT ROAD C-STORE/FUELING
 91.1 S AIRPORT ROAD
 LEBANON, OREGON

DATE:
 AUGUST 18, 2023

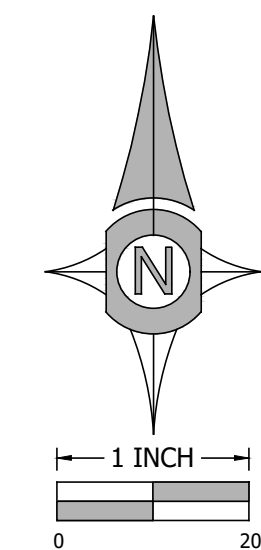
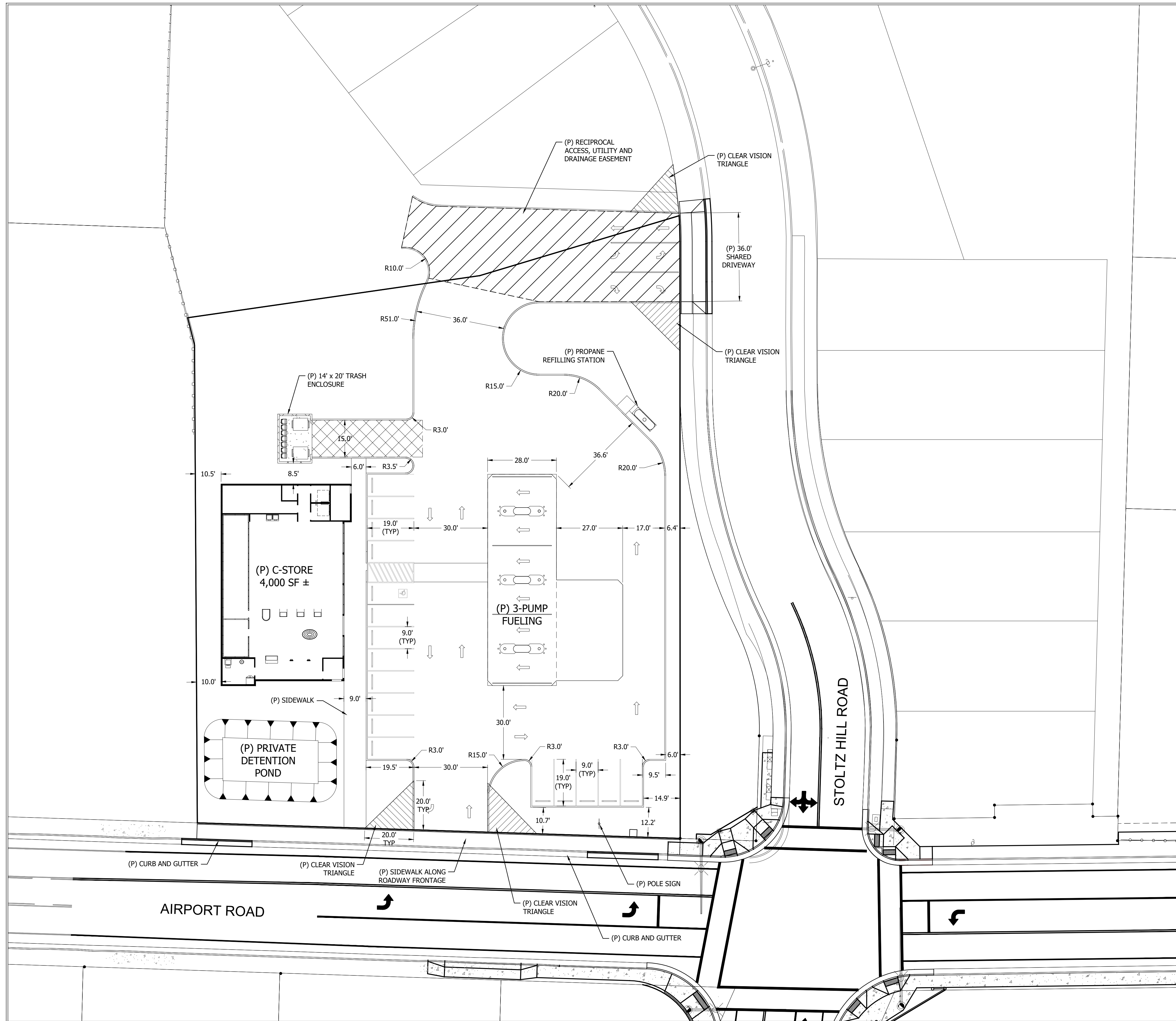
PROJECT:
 APPROPRIATIONAL
 ZONING
 DRAWN BY:
 BSV, MLM
 CHECKED BY:
 BSV

THIS MAP WAS PREPARED FOR
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 CONSTRUCTION.

PLAN REVISIONS	DATE

Sheet **C101**

SCALE: SEE BARSCALE



CLIENT:
 SONA (RANDY) ATHWAL
 6732 SEVEN MILE LANE SE
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UDELL ENGINEERING AND LAND SURVEYING, LLC
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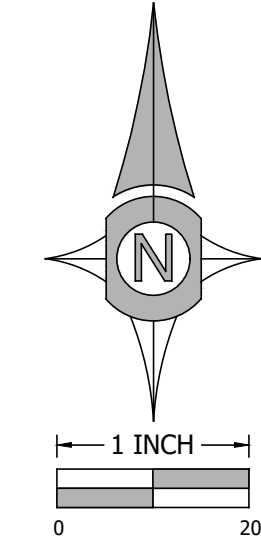
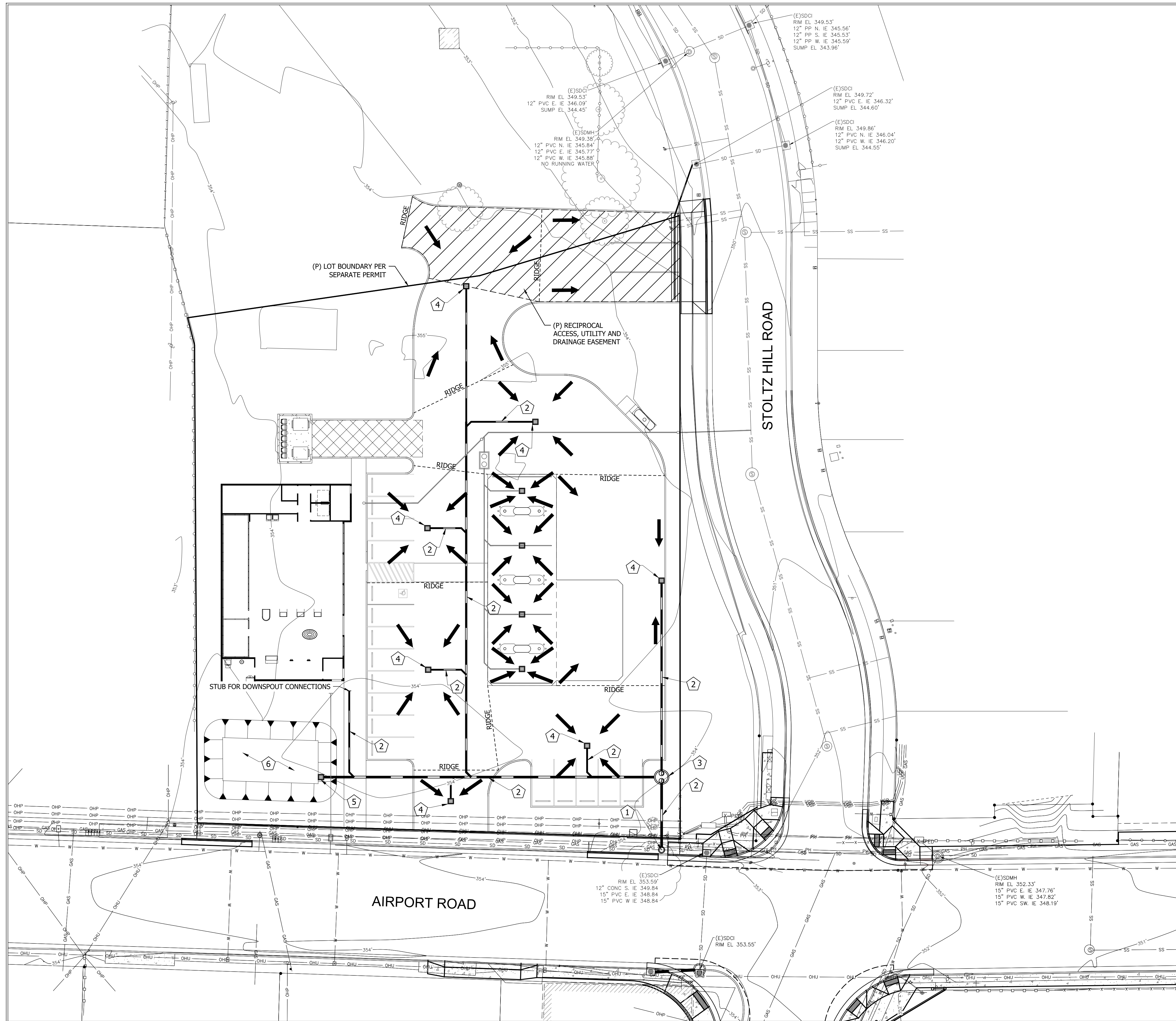
CONCEPTUAL CIVIL SITE PLAN
AIRPORT ROAD C-STORE/FUELING
91.1 S AIRPORT ROAD
LEBANON, OREGON

DATE: AUGUST 18, 2023
 PROJECT: AIRPORT ROAD C-STORE/FUELING
 DRAWN BY: BSV, MLM
 CHECKED BY: BSV

THIS MAP WAS PREPARED FOR PLANNING PURPOSES ONLY. NOT TO BE USED FOR CONSTRUCTION.

PLAN REVISIONS	DATE

Sheet **C200**
 SCALE: SEE BARSCALE

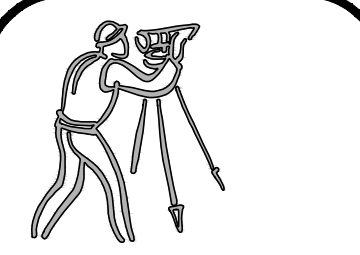


GRADING LEGEND

- 362 - EXISTING SURFACE CONTOUR ELEVATION
- 362 - DESIGN SURFACE CONTOUR ELEVATION
- EXISTING SURFACE DRAIN DIRECTION
- DESIGN SURFACE DRAIN DIRECTION

STORM DRAIN NOTES

- | | |
|--|---|
| | 1 CONSTRUCT MANHOLE OVER EXISTING STORM DRAIN MAINLINE. |
| | 2 INSTALL PRIVATE STORM DRAIN PIPE. |
| | 3 INSTALL FLOW CONTROL MANHOLE. |
| | 4 INSTALL PRIVATE CATCH BASIN. |
| | 5 INSTALL PRIVATE TYPE 'D' DITCH INLET. |
| | 6 INSTALL PRIVATE DETENTION POND. |



CLIENT:
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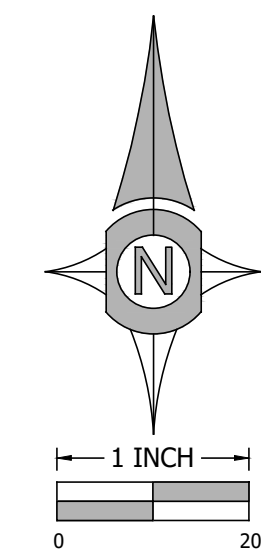
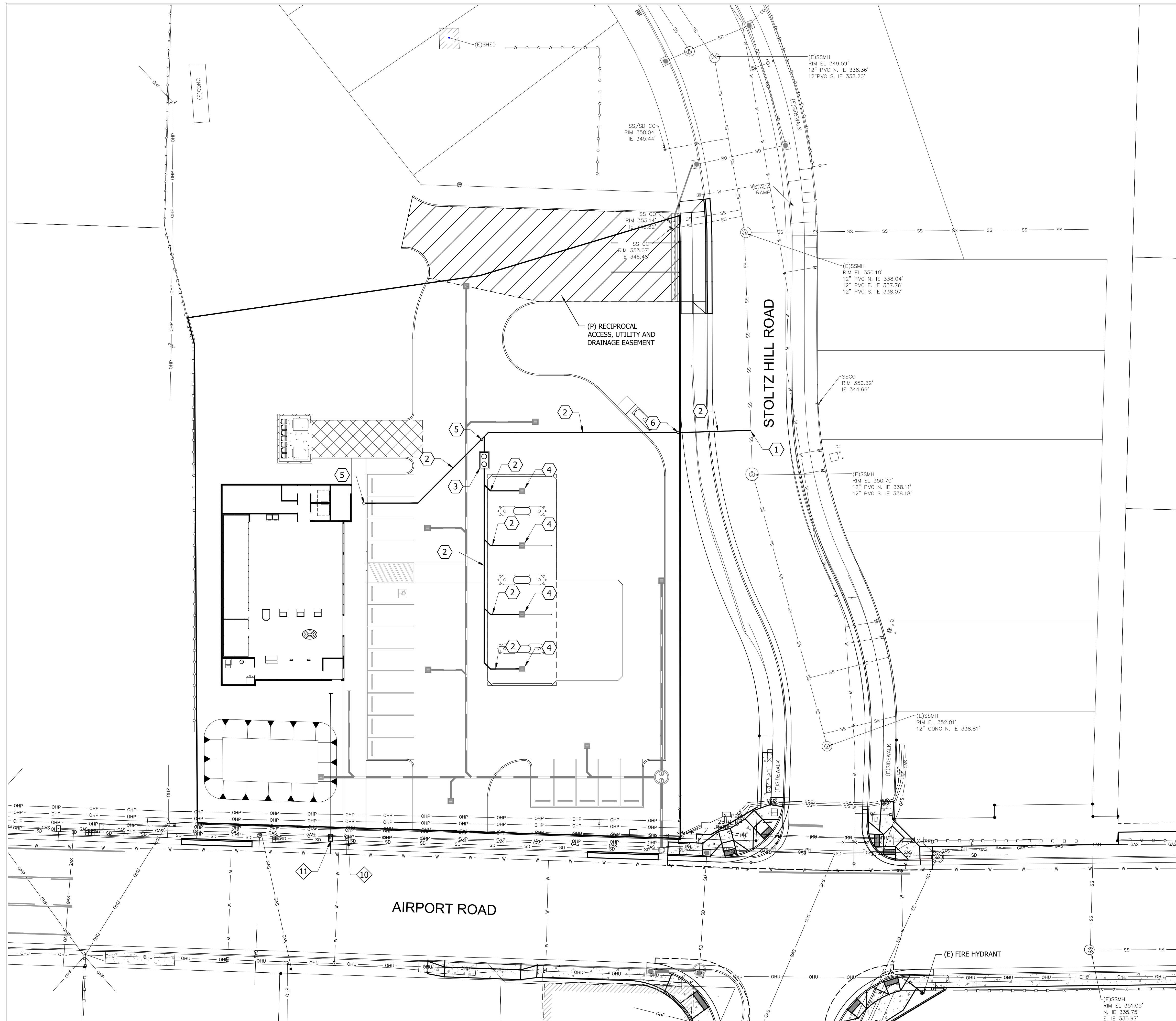
PRELIMINARY GRADING & DRAINAGE PLAN
 AIRPORT ROAD C-STORE/FUELING
 91.1 S AIRPORT ROAD
 LEBANON, OREGON

DATE: AUGUST 18, 2023
 PROJECT: SONA (RANDY) ATHWAL 91.1 S AIRPORT ROAD C-STORE/FUELING
 DRAWN BY: BSV, MLM
 CHECKED BY: BSV

THIS MAP WAS PREPARED FOR PLANNING PURPOSES ONLY. NOT TO BE USED FOR CONSTRUCTION.

PLAN REVISIONS	DATE

Sheet **C300**
 SCALE: SEE BARSCALE



- SANITARY SEWER NOTES**
- 1 CONNECT TO EXISTING SANITARY SEWER MAIN.
 - 2 INSTALL PRIVATE SANITARY SEWER PIPE.
 - 3 INSTALL PRIVATE OIL-WATER SEPARATOR.
 - 4 INSTALL PRIVATE CATCH BASIN TO SEWER PIPE.
 - 5 INSTALL PRIVATE SEWER CLEANOUT.
 - 6 INSTALL PUBLIC PROPERTY LINE SEWER CLEANOUT.

- PUBLIC WATER NOTES**
- 10 EXISTING WATER SERVICE TO BE USED FOR IRRIGATION.
 - 11 INSTALL PRIVATE DOMESTIC WATER SUPPLY LINE - SIZE TO BE DETERMINED WITH BUILDING PERMIT SUBMITTAL.

CLIENT:
 SONA (RANDY) ATHWAL
 6732 SEVEN MILE LANE SE
 ALBANY, OR 97321
 (541) 979.1794

UDELL ENGINEERING AND LAND SURVEYING, LLC
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 (541) 451-5125 PH.
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PRELIMINARY UTILITY PLAN
AIRPORT ROAD C-STORE/FUELING
 91.1 S AIRPORT ROAD
 LEBANON, OREGON

DATE: AUGUST 18, 2023
 PROJECT: PRELIMINARY UTILITY PLAN
 DRAWN BY: BSV, MLM
 CHECKED BY: BSV

THIS MAP WAS PREPARED FOR PLANNING PURPOSES ONLY. NOT TO BE USED FOR CONSTRUCTION.

PLAN REVISIONS	DATE

Sheet **C400**
 SCALE: SEE BARSCALE

From: [Kerin Benjamin](#)
To: [Kelly Hart](#)
Subject: Oct 18 hearing
Date: Tuesday, October 17, 2023 9:24:43 AM

Caution! This message was sent from outside your organization.

Lebanon Planning Commission ~

Regarding the proposal for a new convenience store and gas station on Airport Rd, Map & Tax Lot No. 12S02W15BD01000, please consider my concerns:

This proposal is an excellent one. Grampa's Grocery has already lost the aesthetic of nostalgia, does an incredible amount of business for a convenience store, and poses a danger both for traffic/pedestrian flow and merchandise deliveries. There is little to no visibility for a left-hand turn from Stoltz Hill onto Airport when delivery trucks are blocking the traffic for deliveries.

The effected intersection of Airport Rd/Stoltz Hill Rd is already a major traffic issue in desperate need of a traffic light. At the *very least*, a 4-way stop sign, which would impact the uncooperative home owner much less if the city will not/cannot exercise imminent domain to install the planned traffic light. The intersection at Airport/2nd St had adequate control under stop signs for many years. **Please do not** approve construction of this store and gas station until a traffic light is operational at this intersection.

Another desperately needed traffic light is the intersection of Vaughan Ln/S. Main. Why the city approved construction of the new apartments so close to the road is alarming with such poor consideration for safety impact. We have to nose out beyond the crosswalk (into "bike"/traffic lane) in order to safely assess oncoming traffic. It's my understanding that the city has no plans for a traffic light here. The city of Lebanon has created a safety issue and should be obligated to provide a traffic light as a solution. Traffic backs up worse now with the visibility issue during peak school commute times.

Thank you for listening,

Kerin Benjamin
916-717-0950

From: [James Lutz](#)
To: [Kelly Hart](#)
Subject: Planning Case number A-23-04,
Date: Tuesday, October 17, 2023 4:57:29 PM
Attachments: [image001.png](#)

Caution! This message was sent from outside your organization.

Hello Kelly and Councilors

On behalf of the Applegate Landing Home owners association, we are apposed to a fueling station being built at 911 Airport Road. A zoning change to allow a Fueling station and Minimart in a residential zoned housing neighborhood should not be allowed.

As the City would make the fueling station use a dead end residential cull de sac road for all access from the Fueling station, to the town of Lebanon. This would cause unwanted backing up of traffic on the dead end of Stoltz hill road, as well as continuous traffic lights at all hours of the night. There is already approved conditions for this fueling station on the south side of Airport road.

The Zoning change is apposed and the fueling station entrance on Stoltz is unnecessary and unwanted.

Thank you for upholding our current residential zoning for the citizens of Lebanon.

James C. Lutz



Agenda Item 2



925 S. Main Street
Lebanon, Oregon 97355
TEL: 541.258.4923
www.lebanonoregon.gov

MEMORANDUM

Administration

To: Mayor Jackola and City Council
From: Ron Whitlatch, Interim City Manager
Subject: **Republic Services Annual Rate Increase**

Date: October 25, 2023

I. BACKGROUND

Republic Services has the franchise for providing solid waste services for the City of Lebanon. The Franchise Agreement states that rates shall be approved by the City Council by a Resolution. Based on the letter submitted by Republic Services, they are proposing a 1% increase, effective, January 1, 2024.

II. RECOMMENDATION

Attached is a Resolution for approval of a 1% Rate Increase for Republic Services to be effective January 1, 2024.

**A RESOLUTION REPEALING RESOLUTION NO.)
2022-28 AND APPROVING REPUBLIC)
SERVICES RATE INCREASES)**

RESOLUTION NO. 2023-18

WHEREAS, the cost of providing garbage collection services by Republic Services (the franchisee) have increased since the last annual rate increase effective on January 1, 2023; and

WHEREAS, the franchisee has the necessity of a reasonable return on their investment; and

WHEREAS, the rates in Appendix "A" are comparable to other cities for similar services; and

WHEREAS, a rate increase and fee increase are in the public interest in order to meet basic service needs.

NOW, THEREFORE, BE IT RESOLVED that the Lebanon City Council approved the refuse collection rates as shown in Exhibit "A".

Section 1. The residential, commercial, and industrial rates listed in Exhibit "A" are effective as of January 1, 2024.

Passed by the Lebanon City Council and executed by the Mayor on this 8th day of November 2023 by a vote of ____yeas and ____nays.

CITY OF LEBANON, OREGON

Kenneth Jackola, Mayor
Michelle Steinhebel, Council President

ATTESTED BY:

Julie Fisher, City Recorder

EXHIBIT A - 2024 Lebanon Rates

FRANCHISE 5 - RATE EFFECTIVE 1/1/2024

New Service Increase Rates Effective 1/1/2024	
Residential Rate:	1.00%
Commercial Rate:	1.00%
Industrial Rate:	1.00%

RESIDENTIAL SERVICE RATES:

Residential Roll Cart	Current Monthly	Current Bi-Monthly	New Monthly	New Bi-monthly
1 Can (hand pickup)	\$32.80	\$65.60	\$33.13	\$66.26
20 gal	\$25.64	\$51.28	\$25.90	\$51.79
32 gal	\$28.24	\$56.48	\$28.52	\$57.04
32 gal on-call (per pickup)	\$16.42	N/A	\$16.58	N/A
64 gal	\$33.96	\$67.92	\$34.30	\$68.60
90 gal	\$40.77	\$81.54	\$41.18	\$82.36
Extra Rec	\$11.35	\$22.70	\$11.46	\$22.93
Extra YC	\$14.87	\$29.74	\$15.02	\$30.04
Monthly Recycling Processing Surcharge	\$2.00	\$4.00	\$2.00	\$4.00
Rec only	\$11.35	\$22.70	\$11.46	\$22.93
Up the drive	\$24.30	\$48.60	\$24.54	\$49.09
YC Only	\$14.87	\$29.74	\$15.02	\$30.04

Misc. Charges and Limited Services	Current Rate	New Rate
APP-Appliance Without Freon	\$35.76	\$36.12
APN-Appliance With Freon	\$49.34	\$49.83
FUR-Furniture	\$39.76	\$40.16
BU 1 - Dead large animal	\$149.24	\$150.73
MAT - Mattress or Boxspring	\$33.15	\$33.48
EXB-Extra 32 gal can/bag/box/heavy	\$11.35	\$11.46
EXC- Wash Cart	\$29.21	\$29.50
TRE-Christmas Trees (up to 8 ft and no tinsel, flocked, or after community tree pickup)	\$11.35	\$11.46
TIR-Tires w/o rim	\$16.12	\$16.28
BU3-Tires with rim	\$24.30	\$24.54
Truck Tires-ADTB	\$35.77	\$36.13
Truck Tires with rim-ADTB	\$51.87	\$52.39
Residential Extra Pickup (same day)	\$32.43	\$32.75
Residential Extra Pickup (off day)	\$64.97	\$65.62
Residential Container Exchange (one time per year no charge)	\$25.94	\$26.20
Residential Delivery - Per Cart	\$10.00	\$10.00

EXHIBIT A - 2024 Lebanon Rates

FRANCHISE 5 - RATE EFFECTIVE 1/1/2024

COMMERCIAL SERVICE RATES:				
Temporary Commercial			New Rate	
Container Size	DUMP	MAX RENT	DUMP	MAX RENT
300g YW (On service day only)	47.77	32.79	\$48.25	\$33.12
3 yd On Call	\$107.45	\$43.83	\$108.52	\$44.27
3yd daily rent (starts on 8th day after delivery)	RENT		RENT	
	\$4.64		\$4.69	
NO HEAVY & NON BURNABLES (SOD, DIRT, SHEETROCK, CONCRETE, ROOFING) Rent starts next day				

Commercial Front Load - Automatic Container Size	Weekly Collection Frequency (# Collections Per Week)						New Rate					
	1	2	3	4	5	EXT	1	2	3	4	5	EXT
1.5 yd	\$151.15	\$281.26	\$411.22	\$541.35	\$671.45	\$50.79	\$152.66	\$284.07	\$415.33	\$546.76	\$678.16	\$51.30
2 yd	\$183.64	\$343.63	\$488.92	\$664.37	\$824.52	\$61.16	\$185.48	\$347.07	\$493.81	\$671.01	\$832.77	\$61.77
3 yd	\$228.56	\$428.65	\$630.91	\$831.88	\$1,032.86	\$76.06	\$230.85	\$432.94	\$637.22	\$840.20	\$1,043.19	\$76.82
4 yd	\$272.38	\$514.31	\$755.98	\$997.78	\$1,239.70	\$95.58	\$275.10	\$519.45	\$763.54	\$1,007.76	\$1,252.10	\$96.54
6 yd	\$375.32	\$715.02	\$1,054.85	\$1,394.69	\$1,734.38	\$137.49	\$379.07	\$722.17	\$1,065.40	\$1,408.64	\$1,751.72	\$138.86
8 yd	\$458.19	\$872.31	\$1,286.69	\$1,701.05	\$2,155.98	\$17.08	\$462.77	\$881.03	\$1,299.56	\$1,718.06	\$2,177.54	\$17.25

Commercial Front Load - Manual Container Size	Weekly Collection Frequency (# Collections Per Week)						New Rate							
	1	2	3	4	5	ON CALL / EXT	RENT	1	2	3	4	5	ON CALL / EXT	RENT
1.5 yd	\$179.13	\$339.56	\$500.11	\$660.55	\$820.82	\$50.79	\$37.16	\$180.92	\$342.96	\$505.11	\$667.16	\$829.03	\$51.30	\$37.53
2 yd	\$219.26	\$417.25	\$615.34	\$813.18	\$1,011.16	\$61.16	\$40.54	\$221.45	\$421.42	\$621.49	\$821.31	\$1,021.27	\$61.77	\$40.95
3 yd	\$279.76	\$532.61	\$795.31	\$1,040.37	\$1,298.42	\$76.06	\$43.83	\$282.56	\$537.94	\$803.26	\$1,050.77	\$1,311.40	\$76.82	\$44.27
4 yd	\$334.09	\$637.04	\$937.97	\$1,240.25	\$1,541.04	\$95.58	\$47.12	\$337.43	\$643.41	\$947.35	\$1,252.65	\$1,556.45	\$96.54	\$47.59
6 yd	\$464.07	\$891.14	\$1,313.43	\$1,736.42	\$2,158.56	\$137.49	\$50.25	\$468.71	\$900.05	\$1,326.56	\$1,753.78	\$2,180.15	\$138.86	\$50.75
8 yd	\$562.51	\$1,074.24	\$1,576.66	\$2,078.55	\$2,571.03	\$17.08	\$53.78	\$568.14	\$1,084.98	\$1,592.43	\$2,099.34	\$2,596.74	\$17.25	\$54.32

Commercial Compactors Container Size	Weekly Collection Frequency (# Collections Per Week)					New Rate						
	1	2	3	4	5	EXT	1	2	3	4	5	EXT
2 yd	\$438.55	\$834.48	\$1,230.72	\$1,626.38	\$2,022.32	\$122.32	\$442.94	\$842.82	\$1,243.03	\$1,642.64	\$2,042.54	\$123.54
3 yd	\$559.51	\$1,065.21	\$1,590.62	\$2,080.73	\$2,596.85	\$152.10	\$565.11	\$1,075.86	\$1,606.53	\$2,101.54	\$2,622.82	\$153.62
4 yd	\$668.19	\$1,274.11	\$1,875.92	\$2,480.53	\$3,082.07	\$191.13	\$674.87	\$1,286.85	\$1,894.68	\$2,505.34	\$3,112.89	\$193.04
5 yd	\$798.16	\$1,528.34	\$2,251.43	\$2,976.53	\$3,699.87	\$229.39	\$806.14	\$1,543.62	\$2,273.94	\$3,006.30	\$3,736.87	\$231.68
6 yd	\$928.15	\$1,782.28	\$2,626.87	\$3,472.83	\$3,960.69	\$274.97	\$937.43	\$1,800.10	\$2,653.14	\$3,507.56	\$4,000.30	\$277.72
8 yd	\$1,125.01	\$2,148.47	\$3,153.33	\$4,157.12	\$5,142.05	\$354.16	\$1,136.26	\$2,169.95	\$3,184.86	\$4,198.69	\$5,193.47	\$357.70

COMPACTORS CHARGED AT 3X'S THE STANDARD PICKUP RATE

Misc. Charges and Limited Services	Current Rate	New Rate
Commercial Food Waste - Emptied Weekly	\$70.71	\$71.42
Commercial Food Waste - Emptied Weekly- extra cart (each)	\$11.02	\$11.13
Commercial Container Delivery/Redelivery	\$35.00	\$35.00
Commercial Exchange	\$54.34	\$54.88
Dry Run/Empty Removal-DRY (Commercial)	\$32.43	\$32.75
Each Extra Person Req To Pu Loose Garbage Per Min-EP1	\$2.04	\$2.06
Time per minute to p/u loose garbage-MIN	\$2.04	\$2.06
EP1- Container Off-route pickup- in area (25 min)	\$64.97	\$65.62
EP1- Container Off-route pickup- out of area (25 min)	\$64.97	\$65.62
Extra loose garbage per yard-EXY	\$29.21	\$29.50
EXY- Extra Yardage (Commercial)	\$29.21	\$29.50
LLK-Locking Container Set Up (Commercial)	\$74.52	\$75.27
LOC-Lock & Key	\$30.72	\$31.03
XCS- Container overweigh charges per 500lbs	\$29.35	\$29.64

On Call Containers/EXT Rates	Current Rate	Current Rate	New Rate	New Rate
	Dump Fee	Rent	Dump Fee	Rent
1yd	NA	NA	NA	NA
1.5yd	\$ 50.79	\$ 37.16	\$ 51.30	\$ 37.53
2yd	\$ 61.16	\$ 40.54	\$ 61.77	\$ 40.95
3yd	\$ 76.06	\$ 43.83	\$ 76.82	\$ 44.27
4yd	\$ 95.58	\$ 47.12	\$ 96.54	\$ 47.59
6yd	\$ 137.49	\$ 50.25	\$ 138.86	\$ 50.75
8yd	\$ 177.08	\$ 53.78	\$ 178.85	\$ 54.32

EXHIBIT A- 2024 Lebanon Rates

FRANCHISE 5 - RATE EFFECTIVE 1/1/2024

INDUSTRIAL SERVICE RATES:

Drop Box - Solid Waste - Open Top/Lidded					New Rate			
	10	20	30	40	10	20	30	40
PREPAYMENT	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00
Delivery	\$56.66	\$56.66	\$56.66	\$56.66	\$57.23	\$57.23	\$57.23	\$57.23
Haul	\$258.88	\$258.88	\$274.56	\$290.14	\$261.47	\$261.47	\$277.31	\$293.04
Disposal	\$56.00	\$56.00	\$56.00	\$56.00	\$56.56	\$56.56	\$56.56	\$56.56
Environmental Fee	\$19.71	\$19.71	\$19.71	\$19.71	\$19.91	\$19.91	\$19.91	\$19.91
Dry Run	\$56.51	\$56.51	\$56.51	\$56.51	\$57.08	\$57.08	\$57.08	\$57.08
Temp Rent - starts day after delivery	\$417.36	\$417.36	\$417.36	\$417.36	\$421.53	\$421.53	\$421.53	\$421.53
Perm Rent	\$152.36	\$152.36	\$152.36	\$152.36	\$153.88	\$153.88	\$153.88	\$153.88
Perm Rent Lidded	\$170.66	\$170.66	\$170.66	\$178.59	\$172.37	\$172.37	\$172.37	\$180.38
Perm Rent Lidded not crank	\$205.77	\$205.77	\$205.77	\$213.53	\$207.83	\$207.83	\$207.83	\$215.67

Drop Box - Asbestos					New Rate			
	10	20	30	40	10	20	30	40
PREPAYMENT	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00
Delivery	N/A	\$56.66	\$56.66	\$56.66	N/A	\$57.23	\$57.23	\$57.23
Haul	N/A	\$400.32	\$416.00	\$431.58	N/A	\$404.32	\$420.16	\$435.90
Disposal	N/A	Billed by Landfill	Billed by Landfill	Billed by Landfill	N/A	Billed by Landfill	Billed by Landfill	Billed by Landfill
Environmental Fee	N/A	\$19.71	\$19.71	\$19.71	N/A	\$19.91	\$19.71	\$19.71
Dry Run	N/A	\$56.51	\$56.51	\$56.51	N/A	\$57.08	\$57.08	\$57.08
Temp Rent - starts day after delivery	N/A	\$417.36	\$417.36	\$417.36	N/A	\$421.53	\$421.53	\$421.53
Perm Rent	N/A	\$152.36	\$152.36	\$152.36	N/A	\$153.88	\$153.88	\$153.88
Perm Rent Lidded	N/A	\$170.66	\$170.66	\$178.59	N/A	\$172.37	\$172.37	\$180.38

Drop Box - Concrete/Dirt/Brick	New Rate	
	10yrd Only	10yrd Only
PREPAYMENT	\$325.00	\$325.00
Delivery	\$56.66	\$57.23
Haul	\$258.88	\$261.47
Disposal (if container is contaminated, cust will be charged MSW rate)	\$80.00	\$80.80
Environmental Fee	\$19.71	\$19.91
Dry Run	\$56.51	\$57.08
Temp Rent - starts day after delivery	\$417.36	\$421.53
Perm Rent	\$152.36	\$153.88
Perm Rent Lidded	\$170.66	\$172.37

Drop Box - Woodwaste/Yardwaste					New Rate			
	10	20	30	40	10	20	30	40
PREPAYMENT	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00
Delivery	N/A	\$56.66	\$56.66	\$56.66	N/A	\$57.23	\$57.23	\$57.23
Haul	N/A	\$258.88	\$274.56	\$290.14	N/A	\$261.47	\$277.31	\$293.04
Disposal (if container is contaminated, cust will be charged MSW rate)	N/A	\$59.00	\$59.00	\$59.00	N/A	\$59.59	\$59.59	\$59.59
Environmental Fee	N/A	\$19.71	\$19.71	\$19.71	N/A	\$19.91	\$19.91	\$19.91
Dry Run	N/A	\$56.51	\$56.51	\$56.51	N/A	\$57.08	\$57.08	\$57.08
Temp Rent - starts day after delivery	N/A	\$417.36	\$417.36	\$417.36	N/A	\$421.53	\$421.53	\$421.53
Perm Rent	N/A	\$152.36	\$152.36	\$152.36	N/A	\$153.88	\$153.88	\$153.88
Perm Rent Lidded	N/A	\$170.66	\$170.66	\$178.59	N/A	\$172.37	\$172.37	\$180.38

Compactor - Solid Waste/Cardboard					New Rate			
	15	20	30	40	10	20	30	40
PREPAYMENT	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00	\$325.00
Delivery	\$56.53	\$56.66	\$56.66	\$56.66	\$57.10	\$57.23	\$57.23	\$57.23
Haul - trash	\$333.66	\$333.66	\$360.31	\$360.31	\$337.00	\$337.00	\$363.91	\$363.91
Haul - cardboard	\$56.00	\$56.00	\$56.00	\$56.00	\$56.56	\$56.56	\$56.56	\$56.56
Disposal - trash only	\$19.71	\$19.71	\$19.71	\$19.71	\$19.91	\$19.91	\$19.91	\$19.91
Environmental Fee	\$56.53	\$56.51	\$56.51	\$56.51	\$57.10	\$57.08	\$57.08	\$57.08
Dry Run	\$387.58	\$417.36	\$417.36	\$417.36	\$391.46	\$421.53	\$421.53	\$421.53
Temp Rent - starts day after delivery	\$144.32	\$152.36	\$152.36	\$152.36	\$145.76	\$153.88	\$153.88	\$153.88
Perm Rent	\$168.21	\$170.66	\$170.66	\$178.59	\$169.89	\$172.37	\$172.37	\$180.38
Perm Rent Lidded	\$202.34	\$205.77	\$205.77	\$213.53	\$204.36	\$207.83	\$207.83	\$215.67

Security Box	40	New Rate
Delivery per hour, one hour minimum	\$75.00	\$75.00
Rent	\$120.00	\$120.00

Misc. Charges and Limited Services	Current Rate	New Rate
Delivery - Industrial	\$56.66	\$57.23
Dry Run/Empty Removal-DRY (Industrial)	\$56.51	\$57.08
EXC - Industrial Exchange	\$56.66	\$57.23
EXC - Industrial Exchange Offsite	\$56.66	\$57.23
Industrial Relocate-REL	\$56.66	\$57.23

EXHIBIT A - 2024 Lebanon Rates

FRANCHISE 5 - RATE EFFECTIVE 1/1/2024

Fee	Current	New
Account Origination Fee	\$ 10.00	\$ 10.00
Cart Recovery/Replacement	\$ 75.00	\$ 75.00
Commercial Container Recovery Fee	\$ 480.00	\$ 480.00
Industrial Container Recovery Fee	\$ 3,000.00	\$ 3,000.00
Contaminated Recycle or Yard Waste Cart	\$ 15.00	\$ 15.00
Late Fee	1.50%	1.50%
Monthly Recycle Processing Surcharge	\$ 2.00	\$ 2.00
Returned Check Fee	\$ 25.00	\$ 25.00
Service Interrupt Fee	\$ 30.00	\$ 30.00

Agenda Item 3



925 S. Main Street
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MEMORANDUM

City Recorder's Office

To: Mayor Jackola and City Council

Date: October 24, 2023

From: Ron Whitlatch, Interim City Manager

Subject: Lebanon Chamber of Commerce – Tourism Agreement

I. INTRODUCTION

The City has a contract with the Lebanon Chamber of Commerce to provide tourism services to the community and visitors. The contract was updated and renewed in February of this year.

II. CURRENT REPORT

Cost increases for the operation of the Lebanon Visitors Center have made it increasingly difficult to provide the same level of service with the allocated funding. The agreed contract allocates \$35,000 annually for the operation of the Visitors Center and \$50,000 for the administration of tourism grants. The Chamber has requested an adjustment to the contract which will result in a zero net change overall but would increase funding for operation of the Visitors Center to \$42,000 (a \$7,000 increase) and reduced the funding for administering of tourism grants to \$43,000 (a \$7,000 decrease).

III. RECOMMENDATION

Approve the agreement as revised.

TOURISM SERVICES AGREEMENT

This Agreement is entered into this _____, 2023 by and between the City of Lebanon, a municipal corporation and political subdivision of the State of Oregon, with its principle office at 925 Main Street, Lebanon, Linn County, Oregon, and hereinafter referred to as "City" and the Lebanon Area Chamber of Commerce, a non-profit corporation organized under the laws of the State of Oregon, having its principle place of business located at 1040 Park Street, Lebanon, Linn County, Oregon, hereinafter referred to as "Chamber".

WITNESSETH

WHEREAS, the Lebanon Municipal Code, Chapter 3.12, provides for the imposition of a transit lodging tax on hotel, motel, recreation parks and other persons who do business within the City of Lebanon; and

WHEREAS, Section 3.12.160 of the Lebanon Municipal Code provides the formula for allocating revenues derived from the transient lodging tax to be spent for the promotion of tourism, and that it is the intent that revenue from the transient lodging tax shall be used to promote Lebanon, Oregon; and

WHEREAS, the Chamber desires to undertake and has the ability to support and promote tourism for the City of Lebanon; and

WHEREAS, the City desires to use the services of the Chamber pursuant to the requirements, limitations and direction of LMC Chapter 3.12.

NOW, THEREFORE, IN CONSIDERATION of the mutual promises and covenants contained herein, the parties agree as follows:

1. The Chamber agrees to staff and operate a Visitor's Information Center which will be open Monday thru Friday. Services provided will include, but not be limited to, providing local citizens and visitors with information about Lebanon area visitor facilities, recreational opportunities, city services, and provide a clearinghouse for the dissemination of other requested information about the Lebanon area. In addition, the Chamber shall provide coordination for the dissemination of information which is generated by written or e-mailed requests for information regarding Lebanon and the surrounding area.
2. The Chamber agrees to ensure that the "Visitor Information Center" signs which are on roads and highways in the general vicinity of the City will provide directions as to where persons may obtain further information about the Lebanon area.
3. The Chamber agrees to maintain office and telephone service which shall be open and available to the public from each Monday through Friday, except for holidays or such other times which may be determined reasonably necessary by the Chamber for prudent business reasons or safety issues. The Chamber further agrees to maintain limited Saturday hours for special events as requested by the City.
4. The Chamber agrees to maintain books, records, documents, and other accounting procedures, and to provide an annual report to the City Council that documents evidence of tourism supported functions of the Chamber. The above-described records shall be subject to the inspection, review, or audit by the City of Lebanon.

5. Chamber agrees to indemnify, defend, and hold harmless the City, its agents, officers, and employees, from and against any, and all liability, claims, suits, loss, damages, costs, and expenses arising out of or resulting from the negligent or intentional acts, errors or omissions of the Chamber, its officers, employees, or agents.
6. Chamber agrees to comply with the provisions of all Federal, State, and local laws and ordinances that are applicable to the performance under this contract. The Chamber shall comply with ORS 656.017 and provide Worker's Compensation coverage for all its subject workers.
7. The City shall provide the Chamber with the following funding:
 - a. The City agrees to pay to the Chamber \$42,000 each year out of the Transient Lodging Taxes, which are collected by the City after the date of execution of this agreement, payable in amounts of \$10,500 each quarter. This \$42,000 shall be used by the Chamber for staffing resources at the Visitor's Information Center. Any surplus funds not used for staffing resources at the Visitor's Information Center shall be used by the Chamber pursuant to section b. below. It is the intent of the Parties that the amount set forth herein shall fully cover the full-time staffing and operations at the Visitor's Information Center. If the Chamber determines that the amount set forth herein is insufficient to cover its intended purpose, the Parties agree to meet and confer in good faith to amend or update this Agreement.
 - b. The City agrees to pay to the Chamber \$43,000 each year to be used by the Chamber for grants, marketing or infrastructure related to the promotion of tourism in the City. Payments will be made quarterly in the amount of \$10,750. The Chamber's use of the funds shall be consistent with past and any future general direction of the City but shall be at the discretion of the Chamber. If income from the Transient Lodging Tax is more than 20 percent under its projections of \$200,000 per year, the payments set forth in this section b. shall be decreased a proportionate amount.
8. It is mutually agreed by the parties:
 - a. All funds paid to the Chamber pursuant to this agreement shall be used only as may be provided in this agreement, or according to law.
 - b. The parties always agree the Chamber shall be deemed as an "independent contractor" and not an agent of the City. Accordingly, the Chamber, while providing the labor or services required herein, is free from the direction and control over the means and manner of providing the labor or services, subject only to the right of the City to specify the desired results. The Chamber shall be responsible for obtaining and providing all tools or equipment necessary for the performance of the provisions of this agreement and retains the authority to hire and fire employees to perform the labor or services required herein. The Chamber assumes and continues to have all responsibility for paying its employees, withholding federal and state mandated income taxes, and making all necessary reports of the same to the government entities as required by law.
 - c. The Chamber agrees not to discriminate based on race, religion, sex, color, mental or physical disability or national origin in the enforcement and execution of this agreement.
 - d. This agreement is not subject to transfer by merger, consolidation, sale, assignment or otherwise without prior, express written consent of the City of Lebanon.
 - e. A waiver of any breach of any provision of this agreement by either party shall not operate as a waiver of any subsequent breach of the same or any other provision of this agreement.

- f. The term of this agreement shall be one year. This agreement shall automatically renew on a year-to-year basis unless terminated by either Party. Either party, upon written notice to the other party of no less than 90 days, may terminate this Agreement for any reason. Upon termination of this agreement, any unused Transient Lodging Tax funds shall be immediately paid over to the City. Upon termination, the Chamber shall also provide to the City, within thirty days, a current accounting of all funds expended pursuant to the Agreement up until the time of termination.

IN WITNESS WHEREOF, the parties have executed this Agreement at Linn County, Oregon, the day, and year first above written.

Ron Whitlatch, Interim City Manager

Rebecca Grizzle, Executive Director

Kenneth Jackola, Mayor

Chelle Davis, Board of Directors Chair

Agenda Item 4



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MEMORANDUM

Community Development

To: Mayor Jackola and City Council Date: October 17, 2023
From: Ron Whitlatch, Interim City Manager
Kelly Hart, Community Development Director
Subject: Accessory Dwelling Unit – System Development Charges Reduction Adoption

I. INTRODUCTION

At the September 13, 2023 City Council meeting, Council directed staff to modify the current fee structure for System Development Charges (SDCs) as it relates to Accessory Dwelling Units. The modification would allow Accessory Dwelling Units (ADUs) to be charged 40% of the current fees for Single Family Dwellings.

Attached is an Ordinance to amend the Lebanon Municipal Code to reduce fees for all SDCs for ADUs to 40% of the SDC rate for Single Family Dwellings.

II. RECOMMENDATION

Staff recommending that City Council approve an Ordinance to amend the language of the System Development Charges to include the 40% reduction in fees for Accessory Dwelling Units.

**A BILL FOR AN ORDINANCE AMENDING
SECTION 110 OF CHAPTER 13.12 SYSTEM
DEVELOPMENT CHARGES – EXEMPTIONS
OF THE LEBANON MUNICIPAL CODE**

) **ORDINANCE BILL NO. 2023-19**
)
)
) **ORDINANCE NO. 3014**

WHEREAS, the City Council identified Accessory Dwelling Units should be charged a lesser rate of System Development Charges than a single-family dwelling; and

WHEREAS, at the October 11, 2023 City Council meeting, after reviewing several alternatives, Council identified the preferred rate reduction to be 40% of the rate charged for single-family dwellings and directed staff to draft the necessary documents for adoption.

NOW, THEREFORE, THE CITY OF LEBANON ORDAINS AS FOLLOWS:

Section 1. Section 13.12.110 of Chapter 13 of the Lebanon Municipal Code is hereby replaced in its entirety with the following:

13.12.110 Exemptions and Reductions

- A. Structures and uses established and existing within the City limits on or before January 1, 1977 (except when new structures are created) are exempt from the charge, except water and sewer charges, to the extent of the structure or use existing on that date and to the extent of the parcel of land as it is constituted on that date. Structures and uses affected by this subsection shall pay the water or sewer charges pursuant to the terms of this ordinance upon the receipt of a permit to connect to the water or sewer system.
- B. An alteration, replacement or change in use that does not increase the parcel's or structure's use of a capital improvement are exempt from all portions of the system development charge.
- C. Mini-Warehouse Facilities as described in the Land Development Code are exempt from the Parks System Development Charge.
- D. Projects financed by city revenue are exempt from all portions of the systems development charge.
- E. **Accessory Dwelling Units (ADUs), are given a 40% reduction of the Water, Sanitary Sewer, Transportation, Parks, and Storm Drainage charges for Single Family Dwellings.**

Passed by the Lebanon City Council by a vote of _____ for and _____ against and approved by the Mayor this 8th day of November 2023.

Kenneth E Jackola, Mayor
Michelle Steinhebel, Council President

Attested:

Julie Fisher, City Recorder

Agenda Item 5



925 S. Main Street
Lebanon, Oregon 97355
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MEMORANDUM

Public Works Operations

To: City Council and Mayor Jackola
From: Jason Williams, Public Works Operations Director
Subject: Fluoride

Date: October 27, 2023

I. INTRODUCTION

On April 26, 2000, the Lebanon City Council passed a resolution ordering the Director of Public Works to introduce between 0.7 and 1.0 parts per million of fluoride into the City of Lebanon's public drinking water system.

Fluoridation is funded through city water fees and managed by the Director of Public Works. Below is the information on our current cost and dosing. Attachments included **Attachment A:** Safety Data Sheet on Hydrofluorosilicic Acid **Attachment B:** Photos of Fluoride Containment Area.

Annual Fluoride Costs	FY 2022 - 2023	\$12,800
Annual Caustic Costs	FY 2022 - 2023	\$14,715

Caustic Dosing - Fluoride On	2.50mg/L (milligrams per liter)
Caustic Dosing - Fluoride Off	1.00mg/L (milligrams per liter)

EPA (Environmental Protection Agency) Referenced Dosing:	
Minimum	0.8mg/L (milligrams per liter)
Investigation Level	2.0mg/L (milligrams per liter)
Maximum	4.0mg/L (milligrams per liter)

PPE (Personal Protective Equipment) Required:	
• Power aired purifying respirator system	
• Splash-proof safety goggles	
• Gauntlet neoprene gloves	
• Heavy-duty, acid-proof neoprene apron	

II: RECOMMENDATION

This information is for discussion purposes only.



**CASCADE
COLUMBIA
DISTRIBUTION
COMPANY**

COMPANY IDENTITY: Cascade Columbia Distribution Company
PRODUCT IDENTITY: HYDROFLUOROSILICIC ACID 24%

SDS DATE: 05/18/2015
ORIGINAL: 05/18/2015

SAFETY DATA SHEET

This Safety Data Sheet conforms to ANSI Z400.5, and to the format requirements of the Global Harmonizing System.
THIS SDS COMPLIES WITH 29 CFR 1910.1200 (HAZARD COMMUNICATION STANDARD)
IMPORTANT: Read this SDS before handling & disposing of this product.
Pass this information on to employees, customers, & users of this product.

SECTION 1. IDENTIFICATION OF THE SUBSTANCE OR MIXTURE AND OF THE SUPPLIER

PRODUCT IDENTITY: HYDROFLUOROSILICIC ACID 24%
SYNONYMS: Fluorosilicic Acid, Hexafluorosilicic Acid, HFS, FSA
PRODUCT USES: Chemical processing, water treatment

COMPANY IDENTITY: Cascade Columbia Distribution Company
COMPANY ADDRESS: 6900 Fox Avenue S.
COMPANY CITY: Seattle, WA 98108
COMPANY PHONE: 1-206-763-2351
EMERGENCY PHONES: CHEMTREC: 1-800-424-9300 (USA)
CANUTEC: 1-613-996-6666 (CANADA)

SECTION 2. HAZARDS IDENTIFICATION

DANGER!!



2.1 HAZARD STATEMENTS: (CAT = Hazard Category)

(H200s) PHYSICAL: Corrosive to Metals(CAT:1)
H290 MAY BE CORROSIVE TO METALS.
(H300s) HEALTH: Acute Toxicity, Oral(CAT:4)
H302 HARMFUL IF SWALLOWED.
(H300s) HEALTH: Skin Corrosion/Irritation(CAT:1)
H314 CAUSES SEVERE SKIN BURNS AND EYE DAMAGE.
(H305) Eye Damage/Irritation
H318 CAUSES SERIOUS EYE DAMAGE.

2.2 PRECAUTIONARY STATEMENTS:

EXPOSURE PREVENTION: AVOID ALL CONTACT!

P100s = General, P200s = Prevention, P300s = Response, P400s = Storage, P500s = Disposal

P234 Keep only in original container.
P260 Do not breathe dust/fume/gas/mist/vapors/spray.
P264 Wash with soap & water thoroughly after handling.
P270 Do not eat, drink or smoke when using this product.
P280 Wear protective gloves/protective clothing/eye protection/face protection.
P301+312 IF SWALLOWED: Call a Poison Center/doctor if you feel unwell.
P301+330+331 IF SWALLOWED: Rinse mouth. Do NOT induce vomiting.
P303+361+353 IF ON SKIN (OR HAIR): Take off immediately all contaminated clothing. Rinse skin with water/shower.
P304+340+310 IF INHALED: Remove victim to fresh air & keep at rest in a position comfortable for breathing. Immediately call a poison center/doctor.
P305+351+338 IF IN EYES: Rinse cautiously with water for several minutes. Remove contact lenses if present & easy to do - Continue rinsing.
P310 Immediately call a poison center/doctor.
P363 Wash contaminated clothing before reuse.
P390 Absorb spillage to prevent material damage.
P406 Store in corrosive-resistant container with a resistant inner liner.
P500 Dispose of contents in accordance with local/regional/federal regulations.

SEE SECTIONS 8, 11 & 12 FOR TOXICOLOGICAL INFORMATION.

SECTION 3. COMPOSITION/INFORMATION ON INGREDIENTS

MATERIAL	CAS#	EINECS#	WT %
Water	7732-18-5	231-791-2	75-77
Hydrofluorosilicic Acid	16961-83-4	-	23-25

The specific chemical component identities and/or the exact component percentages of this material may be withheld as trade secrets. This information is made available to health professionals, employees, and designated representatives in accordance with the applicable provisions of 29 CFR 1910.1200 (I)(1).

TRACE COMPONENTS: Trace ingredients (if any) are present in < 1% concentration, (< 0.1% for potential carcinogens, reproductive toxins, respiratory tract mutagens, and sensitizers). None of the trace ingredients contribute significant additional hazards at the concentrations that may be present in this product. All pertinent hazard information has been provided in this document, per the requirements of the Federal Occupational Safety and Health Administration Standard (29 CFR 1910.1200), U.S. State equivalents, and Canadian Hazardous Materials Identification System Standard (CPR 4).

SECTION 4. FIRST AID MEASURES

- 4.1 MOST IMPORTANT SYMPTOMS/EFFECTS, ACUTE & CHRONIC:
See Section 11 for Symptoms/Effects (acute & chronic).
- 4.2 EYE CONTACT:
For eyes, flush with plenty of water for 15 - 20 minutes & get medical attention.
- 4.3 SKIN CONTACT:
In case of contact with skin immediately remove contaminated clothing. Immediately flush with plenty of water. Get medical attention immediately.
- 4.4 INHALATION:
Remove to fresh air. If breathing is difficult, give oxygen. If breathing has stopped, trained personnel should immediately begin artificial respiration. Get medical attention.
- 4.5 SWALLOWING:
GET MEDICAL ATTENTION IMMEDIATELY in all cases. Do NOT induce vomiting. If patient is conscious, rinse mouth with water. Do NOT give liquids to an unconscious or convulsing person.
- 4.6 RESCUERS: Victims of chemical exposure must be taken for medical attention. Rescuers should be taken for medical attention, if necessary. Take a copy of label and SDS to physician or health professional with victim.
- 4.7 NOTES TO PHYSICIAN:
There is no specific antidote. Treatment of overexposure should be directed at the control of symptoms and the clinical condition of the patient. Any material aspirated during vomiting may cause lung injury. Therefore, emesis should not be induced mechanically or pharmacologically. If it is considered necessary to evacuate the stomach contents, this should be done by means least likely to cause aspiration (such as: Gastric lavage after endotracheal intubation).

SECTION 5. FIRE FIGHTING MEASURES

- 5.1 FIRE & EXPLOSION PREVENTIVE MEASURES:
NO open flames.
- 5.2 SUITABLE (& UNSUITABLE) EXTINGUISHING MEDIA:
Use dry powder, foam, carbon dioxide, water spray, sand. Do not use heavy water stream.
- 5.3 SPECIAL PROTECTIVE EQUIPMENT & PRECAUTIONS FOR FIRE FIGHTERS:
Cool closed containers. Use fog nozzles if water is used.
Do not enter confined fire-space without full chemical-resistant suit and self-contained breathing apparatus.

SECTION 5. FIRE FIGHTING MEASURES (CONTINUED)

5.4 SPECIFIC HAZARDS OF CHEMICAL & HAZARDOUS COMBUSTION PRODUCTS:
Hydrofluorosilicic acid is not flammable, but when heated to decomposition, highly toxic and corrosive fumes of fluorides are emitted. May generate flammable and explosive hydrogen gas in contact with most metals.

SECTION 6. ACCIDENTAL RELEASE MEASURES

6.1 PERSONAL PRECAUTIONS, PROTECTIVE EQUIPMENT & EMERGENCY PROCEDURES:
Keep unprotected personnel away. Use complete chemical protective suit with self-contained breathing apparatus.

6.2 ENVIRONMENTAL PRECAUTIONS:
Keep from entering storm sewers and ditches which lead to waterways. Notify authorities if liquid enters these.

6.3 METHODS & MATERIAL FOR CONTAINMENT & CLEAN-UP:
Stop small or large spill at source. Dike area with sand or earth, and contain. Acid may be neutralized with hydrated lime or sodium carbonate. Collect leaking & spilled liquid in sealable containers. Dispose of in accordance with regulations (see Section 13).

SECTION 7. HANDLING AND STORAGE

7.1 PRECAUTIONS FOR SAFE HANDLING:
Isolate from oxidizers, alkalis, heat, & open flame. Use only with adequate ventilation. Avoid breathing of vapor or spray mist. Do not get in eyes, on skin or clothing. Wear goggles, face shield, gloves, apron & footwear impervious to material. Wash clothing before reuse.

7.2 CONDITIONS FOR SAFE STORAGE, INCLUDING ANY INCOMPATIBILITIES:
Incompatible with strong oxidants, strong bases, strong acids, metals, alkalis, stoneware, glass. Keep inside a dry, cool, well-ventilated room. Do not store above 49 C/120 F. Keep away from direct sunlight. Keep container tightly closed & upright when not in use to prevent leakage.

SECTION 8. EXPOSURE CONTROLS/PERSONAL PROTECTION

8.1 EXPOSURE LIMITS:

MATERIAL	CAS#	EINECS#	TWA (OSHA)	TLV (ACGIH)
Water	7732-18-5	231-791-2	None Known	None Known
Hydrofluorosilicic Acid	16961-83-4	-	None Known	2.5 mg/m3

Each component showing 'Yes' under "HAP" is an EPA Hazardous Air Pollutant.

8.2 APPROPRIATE ENGINEERING CONTROLS:

RESPIRATORY EXPOSURE CONTROLS

A respiratory protection program that meets OSHA 29 CFR 1910.134 and ANSI Z86.2 requirements or European Standard EN 149 must be followed whenever workplace conditions warrant a respirator's use.

VENTILATION

LOCAL EXHAUST: Necessary
SPECIAL: None
MECHANICAL (GENERAL): Necessary
OTHER: None
Please refer to ACGIH document, "Industrial Ventilation, A Manual of Recommended Practices", most recent edition, for details.

SECTION 8. EXPOSURE CONTROLS/PERSONAL PROTECTION (CONTINUED)

8.3 INDIVIDUAL PROTECTION MEASURES, SUCH AS PERSONAL PROTECTIVE EQUIPMENT:

PERSONAL PROTECTIONS:

Wear goggles, face shield, gloves, apron & footwear impervious to material. Wash clothing before reuse.

WORK & HYGIENIC PRACTICES:

Provide readily accessible eye wash stations & safety showers.
Wash at end of each workshift & before eating, smoking or using the toilet.
Promptly remove clothing that becomes contaminated. Destroy contaminated leather articles. Launder or discard contaminated clothing.

SECTION 9. PHYSICAL & CHEMICAL PROPERTIES

APPEARANCE:	Liquid, clear, colorless to pale straw
ODOR:	Pungent, Irritating Odor
ODOR THRESHOLD:	Not Available
pH (Neutrality):	1.0
MELTING POINT/FREEZING POINT:	Not Available
BOILING RANGE (IBP, 50%, Dry Point):	> 105 C / > 221 F
FLASH POINT (TEST METHOD):	Not Applicable
EVAPORATION RATE (n-Butyl Acetate=1):	0.291
FLAMMABILITY CLASSIFICATION:	Not available
LOWER FLAMMABLE LIMIT IN AIR (% by vol):	Not available
UPPER FLAMMABLE LIMIT IN AIR (% by vol):	Not available
VAPOR PRESSURE (mm of Hg)	32 hPa (24 mmHg) at 25 C / 77 F
VAPOR DENSITY (air=1):	Not applicable
GRAVITY @ 68/68 F / 20/20 C:	
DENSITY:	1.198
SPECIFIC GRAVITY (Water=1):	1.200
POUNDS/GALLON:	9.7 - 10.2
WATER SOLUBILITY:	Complete
PARTITION COEFFICIENT (n-Octane/Water):	Not Available
AUTO IGNITION TEMPERATURE:	Not Applicable
DECOMPOSITION TEMPERATURE:	Not Available
REFRACTIVE INDEX:	Not Available
VOCs (>0.044 Lbs/Sq In) :	0.0 Vol% / 0.0 g/L / 0.000 Lbs/Gal
TOTAL VOC'S (TVOC)*:	0.0 Vol% / 0.0 g/L / 0.000 Lbs/Gal
NONEXEMPT VOC'S (CVOC)*:	0.0 Vol% / 0.0 g/L / 0.000 Lbs/Gal
HAZARDOUS AIR POLLUTANTS (HAPS):	Not available
NONEXEMPT VOC PARTIAL PRESSURE (mm of Hg @ 20 C)	0.0
VISCOSITY @ 20 C (ASTM D445):	Not Available

* Using CARB (California Air Resources Board Rules).

SECTION 10. STABILITY & REACTIVITY

10.1 REACTIVITY & CHEMICAL STABILITY:

Stable under normal conditions.

10.2 POSSIBILITY OF HAZARDOUS REACTIONS & CONDITIONS TO AVOID:

Isolate from oxidizers, alkalis, extreme heat (above 90 C / 194 F), direct sunlight & open flame.

10.3 INCOMPATIBLE MATERIALS:

Metals, stoneware, strong acids, strong bases, alkali metals, explosives, toxicants, readily oxidizable materials, combustible solids, and organic peroxides. Corrosive to most metals, including steel, aluminum, copper, and copper alloys.

10.4 HAZARDOUS DECOMPOSITION PRODUCTS:

Corrosive fumes of fluorides from extreme heating, carbon monoxide, carbon dioxide.

10.5 HAZARDOUS POLYMERIZATION:

Will not occur.

SECTION 11. TOXICOLOGICAL INFORMATION

11.1 ACUTE HAZARDS

11.1.1 SKIN CONTACT:

Corrosive. Severe burns to skin, defatting, dermatitis. Wash thoroughly after handling.

11.1.2 EYE CONTACT:

Corrosive. Severe burns to eyes, redness, tearing, blurred vision.

11.1.3 INHALATION:

Corrosive. Severe respiratory tract irritation may occur. Vapor harmful.

11.1.4 SWALLOWING:

Corrosive. Harmful if swallowed.
The symptoms of chemical pneumonitis may be delayed.

11.2 SUBCHRONIC HAZARDS/CONDITIONS AGGRAVATED

MEDICAL CONDITIONS AGGRAVATED BY EXPOSURE:

Pre-existing disorders of any target organs mentioned in this Document can be aggravated by over-exposure by routes of entry to components of this product. Prolonged or repeated overexposure to fluoride compounds may cause fluorosis. Fluorosis is characterized by skeletal changes, consisting of osteosclerosis (hardening or abnormal density of bone) and osteomalacia (softening of bones) and by mottled discoloration of the enamel of teeth (if exposure occurs during enamel formation). Symptoms may include bone and joint pain and limited range of motion. Conditions aggravated by exposure may include skin and respiratory (asthma-like) disorders.

11.3 CHRONIC HAZARDS

11.3.1 CANCER, REPRODUCTIVE & OTHER CHRONIC HAZARDS:

This product has no carcinogens listed by IARC, NTP, NIOSH, OSHA or ACGIH, as of this date, greater or equal to 0.1%.

11.3.2 TARGET ORGANS: May cause damage to target organs, based on animal data.

11.3.3 IRRITANCY: Irritating to contaminated tissue.

11.3.4 SENSITIZATION: No component is known as a sensitizer.

11.3.5 MUTAGENICITY: No known reports of mutagenic effects in humans.

11.3.6 EMBRYOTOXICITY: No known reports of embryotoxic effects in humans.

11.3.7 TERATOGENICITY: No known reports of teratogenic effects in humans.

11.3.8 REPRODUCTIVE TOXICITY: No known reports of reproductive effects in humans.

A MUTAGEN is a chemical which causes permanent changes to genetic material (DNA) such that the changes will propagate across generational lines. An EMBRYOTOXIN is a chemical which causes damage to a developing embryo (such as: within the first 8 weeks of pregnancy in humans), but the damage does not propagate across generational lines. A TERATOGEN is a chemical which causes damage to a developing fetus, but the damage does not propagate across generational lines. A REPRODUCTIVE TOXIN is any substance which interferes in any way with the reproductive process.

11.4 MAMMALIAN TOXICITY INFORMATION

LD50 (Oral):	430 mg/kg (rat)	LC50 (inhale)
LC50 (inhale)	850-1070PPM / 1 h (rat)	50

SECTION 12. ECOLOGICAL INFORMATION

12.1 ALL WORK PRACTICES MUST BE AIMED AT ELIMINATING ENVIRONMENTAL CONTAMINATION.

12.2 EFFECT OF MATERIAL ON PLANTS AND ANIMALS:

This product may be harmful or fatal to plant and animal life if released into the environment. Refer to Section 11 (Toxicological Information) for further data on the effects of this product's components on test animals.

12.3 EFFECT OF MATERIAL ON AQUATIC LIFE:

LC50 fish 1 > 10 mg/l (96 h Brachydanio rerio)

12.4 MOBILITY IN SOIL

No information available.

12.5 DEGRADABILITY

Not applicable.

12.6 BIOACCUMULATION

This product does not accumulate or biomagnify in the environment.

SECTION 13. DISPOSAL CONSIDERATIONS

Processing, use or contamination may change the waste disposal requirements. Do not dispose of on land, in surface waters, or in storm drains. Waste should be recycled or disposed of in accordance with regulations. Large amounts should be collected for reuse or consigned to licensed hazardous waste haulers for disposal. **ALL DISPOSAL MUST BE IN ACCORDANCE WITH ALL FEDERAL, STATE, PROVINCIAL, AND LOCAL REGULATIONS. IF IN DOUBT, CONTACT PROPER AGENCIES.**

SECTION 14. TRANSPORT INFORMATION

MARINE POLLUTANT: No
DOT/TDG SHIP NAME: UN1778, Fluorosilicic acid, 8, PG-II
DRUM LABEL: (CORROSIVE)
IATA / ICAO: UN1778, Fluorosilicic acid, 8, PG-II
IMO / IMDG: UN1778, Fluorosilicic acid, 8, PG-II
EMERGENCY RESPONSE GUIDEBOOK NUMBER: 154

SECTION 15. REGULATORY INFORMATION

15.1 EPA REGULATION:

SARA SECTION 311/312 HAZARDS: Acute health

All components of this product are on the TSCA inventory.

SARA Title III Section 313 Supplier Notification

This product contains no chemicals subject to the reporting requirements of Section 313 of the Emergency Planning & Community Right-To-Know Act of 1986 & of 40 CFR 372.

15.2 STATE REGULATIONS:

CALIFORNIA SAFE DRINKING WATER & TOXIC ENFORCEMENT ACT (PROPOSITION 65):

This product contains no chemicals known to the State of California to cause cancer or reproductive toxicity.

SECTION 15. REGULATORY INFORMATION (CONTINUED)

15.3 INTERNATIONAL REGULATIONS

The identified components of this product are listed on the chemical inventories of the following countries:

Australia (AICS), Canada (DSL or NDSL), China (IECSC), Europe (EINECS, ELINCS), Japan (METI/CSCL, MHLW/ISHL), South Korea (KECI), New Zealand (NZIOc), Philippines (PICCS), Switzerland (SWISS), Taiwan (NECSI), USA (TSCA).

15.4 CANADA: WORKPLACE HAZARDOUS MATERIALS INFORMATION SYSTEM (WHMIS)

D2B: Irritating to skin / eyes.

E: Corrosive Material.

This product was classified using the hazard criteria of the Controlled Products Regulations (CPR). This Document contains all information required by the CPR.

SECTION 16. OTHER INFORMATION

16.1 HAZARD RATINGS:

HEALTH (NFPA): 3, HEALTH (HMIS): 3, FLAMMABILITY: 0, PHYSICAL HAZARD: 0

(Personal Protection Rating to be supplied by user based on use conditions.)

This information is intended solely for the use of individuals trained in the NFPA & HMIS hazard rating systems.

16.2 EMPLOYEE TRAINING

See Section 2 (Hazards Identification). Employees should be made aware of all hazards of this material (as stated in this SDS) before handling it.

16.3 SDS DATE: 05/18/2015

NOTICE

The supplier disclaims all expressed or implied warranties of merchantability or fitness for a specific use, with respect to the product or the information provided herein, except for conformation to contracted specifications. All information appearing herein is based upon data obtained from manufacturers and/or recognized technical sources. While the information is believed to be accurate, we make no representations as to its accuracy or sufficiency.

Conditions of use are beyond our control, and therefore users are responsible for verifying the data under their own operating conditions to determine whether the product is suitable for their particular purposes and they assume all risks of their handling, and disposal of the product. Users also assume all risks in regards to the publication or use of, or reliance upon information contained herein.

This information relates only to the product designated herein, and does not relate to its use in combination with any other material or process.

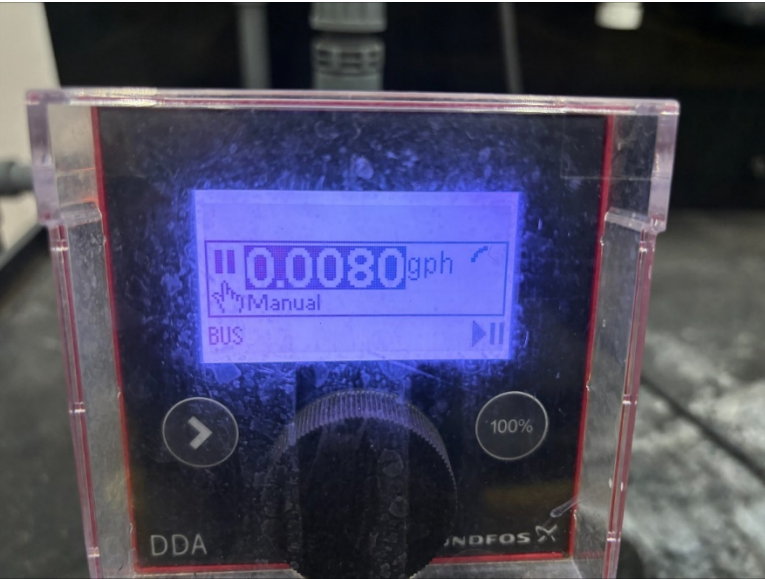
Attachment B



Standard Door Handle – Inside Water Treatment Plant



Standard Door Handle – Inside Fluoride Containment Room



Inside Fluoride Containment Room



Inside Fluoride Containment Room

Attachment B



Flow Meter – Inside Water Treatment Plant



Flow Meter – Inside Fluoride Containment Room



Pressure Gauge – Inside Water Treatment Plant



Pressure Gauge – Inside Fluoride Containment Room

Agenda Item 6



CITY MANAGER'S REPORT

Reporting period: October 2023

I. A. ADMINISTRATION – Ron Whitlatch, Interim City Manager

- City Manager Recruitment process is continuing to move forward. An Executive Session is scheduled for November to lay out next steps for the interview process and to determine which candidates will be interviewed. Several City Councilors have stopped by City Hall and reviewed (in depth) all of the applications received to date.
- Attended the Parks, Trees, and Trails Committee Meeting. Some of the topics that came up included funding for a Parks Master Plan Update, a new Adopt- A Park Program, memorial bench placement along the trails, and discussion regarding the Cheadle Lake Park Improvements Project.
- The City of Lebanon and City of Albany entered into a Memorandum of Agreement (MOA) for operations of Cheadle Lake and its potential impact to the Albany Santiam Canal in March 2022. As part of the memorandum, Lebanon agreed to placing no weir boards (which raise the lake level) in the out fall to the lake. After a year of operation and multiple complaints about low lake levels, Lebanon Staff placed a couple of weir boards in the outfall box in order to keep water in the lake for recreation and to provide necessary water to the entire ecosystem. This has caused Albany Staff some concern. We hope to meet with them in the coming weeks and (as the MOA allows for) discuss changing some of the conditions of the MOA to better fit the needs of Cheadle Lake while also maintaining the integrity of the canal/lake berm.
- Holding regular Department Director Meeting (every two weeks) to keep moving priorities forward.
- Continuing to work with multiple City Departments and Lebanon Fire on the City's current Event Permit Process. Any and all proposed changes would come before the City Council in the form of an Ordinance change at a later date.
- Continue to meet with the Multi-Agency (East Linn County) Disaster Prep Team.

B. HUMAN RESOURCES – Angela Solesbee, HR Director

- Recruitment:
 - Police Officer – Position posted on 9/13 open until filled. Applicants under review.
 - City Manager - Position posted on 9/13 open until filled. Applicants under review.
 - Communications Specialist – Posting closed on 5/25. Candidate in background process.
- Benefits:
 - Open Enrollment for benefit year 2024 closed on 10/27.
- Classification and Compensation:
 - Ongoing - Work has begun on adding/expanding the working conditions/physical requirements to the JD's. JD's are actively being updated as changes/updates are identified.
- Training and Development:
 - October Safety training – Blood Borne Pathogens (BBP)
 - November Safety training – Preventing Cold Related Illnesses
 - October All Employee training – Unconscious Bias in the Workplace: Identifying and Addressing Bias
 - November All Employee training – None
- Performance Management:
 - Assessing and testing the electronic performance evaluation tool under way.

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- 31 evaluations are past due as of 10/31/2023
 - 3 in Finance
 - 6 in City Manager's Office (Director Evals)
 - 6 in Public Works
 - 15 in Police Department
 - 1 in SC/LINX
- Other:
 - ADP Compensation/Total Rewards module complete. No implementation required. Module may be used to process annual COLAs but no other application of module will be utilized. ADP Learning module on hold.

II. CITY RECORDER – Julie Fisher, City Recorder

- Compiled and Prepared the City Council Agenda and Packet
- Minutes: September 13th Work Session and Regular Meeting
- Maintained and sent out the Preliminary Agenda Table
- Resolutions: 1
- Ordinances: 1
- Press Releases: 7
- Public Meeting Notices: 4
- Documents added to ORMS: 111 for a total of 19,970
- Public Records Request: 0
- Records Destruction Certification: 3
- Liquor Licenses Processed: 0
- Contact Us Submissions: 18
- Compiled and Prepared City Attorney Evaluation Items for upcoming December ES
- Maintained the City's social media accounts and website updates
- Began social media weekly postings on how to contact the City and various Departments
- Maintained the City boards/committee database, tracked terms, and archived minutes.
- Tracked all City agreements/contracts and sent out reminders about ones that are expiring
- Tracked and archived deeds, easements, and rights of way

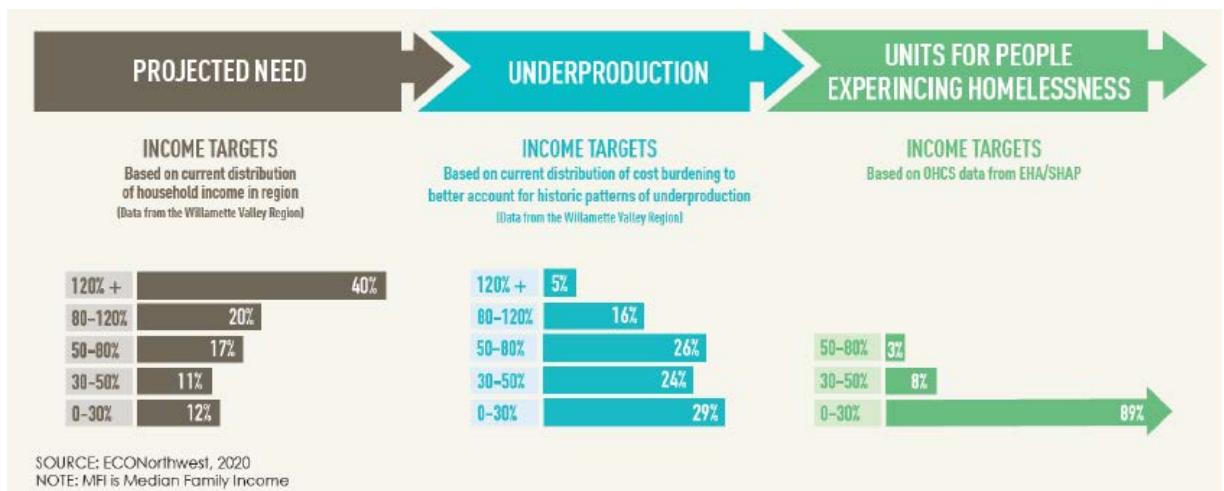
III. COMMUNITY DEVELOPMENT – Kelly Hart, Director

A. Planning:

- The October Planning Commission (PC) meeting included two land use actions:
 - Recommended approval of a Comprehensive Plan and Zoning Map amendment for 911 Airport Road to change the designation from Residential Mixed Density (C/Z-RM) to Mixed Use (C/Z-MU), then approved, contingent on City Council decision on the amendments, an Administrative Review and Conditional Use Permit to build and operate a convenience store and gas station.
 - Approved Conditional Use Permit CU-23-04 for the development and operation of a car wash facility on a property on the east side of S Santiam Highway, north of the Walgreens
- In October, two projects were approved administratively:
 - AR-23-06 for a zero-lot line/townhome development on Walker Road.
 - VAR-23-06 for a fence variance for a property on Sand Ridge Court.
- Staff is currently processing 9 planning applications for 4 projects:
 - CPMA-23-03 and ZMA-23-03 to change the land use designation for the property at 911 Airport Road from Residential Mixed Density to Mixed Use (Public Hearing scheduled before City Council in November)
 - AR-23-03, CPMA-23-01, CU-23-02, PLA-23-03, ZMA-23-01 to amend the land use designation to Mixed Use, consolidate the two parcels, and develop a convenience store and gas station on the southwest

corner of Airport Road and Stoltz Hill Road (application on hold by applicant till decision on alternative site is complete)

- A-23-02 to annex a portion of the property at 1880 Gilbert Street (application on hold per applicant to redefine the annexation boundary)
- DCA-23-01 to amend the development code and municipal code to authorize recreational vehicles as caretaker dwellings associated with private outdoor storage facilities, and to eliminate recreational vehicle parks as a permitted use in the city. (Public hearing scheduled for November Planning Commission meeting and December City Council meeting)
- **Housing Production Strategy:** A DLCD grant has been awarded to the City to complete development code updates associated with the Housing Production Strategy implementation. Staff is working with DLCD on the grant agreement and anticipate starting work with the consultant in February.
- **Community Development Block Grant (CDBG) Program:** The CDBG application has been submitted and is currently under review by the State for the next grant cycle to support the Linn County Housing Rehabilitation Program.
- **Psilocybin:** Map exhibits are being prepared to demonstrate possible authorization areas for psilocybin under state OARs, and a map to demonstrate the authorization areas under potential time, place, and manner ordinance restrictions. Staff is also compiling responses to outstanding questions from the initial work session and anticipate providing a discussion item at the December City Council meeting.
- **Continuum of Care (CoC) and Multi Agency Coordination (MAC) to address homelessness:** Community Services Consortium continues to present to all agencies within the proposed Linn, Benton, Lincoln continuum of care to continue the process to separate from the Rural Oregon Continuum of Care (ROCC). The Linn County MAC is also developing a Request for Proposals (RFP) for proposals from community partner organizations for projects intended to provide services to the unhoused under the Linn County Unsheltered Homelessness Emergency Response Plan utilizing HB 5019 funding. Funding and projects include:
 - Emergency Shelter Operations (Maximum Award: \$282,732)
 - Street Outreach to the Homeless (Maximum Award: \$50,000)
 - Rapid Re-Housing Services (Maximum Award: \$376,093)
- **Governor's Office Housing Production Framework:** The Senior Housing Policy Advisor has continued to communicate with Lebanon regarding housing production and barriers to development. A letter of response to the Governor's draft framework was submitted in October, and a follow-up meeting was held on October 30th. Based on the draft framework, the state is currently short 140,000 housing units. In addition to needing 140,000 units, the state needs to produce over 440,000 units in the next 20 years to keep pace with demand. Based on the housing need, the graphic below demonstrates the projected housing distribution based on income targets.



THE CITY THAT FRIENDLINESS BUILT

In addition, the Housing Production Advisory Council (HPAC) is holding regular forums on housing production and barrier elimination. On October 27th, the HPAC held a public forum on wetlands where two recommendations were considered:

- Option A: Require DSL to create wetland mitigation banks throughout the state to reduce mitigation credit costs, expand the in-lieu-fee program, provide a three-year suspension for the soil temporal loss rule, direct DSL to remove obstacles and extend credits to maximum extent possible and allow cities to exclude wetlands from the available land inventory.
- Option B: Through an emergency order, direct DSL to adopt the new “Waters of the United States” definition for 5 years, which is expected to remove up to 70% of all wetlands from the wetland designation and not subject to Army Corps or DSL review.

B. Building:

- A current list of the larger construction sites include:
 - Riverside Banks Subdivision and Duplexes (Williams Street)
 - 78-unit multifamily development (Russell Drive)
 - Lebanon Fire District property (Oak Street)
 - 28-single family dwelling lots - Cheadle Lake Estates (River Road)
 - Chipotle interior renovations and façade improvements (S Santiam Highway)
 - Seven Oaks Middle School Addition (Cascade Drive)
 - Anderlik Manor Renovation (W Grant Street)
 - Panda Express (Airport/Hwy 20)

C. Economic Development:

- **Rural Economic Alliance (REAL) or 8 Cities Update:** REAL with the support of the Economic Development District within the Oregon Cascades West Council of Governments (OCWCOG) applied for the Technical Assistance Grant with Business Oregon. The grant application identified that the Small Business Development Center (SBDC) would be the technical assistance provider for the grant and the grant scope would be to offer additional assistance to Spanish Speaking and rural small businesses, unfortunately REAL was unsuccessful in obtaining the grant. Based off from the priorities identified from the Strategic Plan REAL members formed smaller working groups to discuss the workplans associated with each priority area and will identify next steps and funding in the coming months. The next grant cycle for the Rural Opportunity Initiative (ROI) grant is open. REAL, partnering with RAIN will apply for the grant again as a scale-up grant to expand REAL’s capacity, while also providing additional entrepreneurial support to the region. REAL reached out to the previous consultant, JayRay, to get a quote to further the work associated with the Strategic Plan.
- **Economic Opportunities Analysis (EOA):** The Economic Development Catalyst, with the Community Development Director has begun the process of implementation of the EOA:
 - Business visitation program: In October, the business visitation program was initiated. The Mayor, with the Economic Development Catalyst and the Community Development Director visited the Laura Gillott Home Team, and Boulder Falls. Each business visit provided insights into the business operations, market trends and projections, and included an open dialogue on opportunities for the City to assist the businesses. Takeaways from the specific meetings included:
 - Laura Gillott Home Team: The current interest rates have hampered the market, and interest rates are anticipated to increase likely once more before leveling out and decreasing. For the availability of housing product, the team identified a significant need for single-family homes, there is limited options in the city for executive housing, and starter homes. A follow-up meeting is anticipated to be scheduled to further evaluate the City’s housing and sales data.
 - Boulder Falls: Oregon State Football has indicated they will continue to use Boulder Falls during the football season, despite the changes occurring in the PAC-12. The hotel is operating at a better than anticipated rate, with an increase in events and conferences at the event center. Extensive discussion focused on opportunities to connect local businesses and restaurants with Boulder Falls to advertise local amenities to share with conference organizers to bolster local business support

from outsiders visiting Lebanon. Opportunities to utilize the hotel tax funds to help develop a city map advertising local attractions and restaurants were discussed. The Economic Development Catalyst will also be facilitating a meeting between Boulder Falls, the Chamber of Commerce and the Downtown Association to open dialogue between the organizations.

The business visitation program is anticipated to be a monthly program. At the direction of the Mayor, each month, the business visits will be scheduled based on Ward boundary, and the Councilor representatives for the Ward will be invited to join.

- **Industrial Site Readiness:** The Economic Development Catalyst is working with a local commercial broker to contact Industrial property owners to identify interest in promoting their site for business recruitment nationally, and to advertise their properties on Oregon Prospector.
- **Downtown Building Restoration Program:** The Downtown Building Restoration (DBR) Program is underway, all applicants have signed and returned their contracts, and work has begun on a few of the projects. As projects finish different activities associated with their projects, the City is providing payments to contractors in compliance with the contracts.
- The Economic Development Catalyst continues to attend Linn County Commissioner meetings in an effort to be more visible to the county and to represent the interests of Lebanon and the REAL group.
- **Broadband Feasibility Study:** The Broadband Feasibility Study for Linn, Benton and Lincoln is wrapping up and the report will be shared shortly. The next step is to develop a regional taskforce to actively pursue grants to extend broadband availability. At this juncture, with the City of Lebanon largely being served by broadband, it is believed the County would be the most appropriate agency to develop the taskforce to support the county areas and smaller communities underserved by broadband.
- **Workforce Development Initiative:** The Economic Development Catalyst is working to organize a meeting between major employers in Lebanon that share childcare related challenges and potentially brainstorm solutions that would be grant eligible. Business Oregon did get additional Childcare funds, but the parameters of the program are not established yet. The Regional Development Officer with Business Oregon will send information as it becomes available.
- **Business Registration Initiative:** The Director of the Oregon Economic Development Association (OEDA) will be meeting with the Lobbyist for OEDA and State Representative Bynum to discuss the Business Registry Data Bill that was proposed in the 2023 long session but was unable to be brought forward. The goal is to resubmit the bill for the 2024 short session. In the meantime, the Economic Development Catalyst is refining the list of businesses in the 97355 area code for outreach and support purposes.

IV. ENGINEERING SERVICES – Ron Whitlatch, Director

- As time allows, Engineering Staff continues to work on designs for additional small sewer replacement/rehab projects. These include East Ash (between Carlson and Creswell), Fourth Street (between Maple and Grant), and main extension along Division Way. Staff is continuing to receive applications for replacement of sanitary sewer laterals (through the Small Sewer Lateral Replacement Program).
- Kennedy Jenks Engineering (KJE) and City Staff are continuing to work on the Wastewater Treatment Plant Facility Plan. Staff is currently reviewing several draft chapters of the Master Plan and will be meeting in mid-November to discuss with Kennedy Jenks. Staff is continuing to complete additional testing to provide data to determine the parameters for the new NPDES Discharge Permit. The Masterplan is scheduled for completion in late fall of 2023; however, it is likely that this will push out several months due to consultant and staff workloads. The new NPDES Discharge Permit is scheduled to be complete during, or shortly after this same time period.

Trails Update:

- Design of the multi-use path along Airport Road (in conjunction with the Airport Road/Stoltz Hill Signal Project) from Seventh Street to Burkhart Creek will be completed with the signal drawings and incorporated into the overall project. Staff has completed a design for a trail to connect the Old Mill Trail and the River Park Trail in the Gills Landing Parking lot. BLT is currently looking for grants along with donations to complete this project. The City has submitted and will be receiving a Recreational Trails Grant to complete a segment of trail on the Albany Property next to River

Park. Design will begin once we have been issued Notice to Proceed. Construction for the Cheadle Lake Extension Trail project has begun. North Santiam Paving Company has a majority of the base rock placed and will be paving the new trail in the coming weeks. The project is being funded by a grant from the State of Oregon.

- Emery & Sons Construction has completed sanitary sewer pipe installation on Crowfoot Road (from South Main to View Lane) and will begin restoration. They hope to be completed by mid-November and begin the final leg of the project on South Main Road (from Crowfoot to Joy). Crowfoot Road is currently closed to traffic from South main Road to Hillview Road. Staff is continuing to send out daily construction impact maps to Emergency Services identifying access to and from the area. Below is the current financial status of the project. This will be updated to reflect changes/additions that occur during construction.

Approved GMP Values		Contingencies Used to Date	
Construction Cost	\$18,029,671.95	Contractor Contingency	\$182,383.65
Contractor Contingency	\$398,026.75	City Contingency	\$692,179.00
City Contingency	\$995,066.86	Allowances Spent to Date	
Contractor Allowances	\$1,902,065.32	Trench Foundation	\$85,690.23
Total GMP Approved	\$21,324,830.88	Dewatering	\$348,932.64

- Staff has been actively working on our required TMDL update to DEQ. The update was submitted and has been approved by Oregon DEQ. As time allows, we will begin updating the City's Five-Year Matrix and submit it to DEQ for approval. In the coming months, there will also be a big push by DEQ to begin the water temperature requirements for each jurisdiction. The City will not know the impacts of this, if any, until the new regulations have been issued. It is possible that the new regulations (which tie into the City's NPDES Permit Update) could require action at the Wastewater Plant in order to mitigate temperature. The TMDL will require more extensive water quality efforts for storm drainage included in future projects and require testing of storm drainage outfalls as well as temperature controls in the near future. In conjunction with the TMDL requirements, Staff will be sending letters to commercial/industrial properties reminding them to have their private catch basins cleaned yearly to help eliminate pollutants from entering drainage ways.
- Staff is finalizing the design and updating the Cost Estimate for the Seventh Street Reconstruction Project (Oak to 'F'). Final drawings have been issued to private utility companies to begin relocation of conflicting utilities this fall. We anticipate advertising the project for bids in December 2023/January 2024. The project is estimated to cost \$3.6 million and will take at least 9 months to complete. Prior to construction, Staff will hold a neighborhood meeting for residents living along Seventh Street to inform them of what to expect, length of construction, and inconveniences associated with it.
- Engineering Staff is currently working on several small projects which include design for drainage pipe replacement on Airport Road at Safeway (existing pipe is undersized leading to flooding in heavy rain events), Sanitary Sewer Extension at US bank/River Center, drainage pipe replacement on Leonard Street, Rapid Flashing Beacon Pedestrian Crossing on South main, updating of our Pre-Treatment Ordinances, multiple sewer lateral replacements, and updating the City's Engineering Design Standards for Development as time and staffing availability allows.
- Staff is working with ODOT for a design approval of several sidewalk access ramps on Grant Street between Park and Second Street. Along with this approval, Staff is actively designing the waterline replacement and street upgrades on Grant Street between Main Street and Fifth Street. Drawings were sent to ODOT for review at the end of October. Based on current workload, we anticipate the project going out for bids in early 2024 (this is contingent upon ODOT review time).
- As Staff is available, we are continuing the process to update the City's five year Capital Improvement Projects Plan. This effort will likely take several months (being done as Staff time allows) to complete. Staff will bring the completed document to the City Council for approval sometime in 2024.
- The City is preparing for a new Traffic Signal at the intersection of Airport Road and Stoltz Hill Road. This project will also extend waterline and sanitary sewer line on Stoltz Hill Road from Antioch Street to Airport Road, sidewalk and

driveway installation, and likely a multi-use path along Airport Road. The Project Team (Kittelsohn Engineers, Udell Engineering, and City Staff) are moving forward with the final design for the project. The project has been delayed for over a year now due to right-of-way needs and ongoing negotiations with a property owner. Linn County is currently in the process of obtaining the needed right-of-way (which is going through eminent domain). The Project Team will begin finalizing the design, so it is ready to advertise for bids as soon as right of way is obtained.

- Kittelsohn Traffic Engineers is in the process of designing a round-about at the Cascade Drive/Weldwood Intersection. They have provided a draft concept of the intersection which Staff has been reviewing. We will be meeting with them in early November to wrap up the 35% conceptual design. We are investigating the possibility of applying for a transportation RAISE Grant or a Safe Routes to School Grant to fund the project.
- The Contract for the demolition of the Old Water Treatment Plant was awarded to Staton Companies, at the October 11th City Council meeting. The contract time for this project is 300 days. Staton has indicated they will begin the project in late February or early March of 2024. They believe the demolition will take substantially less time than what Staff and the Consultant Team had estimated.
- ODOT and City Staff are continuing the study of pedestrian/bicycle safety along HWY 20 and HWY 34 through Lebanon. We have held multiple meetings to discuss bike routes along the highways and alternate routes using City streets. The project is also looking at various intersections and mid-block locations to determine what safety projects could be completed to improve pedestrian and bicycle movements on the highways through Lebanon. Funding for improvements will likely come from ODOT. The final report is now scheduled to be done early 2024.
- Bids for the Rapid Flashing Beacon Project on Tennessee Road (At Beaton Lane) were received in August 2022 with the lowest being over \$100,000, which is double the amount that staff had planned for the project. Staff will be preparing the project to advertise for bids again and also adding the Rapid Flashing Beacon to South Main Road for a combined project. Udell Engineering is currently under contract to design the crossing on South Main in the vicinity of Cedar Drive. It is likely this project will be bid in spring of 2024 due to current workload in the Engineering Department. Build Lebanon Trails has also committed \$20,000 to this project in an effort to get it done.
- Cheadle Lake Park Utility Extension Project is underway. Northcore USA has finished installation of waterline on Cascade Drive and Crowfoot Road. They have also completed the bore to get the waterline under HWY 20 and the RR Tracks. The project extends waterline from Cascade Drive (in front of Seven Oaks School) along Crowfoot Road, crossing HWY 20 and along Weirich Road to the park. There is approximately 600 feet of sanitary sewer that is currently being installed along Weirich Road from the main park entrance to the southern property line of the park. The project is scheduled to be complete in January 2024.
- Udell Engineering is working on a conceptual master plan for the park which has been reviewed by the Parks, Trees, Trails Committee. The cost estimate for the initial conceptual design was substantially more than we have available in funding. Staff and Udell Engineering will come up with an alternate design with a much more paired down scope, with the focus being on accessibility improvements, backbone utilities, and hopefully a stage/event area. It is likely construction will take place of any improvements will be in 2025.
- Udell Engineering is under contract to provide Engineering Design services for a sanitary sewer line replacement on North Third Street. Staff is working with Udell to determine the best approach to replacing the line since it is located in the alley with very little maneuvering room. The existing line has multiple failures and a substantial amount of infiltration. Once design is complete, Staff will advertise the projects for bids, likely spring/summer of 2024.

Development:

- Paventy & Brown Orthodontics plans approved for construction. ODOT permit approved. Building interior design modifications are underway.
- Steven King site plan for Airport Road has been approved and building permits issued, contractor working on final site improvements, driveway approach and detention pond complete.
- McKinney Phase II apartment complex off of Russel Drive is currently under construction. Final walk-through schedule to provide punch list to contractor for repairs.
- Site plan approved for a new apartment building off of S Main south of Walker Road. Contractor working on punch list items. Buildings under construction.

- Dairy Queen site plan approved for construction. Building permits issued to start construction. Developer currently getting bids for the project.
- Plans approved for Hickory Lake Apartments off of Airport Road. Developer waiting on erosion control permit approval.
- Cheadle Lake Estates subdivision public improvements accepted, several homes remaining still under construction.
- New subdivision at Franklin & Russell Streets is currently under construction. All public improvements completed and passed testing. Punch list to contractor for repairs prior to acceptance of public improvement.
- Plans approved 3-duplexes off 9th Street. Water line extension complete and online. Buildings under construction. Private storm drainage under review with design engineer for modifications. Punch improvement and site punch list provided to the contractor.
- Plans approved for water main extension to Airport Storage. Site plan approved for phased storage pod placement. Developer has on hold to complete additional phases.
- Cascade Estates Subdivision complete and public improvements accepted. Contractor in 1-year warranty period. Home construction is under way along private street.
- Plans returned to engineer for revisions for Bate's storage facility on Hansard Ave.
- Plans returned to engineer for revisions for storage facility off of Industrial Way.
- Site and public improvements for new Fire Station approved. Utility crossing on 12th Street complete contractor working onsite. Building under construction and additional off-site work being complete along Oak Street.
- Plans approved for Buckmaster Plumbing new storage building. Contractor working building addition. Improvements schedule for later this year.
- Plans pending approved for half street and utility extension on the corner of Stoltz Hill & Kees Streets. Plans stamped approved and ready for permit to start improvements.
- Weldwood Dr storage plans approved, public improvement permit issued contractor to start construction later in October, first of November.
- Plans approved for Chipotle Restaurant. Contractor working on building remodel.
- Plans approved for new sewer extension along Oak St.
- Plans approved for new Panda Express Building corner of Airport and Hwy 20. Contractor complete sewer lateral connection during night work additional night work schedule for water main and storm connection.
- Plans approved for new subdivision Madelyn Meadows between Walker & Wassom Street. Contractor working on sewer main extension and excavation of new private street.
- Plans approved for a new apartment complex on the corner of Airport & S 2nd Street. Contractor working on check list for permit issuance. Public improvements bonded building permits issued for foundation only.
- Traffic Impact Analysis for Crowfoot Subdivision under Engineering Service Agreement contract. Traffic counts to be collected once S Main Road & Crowfoot open to traffic.

V. FINANCE SERVICES – Brandon Neish, Finance Director

- **Accounts Payable:**
 - Payments made in October 2023; 259 checks were processed for payments of \$2,945,522.59.
- **Payroll:**
 - Payroll was processed on October 13th and October 27th for all employees. In total, 133 employees were paid during the month of August.
 - There have been three (3) manual checks processed since July 1, 2023 for payroll errors:
 - July 2023:
 - Direct deposit returned (employee entered wrong deposit information).
 - Cash out missed during payroll entry (paperwork was delivered timely).
 - August 2023:
 - No payroll errors identified in August 2023.

- September 2023:
 - Team leader pay missed. Team Leader due to configuration error with calculation (same issue from May 2023 but missed one individual when fixed then).
- October 2023:
 - No payroll errors identified in October 2023.
- **Additional:**
 - Finance is gearing up for our final field audit October 30 – November 3. Necessary workpapers and a final trial balance were provided on-time to the City’s auditors and we are looking forward to completing all necessary reports timely.
 - Staff completed the implementation of DebtBook, a software designed to track debt, leases, and new accounting requirements for IT/software contracts. This work was vital for successful implementation of GASB 96, a new rule from the Governmental Accounting Standards Board requiring additional accounting for long-term, subscription-based IT contracts. A big thank you goes to Brent Hurst, IT Director and Erin Gomez, IT Administrative Assistant for their efforts in pulling together contracts and walking through the implementation with us.
 - Finance staff attended the Oregon Government Finance Officers’ Association Fall Conference in Eugene October 16 – 17. There were multiple training sessions on financial reporting and audit changes that was extremely beneficial for the two staff that attended. Staff will continue to find applicable training opportunities to improve reporting and activity within Finance and City-wide.
 - Finance staff has amended how lockout days function. Staff are coming in at 7am to prepare the list and record any last minute payments to get the list out earlier and allow customers additional time. In the evening, staff is staying until 7pm to provide customers additional support and get water services restored. This has been done for a few months now and seems to be working well. Lockouts are also now always on the 3rd Wednesday of each month to provide some consistency.
- **Utility Billing for October 2023:**
 - 6,444 billing statements (including electronics) were mailed October 26th for a total of \$1,158,460 in utility revenue.
 - 51 Owner Lien (past due) notification letters were mailed.
 - On October 12th, 395 phone calls went out to notify customers they have a past due balance.
 - 73 accounts were locked out on October 18th for past due bills.
 - There was a total of 300 service requests: 59 move ins, 73 lock-offs, 9 re-read meters, 77 reconnects, 31 move outs, 8 leak checks, 1 changed meter, 28 turn ons, 5 turn offs, 3 meter installations, zero meter removals, no dead meters, no water quality checks, no pressure tests, 1 emergency request, no meter tests, 2 meter locates and 3 miscellaneous requests.

Utility Billing Data

	Oct-22	Nov-22	Dec-22	Jan-23	Feb-23	Mar-23	Apr-23	May-23	Jun-23	Jul-23	Aug-23	Sep-23	Oct-23
Active Accounts	6,433	6,439	6,442	6,445	6,448	6,459	6,459	6,459	6,459	6,459	6,516	6,516	6,555
Penalty Applied	977	1,013	976	883	711	953	721	N/A	296	236	265	280	365
Lock Offs	110	125	N/A	118	56	68	68	83	71	74	0	100	73

Municipal Court Data

	Oct-22	Nov-22	Dec-22	Jan-23	Feb-23	Mar-23	Apr-23	May-23	Jun-23	Jul-23	Aug-23	Sep-23	Oct-23
Charges Filed	91	111	45	135	94	134	100	94	129	102	137	166	109
Show Cause Issued	49	39	42	43	43	39	34	43	42	54	48	56	61
Licenses Suspensions Issued	12	18	9	23	17	10	30	22	23	20	38	32	30
Warrants Issued	110	203	102	186	105	159	180	154	144	160	231	203	150
Charges Disposed	76	103	52	114	157	99	168	115	80	137	141	123	121

THE CITY THAT FRIENDLINESS BUILT

VI. INFORMATION TECHNOLOGY SERVICES – Brent Hurst, Director

A. IT:

- The web site upgrade project is starting in collaboration with the City Recorder and other departments.
- The budgeted SCADA upgrade project continues to be worked on.
- Staff participated in multiple Cyber Security meetings and awareness events during October in Salem and online with MS-ISAC and CISA.
- Staff assisted with Zoom & YouTube Santiam Travel Station (STS) meetings for City Council, Parks, Trees, and Trails, and other staff meetings as requested.
- Staff and Public Works continue the process of upgrading outdated cameras around the City that need replacement.
- Progress continues with Lebanon Fire District on the Station 31 construction project.
- Weekly and monthly security and vulnerability patches were completed.
- Staff addressed multiple other routine break-fix issues, equipment replacements, and maintenance renewals for IT.

B. GIS:

- Continued GIS updates for the Sanitary Sewer GIS system upgrade project. This will be a multi-month project similar to the Water GIS system update completed Spring 2023.
- Worked with Finance and Public Works to update meter reading routes for City water services. This is nearing completion.
- Work on the 2023 Aerial Photography flyover project continues. Large prints have been taken delivery of for use in the City.
- Assisted with new TV Van and coordinated TV Van camera system configuration for new vehicle.
- Working on the annual water distribution system flush project preparation for the water crews.
- Continued working on Water Distribution System Air Relief Valve Coordination.
- Worked with Planning on planning and development projects for Council as requested by planning.
- Coordinated new address updates with Community Development and Linn County GIS.
- The monthly tax lot updates from Linn County were completed.

Summary:

- During the past month, IT and GIS closed 368 tickets or work orders. This includes system generated tickets that needed analysis and resolution in addition to end user requests for help.

VII. LIBRARY – Kendra Antila, Director

- We are in the process of uploading back issues of LHS yearbooks from 1915 – 2016 to the city's website.
- Our Friends of the Library will be holding their 3rd annual silent auction from October 30th through November 11th. Proceeds from the auction will benefit the children's room expansion. The Friends hope to donate another \$20,000 towards the project this year.
- We will be hosting Family Game Night on Friday, November 3rd from 6:00 – 8:00 pm. Nerf Capture the Flag for ages 6 to adult will be played from 6-7pm, followed by BINGO for all ages from 7-8pm.
- Back by popular demand, we'll hold our second annual puzzle race on November 15th. Up to 9 teams will compete to see who can complete the 500 piece puzzle the fastest.

VIII. POLICE – Frank Stevenson, Chief of Police

- For the month of October, the Patrol Division had approximately 1,500 calls for service, conducted 119 traffic stops, made 74 arrests, issued 31 traffic citations, and wrote 152 case reports.
- The Detective Division remains busy. They were assigned one (1) new case, assisted on three (3) separate overdoses to locate the supplier, were able to close out three (3) cases, and reviewed ninety-four (94) DHS referrals where child

abuse was reported. Detectives also assisted with one search warrant out of Albany and attended a child abuse investigation training course through the ABC House.

- Recruit Garcia began his multiple-month training at Department of Public Safety Standards and Training (DPSST) the first part of October; Recruit Officer Belknap began in the middle of the month.
- Recruit Herrejon has completed the Field Training and Evaluation Program and is officially a solo officer as of 10/23/2023.
- Recruit McKinney is continuing with field training. Currently he is riding with an assigned field training officer and is scheduled to begin attending the DPSST Academy mid-November.
- We had another recruit resign this month due to complications experienced during training, bringing the total to three recruits who have resigned during the training and probation phase; we currently have an open recruitment process underway to fill vacancies.
- The Community Services Division remains active in the community. This past month, Community Policing staff responded to approximately 99 calls for service which covered a variety of topics (ordinance violations, parking complaints, park patrol, abandoned vehicles/trailers, transient calls and welfare checks).
- Staff continued to work on ODOT grants for distracted driving, DUII, speed and seatbelt violation enforcement.
- Five (5) online Traffic Safety classes were administered.
- This month, there have been a total of three (3) individuals who have used the overnight sleeping area; two (2) used the Oak Street site and one (1) used the Justice Center site.
- There were six (6) use-of-force incidents to report during the past month: (1) A male juvenile at the high school pushed a school administrator and grabbed ahold of the School Resource Officer's vest. The juvenile tried to forcefully push past the officer to get to another student; the officer was able to grab ahold of the juvenile's arms and take him to the ground. While on the ground, the juvenile tried to fight, as well as bang his own head on the floor. The officer placed his own arm in front of the juvenile's head to prevent any injury. Other officers arrived and were able to take the juvenile into custody with no injuries. (2) A Linn County Deputy was transporting a male subject to the hospital for a mental health check, and requested LPD assistance, when the subject began hitting his head on the inside of the deputy's vehicle. The deputy removed the male subject from the patrol vehicle, and LPD placed the subject in our WRAP restraint device. The subject continued to be verbally aggressive but was physically cooperative while under constraint of the WRAP tool. (3) Officers assisted the Linn County Sheriff's Office on a vehicle pursuit; after the vehicle came to a rest, the suspect was resisting, and officers applied handcuffs to the subject while holding them against the vehicle. Officers then held the subject on the ground and placed the subject in the WRAP restraint. (4) A subject involved in a motorcycle pursuit ultimately crashed and then fled on foot. An officer was able to locate the subject hiding in bushes and pointed his firearm at the subject. The subject decided to comply and was taken into custody without further incident. (5) A subject was trying to break into the liquor store and had been reported as starting fights with people in the parking lot and also trying to break into vehicles. Upon officers' arrival, the subject was being held down by a citizen. The subject got up and officers had to push him against a vehicle to try and control him. While officers tried to handcuff the subject, he went to the ground and started rolling around. While on the ground, the subject tried to grab for one of the officer's firearms. The officer attempted to deploy his Taser on the subject but was unsuccessful. Officers then grabbed the subjects' arms and put their knees on his legs to gain control of the subject. The subject was then handcuffed and transported to Lebanon Hospital, where he was chemically sedated. He was later transported to Linn County Jail. The subject sustained minor scrapes on his head, shoulder, and knees, while the responding officer sustained small scrapes on his knees and wrist. (6) Two subjects were contacted in a stolen vehicle. An officer pointed his firearm at the subjects and issued verbal commands. Both subjects complied and were taken into custody without incident. Every use-of-force incident is reviewed extensively by a Sergeant, Lieutenant, the Captain, and finally the Chief.
- There were two incidents of successful NARCAN deployment by officers this month. Due to their quick action, one male subject survived after he ingested fentanyl after running from police, and another male subject was saved after he had overdosed on fentanyl. Both male subjects were treated at the hospital and later released.
- There were five (5) pursuits this month. (1)The Linn County Sheriff's Office requested assistance in locating a felony hit-and-run suspect vehicle. LPD located the vehicle, and when a traffic stop was attempted, the vehicle sped away

and a pursuit ensued. Lebanon, Linn County and OSP assisted as the suspect vehicle drove from Berlin Road through Lebanon, and ended toward the east end of Sweet Home. Two OSP patrol vehicles were damaged when they boxed the suspect vehicle in. The suspect was taken into custody, and no injuries were reported. (2) An officer attempted a traffic stop on a motorcycle for a minor traffic violation. When the motorcycle began to drive recklessly, the officer terminated the pursuit. Soon after, it was learned the suspect crashed the motorcycle and fled on foot. He was later located and taken into custody; no injuries were reported. (3) LPD assisted the Linn County Sheriff's office with a low-speed pursuit through town. The driver of the suspect vehicle decided to pull over, and was taken into custody without further incident. (4) While on patrol, an officer tried to conduct a traffic stop on a driver who was felony-level suspended and had several active warrants. The driver failed to stop, driving away at a high rate of speed. The officer terminated the pursuit due to the reckless nature of the driver, who ultimately got away. (5) Officers attempted to contact a reported suspicious subject parked in a vehicle. As an officer went to make contact, the subject drove off in the vehicle at a high rate of speed. Officers attempted to stop the vehicle; however, the driver went through a construction site and officers terminated the pursuit. Each pursuit was reviewed extensively and found to be within policy.

IX. PUBLIC WORKS – Jason Williams, Director

A. Collections (Sanitary-Storm)

- The new TV Van has been used several times and is working good overall. A new video capture card seems to have corrected our recording issues.
- Mowing:
 - Maintenance mowing and weed eating, some areas are getting too wet to safely mow for the remainder of the season.
- Manholes:
 - Responded to report of lid off a manhole on Walker Road, corrected issue and lid has remained in place. Contractor who installed the manhole was notified of the incident.
- Sewer Mains:
 - Cleaned 400 feet and video inspected 0 Feet of sanitary sewer main line.
 - Flushed low flow, dead end sewer mains.
 - Found and cleared a sewer main plug, issue appeared to be landscaping rock that had entered through a cleanout and caused a blockage as well as large quantity of "flushable" wipes.
- Sewer Laterals:
 - Assisted 6 customers with sewer lateral issues.
 - Video inspected 345 feet of sanitary sewer laterals.
 - No cleanouts installed.
 - Conducted 2 sewer lateral replacement investigations.
 - 0 -entered program
 - 0 -not eligible or did not require replacement
 - 2 -working with customer to gather more information
 - Note: We were able to get all private laterals to at least a temporary working condition
- Storm:
 - Cleaned ditch inlets and culverts to improve storm water flow.
 - Cleaned catch basins in several problem areas.
 - Completed several rounds of catch basin/curb inlet clearing during rain events.
- Assisted with water main installation on N. Williams Street.
- Assisted Vactoring De-watering wells on Weirich Drive.
- Cleaned catch basins for Linn County in Millersburg as labor exchange for Berlin Road guard rail installation.
- Reviewed pre-plans for new construction.
- Monthly equipment checks and maintenance completed.
- Checked River Park RV Dump Station holding tank, and operation of the pump.

- Checked Gill's Landing Pump Station and pump operation.
- Abandoned storm main at old water treatment plant in preparation of demolition.
- Re-located sanitary main cleanout and abandoned sanitary laterals at old water plant in preparation of demolition.
- Started fall well meter reads.
- Working on two warranty inspections: Westside interceptor (post construction punch list) and Teen Challenge Sanitary Extension (1 yr. warranty)
- Assisted with water service orders.
- Work was completed at 3400 Cooperative Way to prevent back-ups.

B. Parks

- Opened, closed, and cleaned parks restroom buildings daily.
- All parks and trails system garbage's checked daily and emptied.
- The trail system is cleaned weekly of leaves and other debris.
- All park and school irrigation systems have been shut down and winterized.
- Daily mowing is taking place as well as management of the weeds along trails, in parks, and at school facilities.
- 31 hours were spent on vandalism repair. There has also been an increase in daily vandalism and trash pick-up which has increased the amount of time it takes crews to clean restrooms and complete trash pick-up.
- Cleaning gutters and roofs on gazebos and bathrooms to get ready for rainy season.
- Cleaning exterior of all bathrooms and spraying insects.
- The park sign at Christopher Columbus was replaced.
- Picnic tables have been removed from city parks for repair. Crews left some tables at Booth Park, Century Park, and Christopher Columbus Park for use during this time.
- Crews began repainting the interior of all park restrooms.
- Started trimming trees and shrubs at all city parks.
- Three toilets were broken by vandals at the Bob Smith Memorial Park restrooms, replacement toilets have been ordered.
- Gills Landing:
 - There were 72 RV park reservations for the month of October.
 - There were 3 shelter rentals for the month of October.

C. Streets

- 1 – day was spent sweeping Brownsville.
- Crews graded alley ways and filled potholes throughout town.
- Crews replaced street signs / posts that had been damaged, graffitied, or were faded.
- Crews completed cleaning and repainting yellow curbs.
- Placed and removed barricades / stop signs for internal / external projects, and public events.
- Completed vehicle repairs and maintenance.
- Mowed public rights-of-ways, trails, and Cheadle Lake grounds.
- Completed tree removal on 7th / Sherman and Porter / Russel Road.
- Assisted Distribution Department with water service orders including leak checks, turn-on, emergency shut offs, and water service repairs.
- Assisted Collections Department.
- Submitted locates for upcoming projects, including sign installation on 2nd and Mary Street.
- Completed vehicle and equipment maintenance.
- Crews started installation of thermal throughout town.

D. Wastewater Treatment Plant

- The Lebanon WWTP is full compliance with our NPDES permit month to date.
- Influent flow for the month is averaging 1.9 MGD.

- We are in our 22nd out of 24 months of permit renewal sampling now and going well.

E. Water

- Meter reading was completed.
- Daily water service orders including leak checks, locates, taste and quality issues, water samples and other customer concerns continue.
 - 82 locates were completed throughout Lebanon for the month of October.
- Changed out failing meter boxes and dead meters.
- Worked on the water maintenance list.
- Vehicle Maintenance.
- Worked daily on service line leaks.

F. Water Treatment Plant

Production	
Monthly Water Use (Intake Flow Meter)	125.05 MG
Finish Water Produced	85.69 MG
Water Sent to Cheadle Lake	33.26 MG

Water Quality					
Finish Chlorine			CT Basin Turbidity		
Min > 0.20 mg/L	Max < 4.00 mg/L	Average ~ 1.00 mg/L	Min	Max < 1.000 NTU	Average
0.96	1.76	1.34	0.10	0.10	0.020
Finish pH			Filtrate Fluoride (Average of Each Day)		
Min > 7.00 pH	Max < 9.00 pH	Average	Min	Max < 4.00 mg/L	Average ~ 0.70 mg/L
7.35	8.09	7.65	0.00	1.43	0.55

Maintenance/Operations:

- The onsite Chlorine Generation System is working great; we are just about finished blending the storage tank.
- Intake Rab Screen rebuild on 10/4.
- Inspection on all distribution air relief valves went well, all are in working order.

X. SENIOR SERVICES and LINX – Kindra Oliver, Director

- We are planning our Veterans Recognition Reception for Thursday, November 9th. Our program includes students from Seven Oaks' leadership class sharing some cards and short presentations of gratitude, the JROTC posting and retiring the colors, and more! The Linn County Veterans Services will also have an information table available during this time. It is a privilege to get to honor veterans and their families from our community every year.
- Our biggest event of the year is our Thanksgiving Banquet, which we will be holding in person this year, on Thursday, November 16th. During the COVID pandemic, we provided a remote banquet by delivering Thanksgiving meals to hundreds of seniors in 2021, 2022, and 2023. This year, we are happy to be reconnecting with community partners, old and new to hold the banquet in person. We are bolstering volunteer numbers to get the help needed to feed 250 seniors. This year we will have some entertainment from the Parks family and their friends from the Oregon Fiddlers Association. The Lebanon High School Cheerleaders will be helping to serve the meal. Thank you to all for helping to make this wonderful event happen.
- Thank you to Cascades West Council of Governments and Older Americans Act funding, the Lebanon Senior Center

has ten new I-pads to implement technology programs and have available for use at the Senior Center. We plan to offer an I-Pad 101 class in November and December to help seniors get familiar with the I-pads and will be offering to check them out to use in our library during our hours of operation. Programming is aimed to help address isolation and loneliness by helping to stay connected with family and friends, access resources on the internet, engage in healthy brain activities, participate in technology programs and help keep seniors engaged in social groups. One of our regular tech volunteers has offered his time to facilitate the introductory class and offer one-on-one assistance, by appointment.

- The Lebanon Square Circlers have expanded their classes at the Senior Center. In addition to their free lessons every Tuesday, they are holding their Sunday evening classes and every other Saturday dances here as well.

*Executive Session

Per ORS 192.660(2)(e) To conduct deliberations with persons designated by the governing body to negotiate real property transactions.

** Executive Sessions are closed to the public due to the highly confidential nature of the subject. It is unlawful to discuss anything outside of the Executive Session.*

*Executive Session

Per ORS 192.660(2)(a) To consider the employment of a public officer, employee, staff member or individual agent.

** Executive Sessions are closed to the public due to the highly confidential nature of the subject. It is unlawful to discuss anything outside of the Executive Session.*